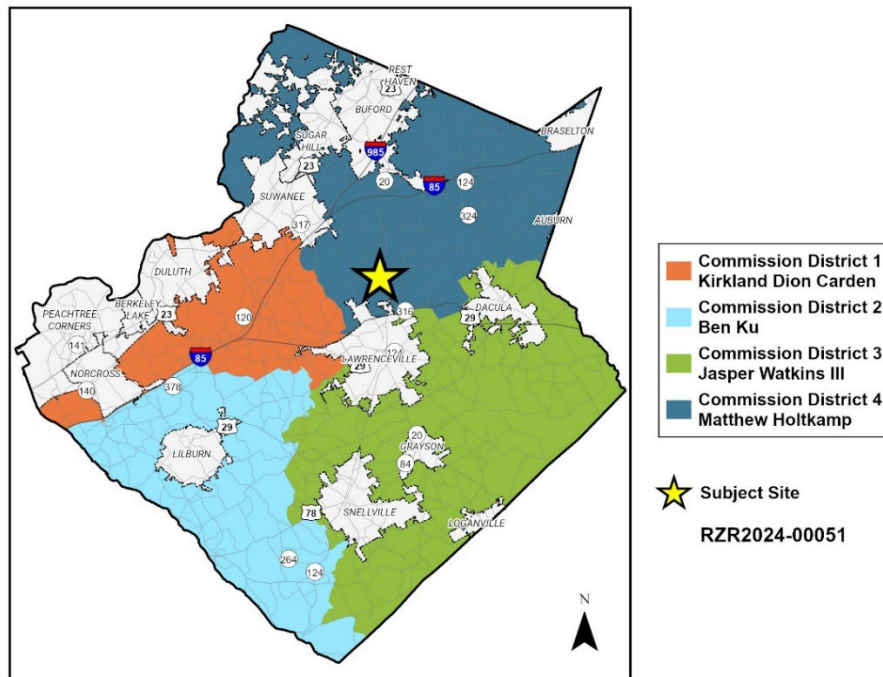


PLANNING AND DEVELOPMENT DEPARTMENT CASE REPORT

Case Number: RZR2024-00051
Current Zoning: R-100 (Single-Family Residence District)
Request: Rezoning to R-IF (Residential In-Fill District)
Address: 598 Russell Road
Map Number: R7066 002
Site Area: 12.15 acres
Units: 105
Proposed Development: Townhouses and Single-Family Detached Subdivision
Commission District: District 4 – Commissioner Holtkamp
Future Development Type: Neighborhood Low Mix

Staff Recommendation: APPROVAL WITH CONDITIONS

**Planning Commission
Recommendation:** PUBLIC HEARING HELD/RECOMMENDATION TABLED TO 12/2/2024



Planning Commission Advertised Public Hearing Date: 11/4/2024 (Public Hearing Held 11/4/2024/Recommendation Tabled to 12/2/2024)

Board of Commissioners Advertised Public Hearing Date: 11/12/2024

Applicant: Havenn Homes, LLC
1100 Circle 75 Parkway, Suite 270
Atlanta, GA 30339

Owner: Gregory Peevy
206 Smoke Rise Drive
Warner Robins, GA 31088

Contact: Gabrielle Schaller

Contact Phone: 770.232.0000

Zoning History

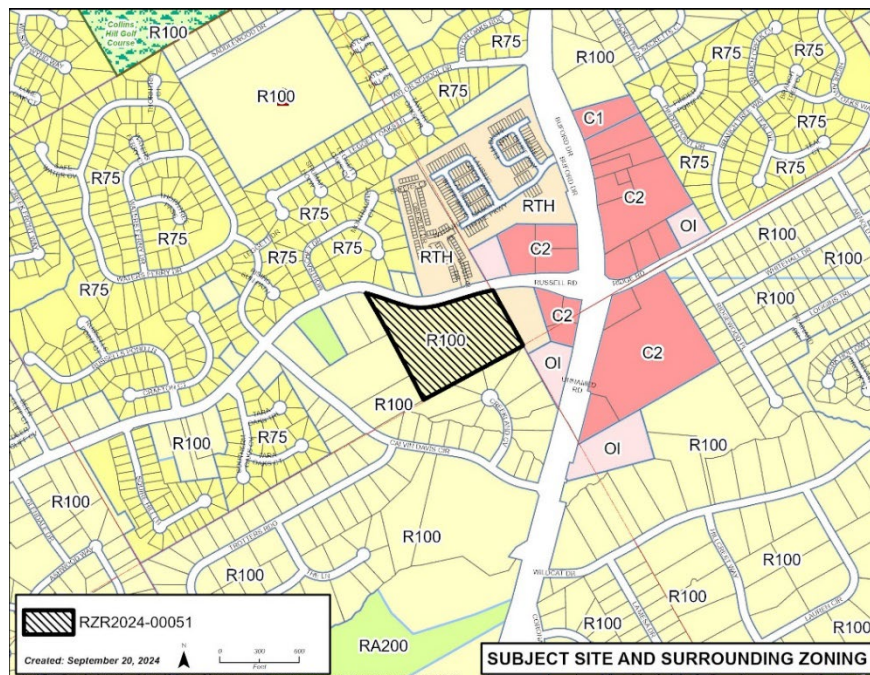
The subject property is zoned R-100 (Single-Family Residence District). No prior zoning cases are on record for this property.

Existing Site Condition

The subject property is a 12.15-acre parcel located along Russell Road, west of its intersection with Buford Drive. The property contains a single-family detached residence. The land is wooded and falls approximately 30 feet from the street to a stream running along the eastern side of the property. There is a sidewalk along the Russell Road frontage. The nearest Gwinnett County Transit stop is approximately 2.4 miles from the subject property.

Surrounding Use and Zoning

The subject property is surrounded by single-family detached subdivisions, townhouses and commercial uses. Waverly Village, a townhouse community, is located to the north across Russell Road. An undeveloped property is located to the east. Further east are commercial properties at the intersection of Buford Drive and Russell Road. Single-family residences on large lots and in subdivisions are located to the north, south, and west of the property. A child day care center is located across Russell Road beside the Waverly Village townhouses. The following is a summary of surrounding uses and zoning:



| Location | Land Use | Zoning | Density |
|----------|--|-----------------------|---|
| Proposed | Townhouses and Single-Family Residential | R-IF | 8.64 units per acre |
| North | Townhouses Single-Family Residential Single-Family Residential | R-TH R-100 R-75 | 7.99 units per acre 1.56 units per acre 2.17 units per acre |
| East | Undeveloped | R-TH | 6.25 units per acre (approved) |
| South | Single-Family Residential | R-100 | 1.67 units per acre |
| West | Single-Family Residential | R-100 | 0.77 units per acre |

Project Summary

The applicant requests rezoning of a 12.15-acre property from R-100 to R-IF for a townhouse and single-family detached residential subdivision, including:

- A total of 105 units, comprised of 85 townhouses and 20 single-family detached residences, yielding a density of 8.64 units per acre.
- 50 two-story rear-loaded townhouses with one-car garages in the center of the development.
- 25 three-story rear-loaded townhouses with two-car garages fronting Russell Road.
- 10 two-story front-loaded townhouses with one-car garages along the eastern side of the development.
- All townhouses with a minimum heated floor area of 1,350 square feet.
- 20 single-family detached residences with a minimum heated floor area of 1,500 square feet and two-car, front-loaded garages along the southern and western property lines.
- Exterior building materials of brick and cementitious siding and board and batten.
- 3.30 acres, or 27.1% of total open space dispersed throughout the site.
- 0.75 acres, or 6.2% of usable open space including a centralized green space with a firepit and a dog park, located in the northwest portion of the site.
- Guest parking comprised of eight parallel on-street parking spaces and a parking lot with 14 spaces located next to the mail kiosk.
- A stormwater management facility in the southeastern corner of the site.
- 27-foot-wide public internal streets with 4-foot-wide sidewalks on both sides and in front of the townhouses fronting Russell Road, providing connection to the existing 5-foot-wide external sidewalk.
- A full-access entrance from Russell Road with a left-turn lane and deceleration lane into the site.
- A 20-foot-wide buffer along the western and southern property lines, abutting single-family detached residences.
- A sewer connection to the east within a 40-foot-wide easement.

Zoning and Development Standards

The applicant is requesting a rezoning to R-IF for a townhouse and single-family detached residential subdivision. The following is a summary of applicable development standards from the Unified Development Ordinance (UDO):

| Standard | Required | Proposed | Meets Standard? |
|---|--|------------------------------------|-----------------|
| Front Yard Setback | 10' | >10' | YES |
| Rear Yard Setback | Minimum 20' | >20' | YES |
| Side Yard Setback | Minimum 7.5' | 7.5' | YES |
| Garage Door Setback | Minimum 18' | 18' | YES |
| Street Frontage Setback* | Minimum 40' setback 10' landscape strip | 40' setback 10' landscape strip | YES |
| Building Height | Maximum 45' | <40' | YES |
| Open Space | Minimum 10% or 1.22 acres | 27.1% or 3.30 acres | YES |
| Usable Open Space | Minimum 5% or 0.61 acres | 6.2% or 0.75 acres | YES |
| Single-Family Detached Dwelling Parking | Minimum 2 spaces per dwelling Maximum 4 spaces per dwelling | 2-4 spaces per dwelling | YES |
| Townhouse Parking | Minimum 2 spaces per dwelling Maximum 3 spaces per dwelling | 2-3 per dwelling | YES |
| Townhouse Guest Parking | 22 spaces | 22 spaces | YES |

*Required when the sides or rears of residences are oriented toward an external right-of-way.

Public Participation

The applicant held a community meeting on September 26, 2024 at the Rhodes Jordan Park community recreation center in Lawrenceville. There were five community members in attendance, who raised a wide range of questions and concerns, including housing products, buffers, traffic and environmental impact. The public participation plan and report are shown in Exhibit H.

Internal and External Agency Review

In addition to these Development Standards, the applicant must meet all other UDO requirements related to infrastructure improvements. Internal and External agency review comments are attached (Exhibit I). Standard site and infrastructure improvements will also be required related to stormwater, transportation, water, and sewer utilities. Recommended improvements not already required by the UDO have been added as staff recommended conditions.

Staff Analysis

Rezoning Request Analysis: According to the UDO, if a proposed amendment is for the rezoning of property and involves a change in zoning classification, the Department shall evaluate the request and make a recommendation with respect to the standards governing exercise of zoning power as defined in Section 270-20.6. After this evaluation, staff makes the following findings based on the standards from the UDO:

A. Whether a proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

The proposed rezoning would allow a subdivision with single-family detached residences and townhouses. Both land uses are found in the immediate vicinity of the subject property. The density of the proposed subdivision is comparable to that of the Waverly Village townhouse community located directly across Russell Road. The proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

B. Whether a proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property.

The proposed rezoning would allow housing types that are already found in the immediate vicinity of the subject property. The subdivision has been designed to locate single-family detached residences against existing single-family detached residences. In addition, a non-required 20-foot-wide buffer has been proposed along the western and southern property lines to mitigate any potential impacts. The proposed rezoning will not adversely affect the existing use or usability of adjacent or nearby property.

C. Whether the property to be affected by a proposed rezoning has a reasonable economic use as currently zoned.

The property has a reasonable economic use as currently zoned.

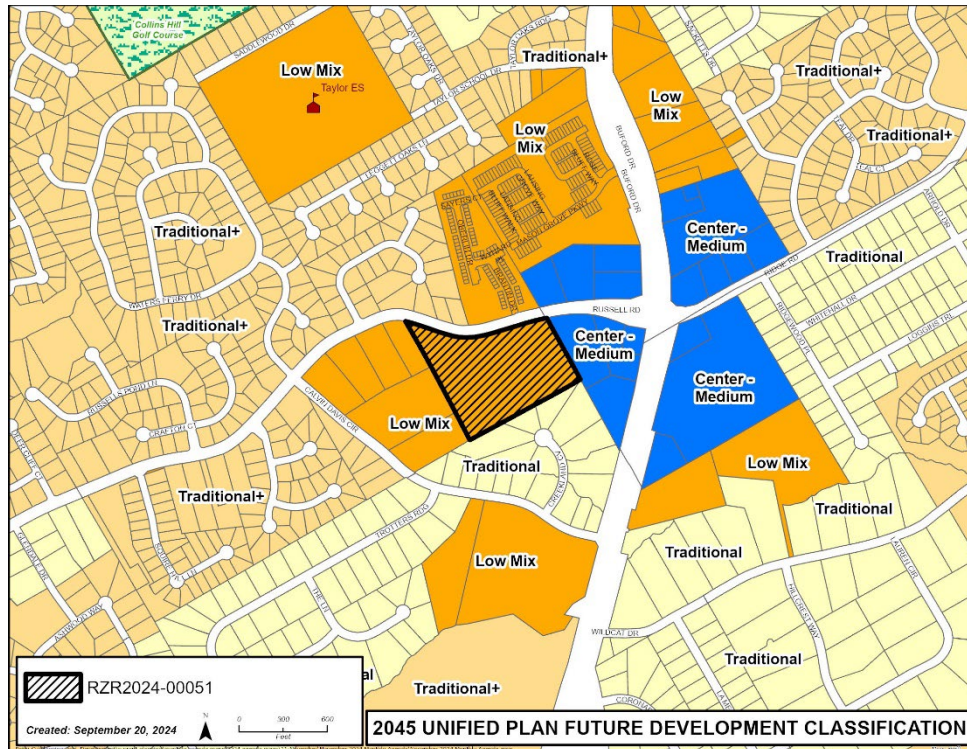
D. Whether the proposed rezoning will result in a use which will or could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools.

An increase in impacts on public facilities would be anticipated in the form of traffic, utility demand, and stormwater runoff; however, these impacts would be mitigated with appropriate conditions, site development requirements, and planning. The traffic impact study estimated that the development would generate 53 morning and 63 evening peak hour trips and that the level of service at surrounding intersections would remain mostly unchanged from no-build conditions. The study recommended a left-turn lane and deceleration lane at the site entrance. An increased impact is anticipated on school enrollment. Agency review comments related to any potential improvements concerning this rezoning request are attached (Exhibit I).

E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map; and

The Unified Plan's Future Development Map indicates "Neighborhood Low Mix" for the subject property, which provides for missing middle housing types in predominantly residential areas. This type supports a diversity of housing types and greater affordability than entirely single-family residential areas. Multimodal goals include reduced block sizes, improved street connectivity, and close proximity to a county transit stop. Residences with side or rear-loaded garages are also appropriate for this type.

The proposed rezoning would introduce a primarily rear-loaded townhouse community supplemented with single-family residences that are comparable in size to existing homes in the surrounding area. Although "Neighborhood Low Mix" intends to introduce only missing middle housing types, the proposed single-family detached dwellings located along the perimeter of the site would provide a transition from the existing subdivisions with larger lots to the townhomes internal to the property. The site layout incorporates connected streets, sidewalks, walkable block sizes, and thoughtful usable open space as intended by the future development type. The proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map.



F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning.

There are no other conditions giving support for approval or disapproval of the proposal.

Staff Recommendation

Based on the staff's evaluation of the request and the standards governing the exercise of zoning power, the Department of Planning and Development recommends **APPROVAL WITH CONDITIONS** of the rezoning request.

Staff Recommended Conditions

Approval as R-IF for a townhouse and single-family detached residential subdivision subject to the following conditions:

1. The development shall be limited to townhouses and single-family detached dwellings and accessory uses and structures, not exceeding 105 units.
2. The proposed development shall be constructed in general conformance with Exhibit B: Site Plan dated received October 18, 2024, and Exhibit C: Elevations dated received August 29, 2024 and September 12, 2024, by the Department of Planning and Development, subject to the review and approval of the Department of Planning and Development.

3. The minimum heated floor area of each dwelling shall be 1,350 square feet for townhouses and 1,500 square feet for single-family detached residences.
4. Usable open space shall be provided including but not limited to, a centralized green space with a firepit, and a dog park, subject to the review and approval of the Department of Planning and Development.
5. A 20-foot-wide landscaped buffer with a six-foot-tall privacy fence shall be placed along the southern and western property lines, in general conformance with Exhibit B: Site Plan dated received September 12, 2024.
6. Building lots shall not be located within any stream buffer or their associated impervious setbacks.
7. The development entrance shall align with Brantin Drive, subject to the review and approval of the Gwinnett County Department of Transportation.
8. A deceleration lane and a left-turn lane shall be provided along Russell Road, subject to the review and approval of the Gwinnett County Department of Transportation.
9. Stormwater BMP facilities that are not designed as an amenity shall be screened from view of adjoining properties and rights-of-way by landscaping and/or decorative fencing that is in compliance with the Gwinnett County Stormwater Management Manual, subject to review and approval by the Department of Planning and Development.
10. Natural vegetation shall remain on the property until the issuance of a development permit.
11. All grassed areas within lots and usable open space shall be sodded.

Exhibits:

- A. Site Visit Photos
- B. Site Plan
- C. Building Elevations
- D. Letter of Intent and Applicant's Response to Standards
- E. Application and Disclosure of Campaign Contributions
- F. DWR Sewer Capacity Certification Letter
- G. Traffic Impact Study
- H. Public Participation Plan and Report
- I. Internal and External Agency Review Comments
- J. Maps
- K. Fence Exhibit Presented at the November 4, 2024 Planning Commission Public Hearing

Exhibit A: Site Visit Photos



View of existing residence on the site



View of site frontage along Russell Road



View of Russell Road towards Buford Drive



View of Waverly Village townhouses across Russell Road from the site

Exhibit B: Site Plan

[attached]

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Exhibit C: Building Elevations

[attached]

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ELEVATION C



ELEVATION D



ELEVATION E

Fathom

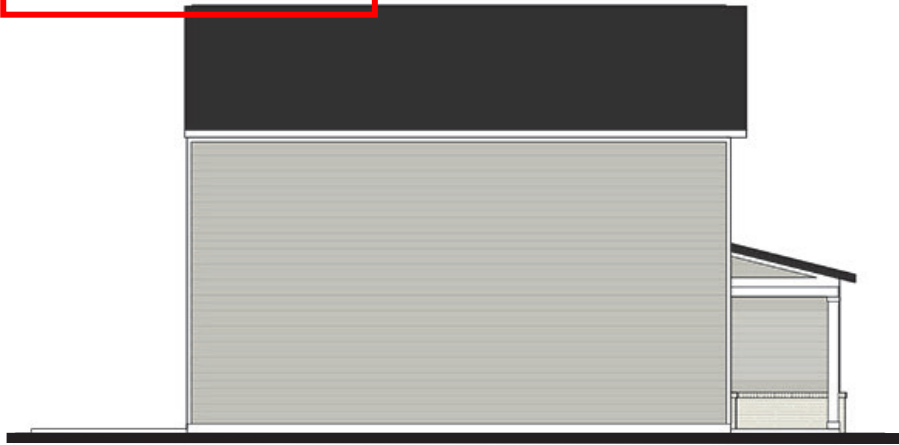
Front Elevations



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2 LEFT ELEVATION "D"
SCALE 1/8"=1'-0"



1 FRONT ELEVATION "D"
SCALE 1/8"=1'-0"



4 RIGHT ELEVATION "D"
SCALE 1/8"=1'-0"



2 REAR ELEVATION "D"
SCALE 1/8"=1'-0"

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Examination and approval of this set of drawings is hereby certified to be in accordance with the provisions of the Georgia Professional Code of Ethics, Chapter 17, Article 1, Section 17-17-1.

| NO. | DATE | ISSUES & REVISIONS |
|-----|-----------|--------------------|
| 1 | 8/29/2024 | PERMIT SET |

PROJECT:
PILOT
29'-9" WIDE SINGLE FAMILY

CONTENTS:
ELEVATION "D"

DATE: PROJ. NO:

SHEET NO:

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1 RUSSELL ROAD DOGWOOD BUILDING - FRONT ELEVATION
SCALE 1/4"=1'-0" 3 UNIT BUILDING



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RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING

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SCALE 1/4"=1'-0" 3 UNIT BUILDING



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SUITE 270
ATLANTA, GA 30339

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DOGWOOD
ELEVATION A

1 RUSSELL ROAD DOGWOOD BUILDING - LEFT ELEVATION
SCALE 3/8"=1'-0" 3 UNIT BUILDING



DOGWOOD
ELEVATION D

2 RUSSELL ROAD DOGWOOD BUILDING - RIGHT ELEVATION
SCALE 3/8"=1'-0" 3 UNIT BUILDING



1100 CIRCLE 75 PARKWAY
SUITE 270
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RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING

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1 RUSSELL ROAD 18' TOWNHOME BUILDING - FRONT ELEVATION
SCALE 1/4"=1'-0"
5 UNIT BUILDING



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18' WIDE TOWNHOME
5 UNIT TOWNHOME BUILDING

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1 RUSSELL ROAD 18' TOWNHOME BUILDING - REAR ELEVATION
SCALE 1/4"=1'-0"
5 UNIT BUILDING



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18' WIDE TOWNHOME
5 UNIT TOWNHOME BUILDING

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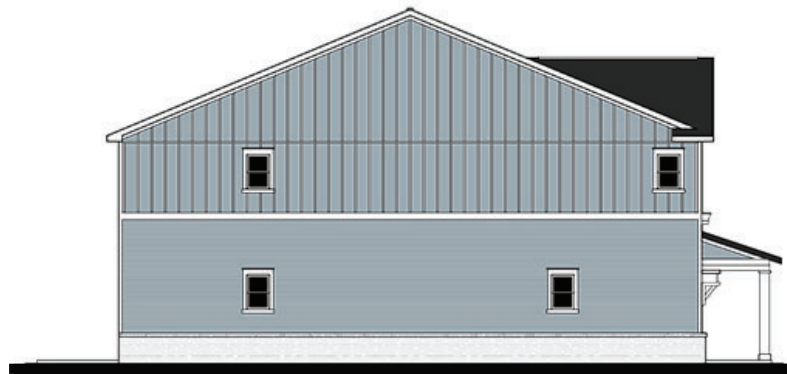
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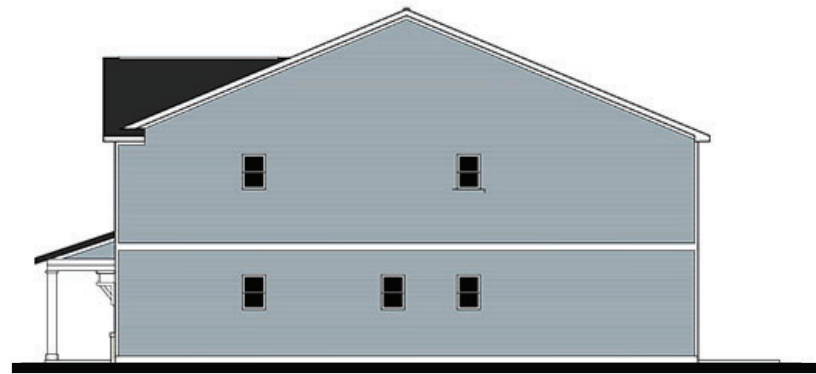
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SCALE 3/8"=1'-0"



2 RIGHT ELEVATION "E"
SCALE 3/8"=1'-0"



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5 UNIT TOWNHOME BUILDING

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1 RUSSELL ROAD VANDERBILT BUILDING - FRONT ELEVATION
SCALE 1/4"=1'-0"
5 UNIT BUILDING



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VANDERBILT
5 UNIT TOWNHOME BUILDING

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UNIT #5
ELEVATION 'C'

UNIT #4
ELEVATION 'E'

UNIT #3
ELEVATION 'D'

UNIT #2
ELEVATION 'B'

UNIT #1
ELEVATION 'A'

1 RUSSELL ROAD VANDERBILT BUILDING - REAR ELEVATION
SCALE 1/4"=1'-0"
5 UNIT BUILDING



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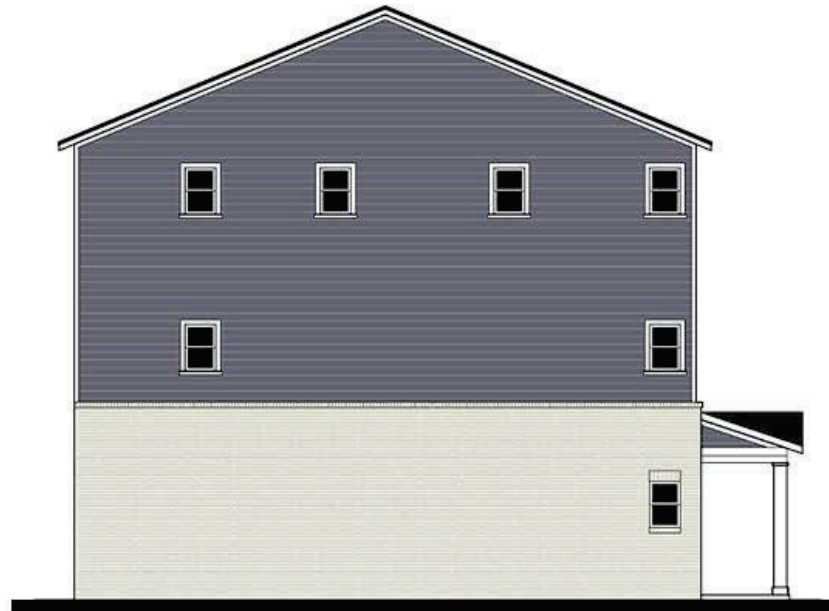
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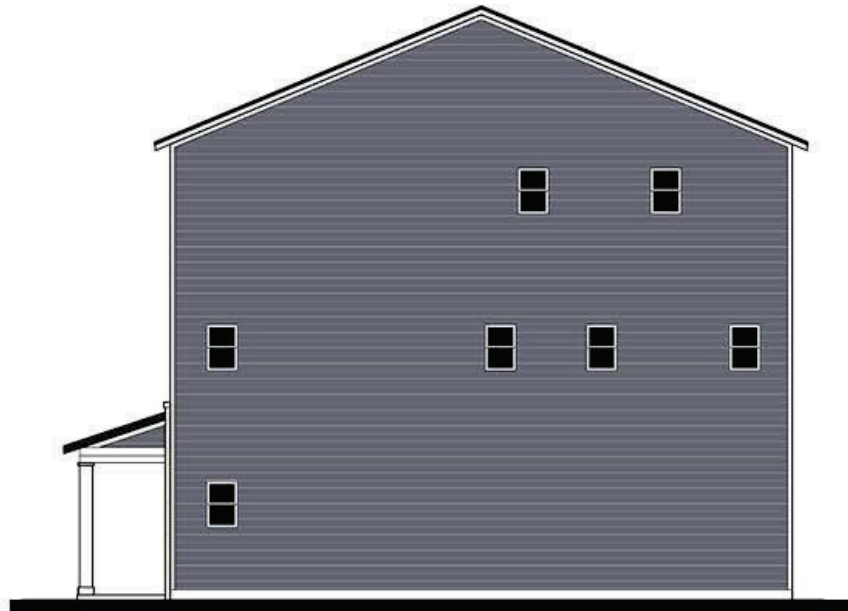
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UNIT #1
ELEVATION "A"

1 LEFT ELEVATION "A"
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UNIT #5
ELEVATION "C"

2 RIGHT ELEVATION "C"
SCALE 1/4"=1'-0"



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VANDERBILT
5 UNIT TOWNHOME BUILDING

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PILOT
29'-9" WIDE SINGLE FAMILY

CONTENTS:

ELEVATION "E"

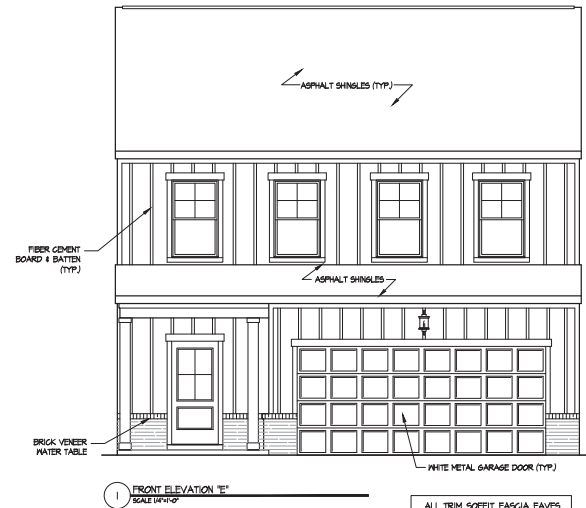
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





ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL



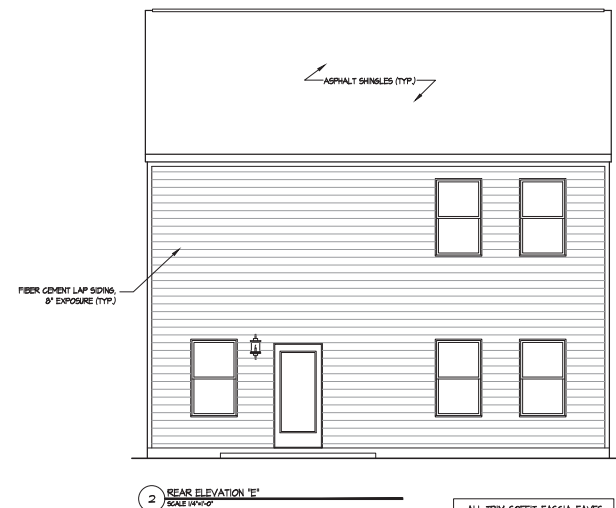
ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL

MATERIAL HATCHING TABLE

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|  | -FIBER CEMENT LAP SIDING 8' EXP. TYP. |
|  | -FIBER CEMENT BOARD AND BATTEN |
|  | -ALL TRIM, SOFFITS, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL. |
| | -ALL ROOFING TO BE ASPHALT SHINGLE |



ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL

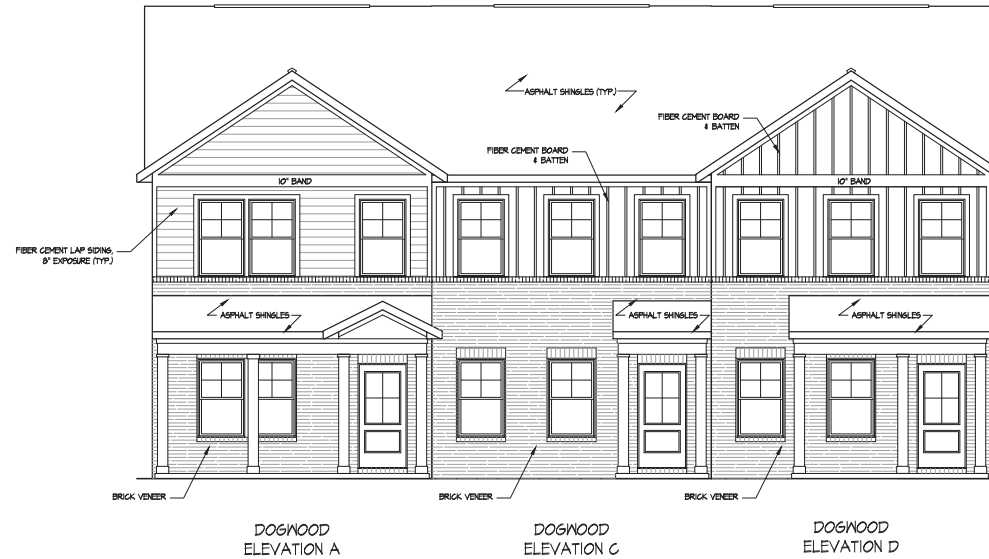


ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL

RECEIVED

9.12.2024

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL.



1 RUSSELL ROAD DOGWOOD BUILDING - FRONT ELEVATION
SCALE 1/4"=1'-0"
3 UNIT BUILDING

| MATERIAL HATCHING TABLE | |
|-------------------------|---|
| | FIBER CEMENT LAP SIDING 8" EXPOSURE (TYP.) |
| | FIBER CEMENT LAP SIDING 4" EXPOSURE (TYP.) |
| | FIBER CEMENT BOARD AND BATTEN |
| | ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL. |
| | ALL ROOFING TO BE ASPHALT SHINGLES |



1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

PRINT RECORD

DATE DESCRIPTION

| | | |
|---|----------|--------|
| 1 | 08.20.24 | PERMIT |
| 2 | | |
| 3 | | |
| 4 | | |

RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING

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SEAL

RELEASE DATE

August 20, 2024

SCALE: 1/8"=1'-0" ON 11x17 SHEET

DRAWING TITLE

BUILDING
ELEVATIONS

Drawn By

Checked By

Project#

A-201

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1 RUSSELL ROAD DOGWOOD BUILDING - REAR ELEVATION
SCALE 1/4"=1'-0"
3 UNIT BUILDING

| MATERIAL HATCHING TABLE | |
|-------------------------|--|
| | FIBER CEMENT LAP SIDING 8\"/> |
| | FIBER CEMENT LAP SIDING 8\"/> |
| | FIBER CEMENT BOARD AND BATTEN |
| | ALL TRIM, SUPPORT FASCIA BOARDS, COLUMNS TO BE FINISHED FIBER CEMENT BOARD |
| | ALL ROOFTOP TO BE ASPHALT SHINGLES |



HAVENN
HOMES
1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

PRINT RECORD

DATE DESCRIPTION

| | | |
|---|----------|--------|
| 1 | 08.20.24 | PERMIT |
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| 3 | | |
| 4 | | |

RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING

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SCALE: 1/8"=1'-0" ON 11x17 SHEET

DRAWING TITLE

BUILDING
ELEVATIONS

Drawn By

Checked By

Project#

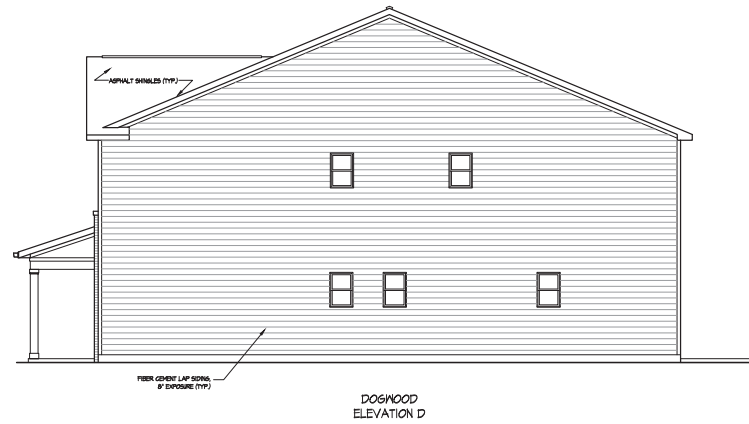
A-202

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1 RUSSELL ROAD DOGWOOD BUILDING - LEFT ELEVATION
SCALE 3/16"=1'-0" 3 UNIT BUILDING



2 RUSSELL ROAD DOGWOOD BUILDING - RIGHT ELEVATION
SCALE 3/16"=1'-0" 3 UNIT BUILDING

| MATERIAL HATCHING TABLE | |
|-------------------------|--|
| | FIBER CEMENT LAP SIDING 8" EXPOSURE (TYP) |
| | FIBER CEMENT LAP SIDING 8" EXPOSURE (TYP) |
| | FIBER CEMENT BOARD AND BATTEN |
| | ALL TEXT SHOULD BE PLACED OUTSIDE OF THE DRAWING AREA TO BE PRINTED FROM THE SHEET OVERLAP |
| | ALL ROOFING TO BE ASPHALT SHINGLES |



1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

PRINT RECORD

DATE DESCRIPTION

| | | |
|---|----------|--------|
| 1 | 08.20.24 | PERMIT |
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| 3 | | |
| 4 | | |

RUSSELL ROAD
DOGWOOD
3 UNIT TOWNHOME BUILDING

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SCALE: 1/8"=1'-0" ON 11x17 SHEET

DRAWING TITLE

BUILDING
ELEVATIONS

Drawn By

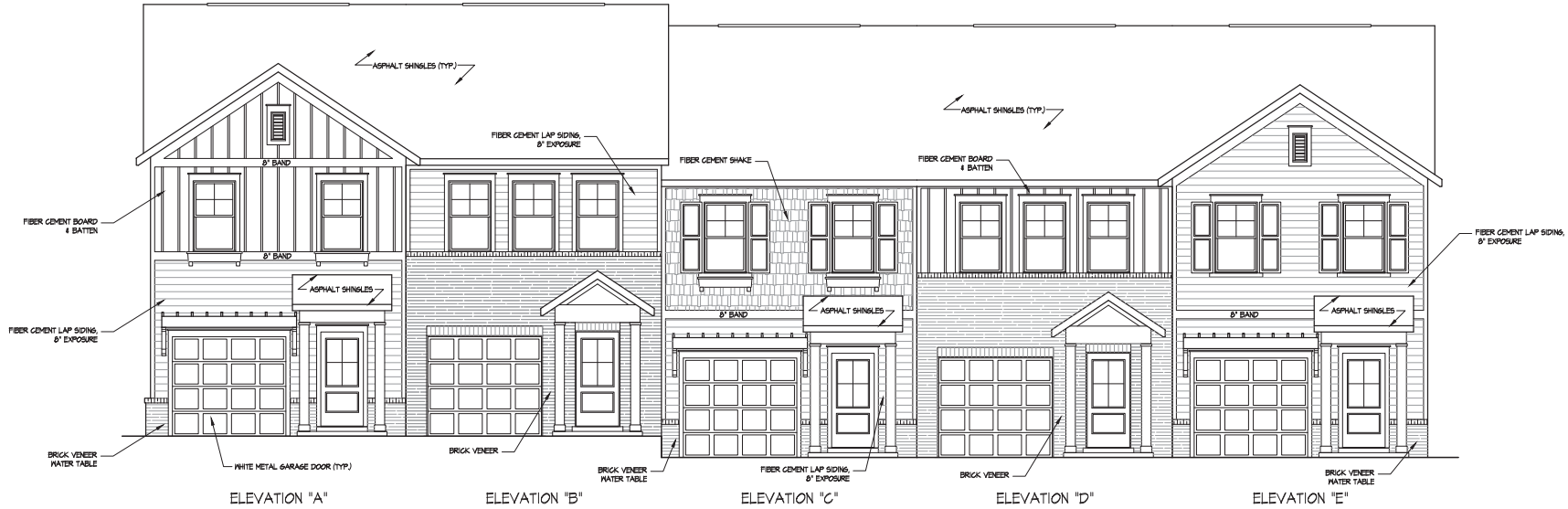
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1 RUSSELL ROAD 18' TOWNHOME BUILDING - FRONT ELEVATION
SCALE 1/4"=1'-0"
5 UNIT BUILDING

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMS TO BE PAINTED FIBER
CEMENT MATERIAL

| MATERIAL HATCHING TABLE | |
|-------------------------|--------------------------------------|
| | FIBER CEMENT LAP SIDING 8" EXP. TYP. |
| | FIBER CEMENT BOARD 4 BATTEN |
| | BRICK VENEER |
| | ASPHALT SHINGLES (TYP.) |



1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

PRINT RECORD

DATE DESCRIPTION

| | | |
|---|----------|--------|
| 1 | 08.20.24 | PERMIT |
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| 3 | | |
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RUSSELL ROAD
18' WIDE TOWNHOME
5 UNIT TOWNHOME BUILDING

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SCALE: 1/8"=1'-0" ON 11x17 SHEET

DRAWING TITLE

BUILDING
ELEVATIONS

Drawn By





Checked By

Project#

A-201

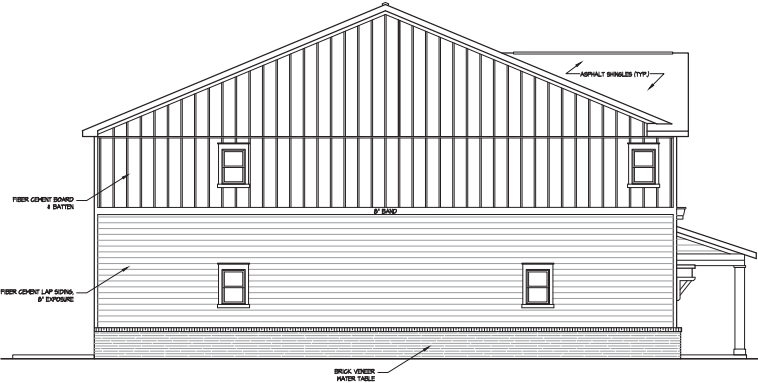


MATERIAL HATCHING TABLE

| | |
|---|---|
|  | -FIBER CEMENT LAP SIDING 8' EXP. TYP. |
|  | -FIBER CEMENT LAP SIDING 5' EXP. TYP. |
|  | -FIBER CEMENT BOARD AND BATTEN |
|  | -ALL TRIM SOFFIT, FRIEZE, SAVER, COLUMNS PAINTED FIBER CEMENT MATERIAL |
| | -ALL ROOFING TO BE ASPHALT/SHINGLE |

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1 LEFT ELEVATION "A"
SCALE 3/16"=1'-0"



2 RIGHT ELEVATION "E"
SCALE 3/16"=1'-0"

| MATERIAL HATCHING TABLE | |
|-------------------------|--|
| | FIBER CEMENT LAP SIDING 8' EXP. TYP. |
| | FIBER CEMENT LAP SIDING 8' EXP. TYP. |
| | FIBER CEMENT BOARD AND SHINGLES |
| | ALL TEN SPORTS FACILITY BRICK VENEER TO BE PAINTED FIBER CEMENT MATERIAL |
| | ALL ROOFING TO BE ASPHALT SHINGLES |



PRINT RECORD

DATE DESCRIPTION

| | | |
|---|----------|--------|
| 1 | 08.20.24 | PERMIT |
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| 3 | | |
| 4 | | |

RUSSELL ROAD
18' WIDE TOWNHOME
5 UNIT TOWNHOME BUILDING

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BUILDING
ELEVATIONS

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MATERIAL HATCHING TABLE

| | |
|--|---|
| | -FIBER CEMENT LAP SIDING 8" EXP. TYP. |
| | -FIBER CEMENT LAP SIDING 9" EXP. TYP. |
| | -FIBER CEMENT BOARD AND BATTEN |
| | -ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL |
| | -ALL ROOFING TO BE ASPHALT SHINGLE |



1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

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| # | DATE | DESCRIPTION |
|---|----------|-------------|
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| 3 | | |
| 4 | | |

RUSSELL ROAD
VANDERBILT
5 UNIT TOWNHOME BUILDING

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RUSSELL ROAD VANDERBILT BUILDING - REAR ELEVATION
SCALE 1/4"=1'-0"
5 UNIT BUILDING

MATERIAL HATCHING TABLE

| | |
|--|---|
| | -FIBER CEMENT LAP SIDING 8" EXP. TYP. |
| | -FIBER CEMENT LAP SIDING 9" EXP. TYP. |
| | -FIBER CEMENT BOARD AND BATTEN |
| | -ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL |
| | -ALL ROOFING TO BE ASPHALT SHINGLE |



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HOMES**
1100 CIRCLE 75 PARKWAY
SUITE 270
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| # | DATE | DESCRIPTION |
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| 1 | 08.20.24 | PERMIT |
| 2 | | |
| 3 | | |
| 4 | | |

RUSSELL ROAD
VANDERBILT
5 UNIT TOWNHOME BUILDING

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SCALE: 1/8"=1'-0" ON 11x17 SHEET

DRAWING TITLE

**BUILDING
ELEVATIONS**

Drawn By

Checked By

Project#

A-202

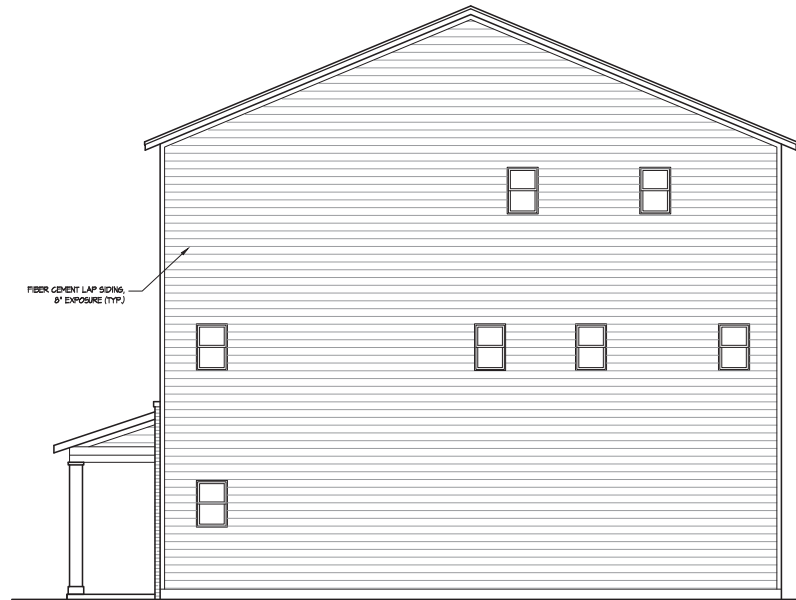
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UNIT #1
ELEVATION "A"

1 LEFT ELEVATION "A"
SCALE 1/4"=1'-0"



UNIT #5
ELEVATION "C"

2 RIGHT ELEVATION "C"
SCALE 1/4"=1'-0"

MATERIAL HATCHING TABLE

| | |
|------------------------------------|--|
| | -FIBER CEMENT LAP SIDING 8" EXP. TYP. |
| | -FIBER CEMENT LAP SIDING 9" EXP. TYP. |
| | -FIBER CEMENT BOARD AND BATTEN |
| | -ALL TRIM, SOFFIT, FASCIA, EAVES, GABLES TO BE PAINTED FIBER CEMENT MATERIAL |
| -ALL ROOFING TO BE ASPHALT SHINGLE | |



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HOMES**
1100 CIRCLE 75 PARKWAY
SUITE 270
ATLANTA, GA 30339

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| # | DATE | DESCRIPTION |
|---|----------|-------------|
| 1 | 08.20.24 | PERMIT |
| 2 | | |
| 3 | | |
| 4 | | |

RUSSELL ROAD
VANDERBILT
5 UNIT TOWNHOME BUILDING

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SEAL

RELEASE DATE

August 20, 2024

SCALE: 1/8"=1'-0" ON 11x17 SHEET

DRAWING TITLE

BUILDING
ELEVATIONS

Drawn By


Checked By

Project#

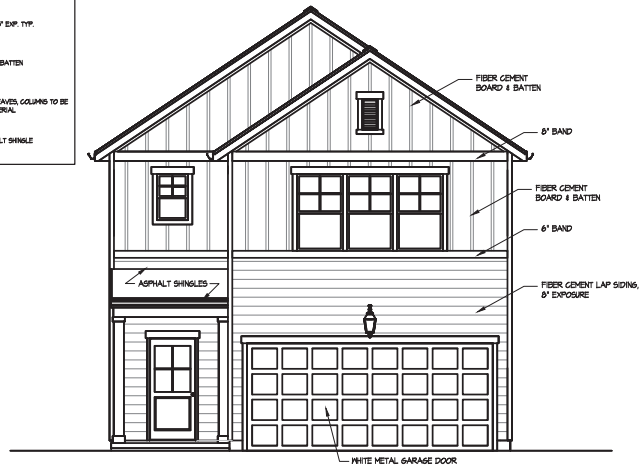
A-203

9.12.2024

MATERIAL HATCHING TABLE

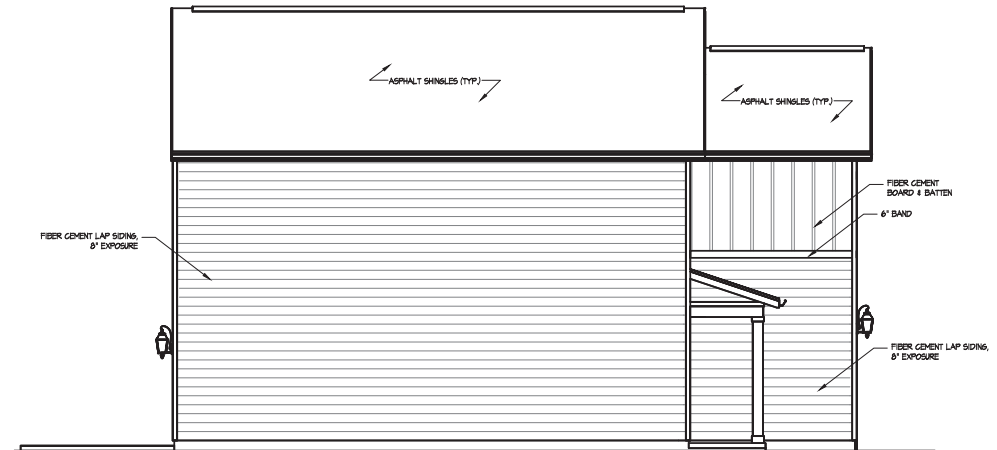


- FIBER CEMENT LAP SIDING 8" EXP. TYP.
- FIBER CEMENT LAP SIDING 8" EXP. TYP.
- FIBER CEMENT BOARD AND BATTEN
- ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL
- ALL ROOFING TO BE ASPHALT SHINGLE



1 FRONT ELEVATION "C"
SCALE 1/4"=1'-0"

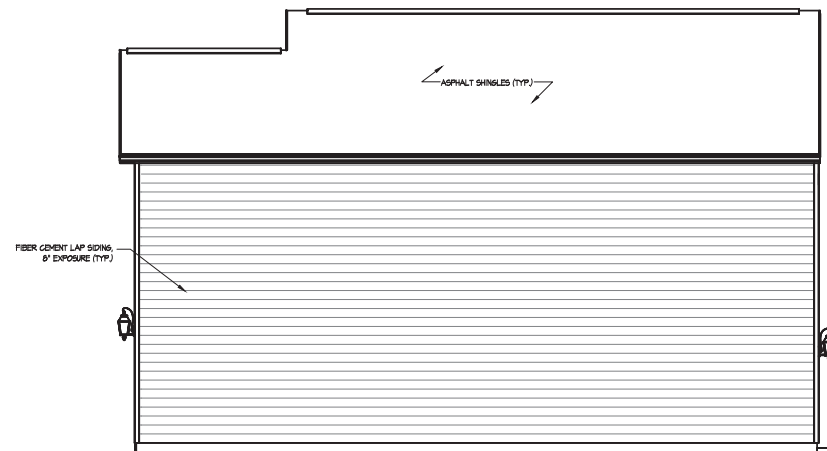
ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL



2 LEFT ELEVATION "C"
SCALE 1/4"=1'-0"



3 REAR ELEVATION "C"
SCALE 1/4"=1'-0"



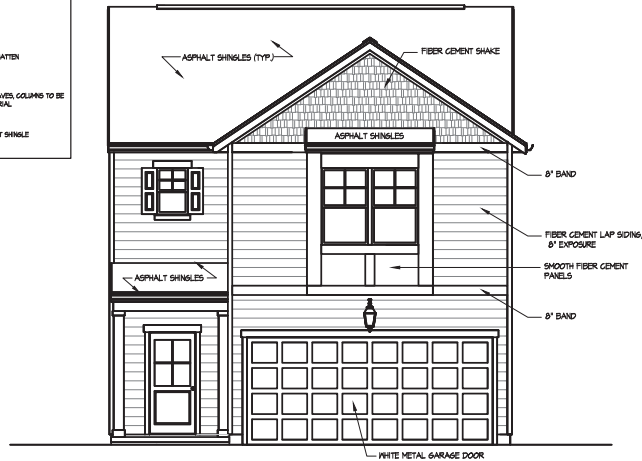
4 RIGHT ELEVATION "C"
SCALE 1/4"=1'-0"

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9.12.2024

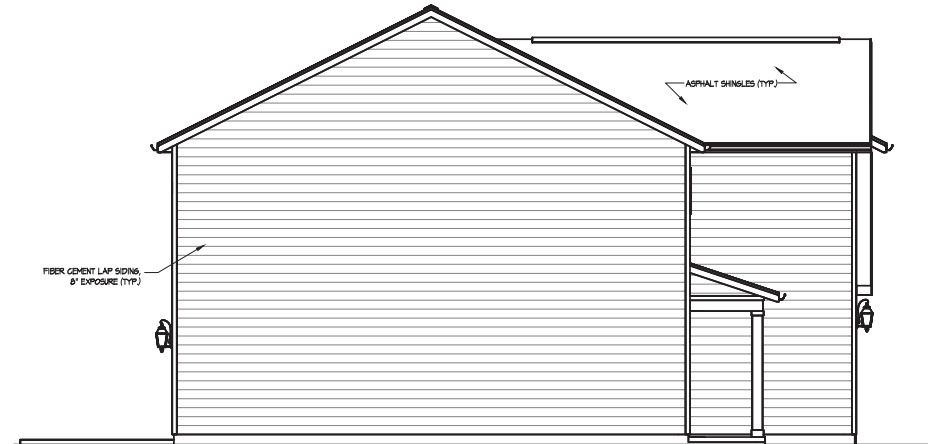
MATERIAL HATCHING TABLE

| | |
|--|---|
| | -FIBER CEMENT LAP SIDING 8' EXP. TYP. |
| | -FIBER CEMENT SHAKE |
| | -FIBER CEMENT BOARD AND BATTEN |
| | -ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL |
| | -ALL ROOFING TO BE ASPHALT SHINGLE |



1 FRONT ELEVATION "D"
SCALE 1/8"=1'-0"

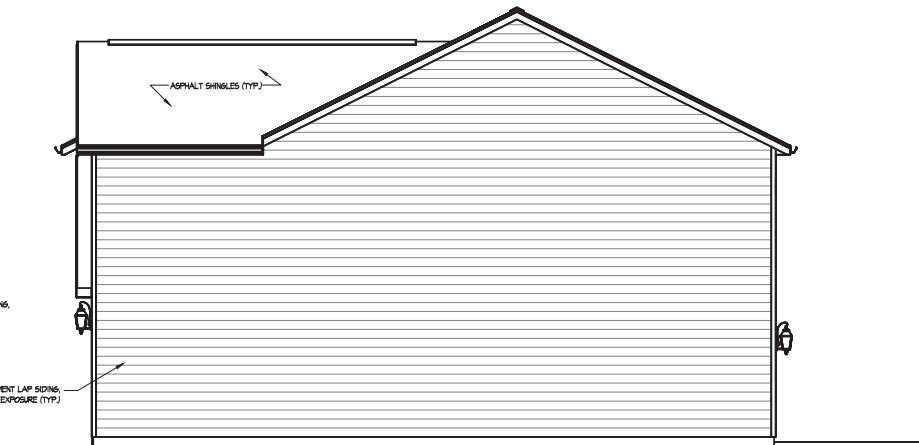
ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL



2 LEFT ELEVATION "D"
SCALE 1/8"=1'-0"



3 REAR ELEVATION "D"
SCALE 1/8"=1'-0"



4 RIGHT ELEVATION "D"
SCALE 1/8"=1'-0"

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DESIGN GROUP, LLC
Architecture/Planning
875 Peachtree City and Tucker, Georgia
678-461-5333
www.ah2designgroup.com



SEAL:

8/12/2024
1100 CIRCLE TO COMMUNITY
SUITE 270
ATLANTA, GA 30320

| NO. | ISSUES & REVISIONS | DATE | PREPARED BY |
|-----|--------------------|-----------|-------------|
| 1 | | 8/12/2024 | |
| 2 | | | |
| 3 | | | |
| 4 | | | |
| 5 | | | |
| 6 | | | |
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| 8 | | | |
| 9 | | | |
| 10 | | | |

PROJECT:
FATHOM ELEVATION "D"
26' WIDE SINGLE FAMILY

CONTENTS:
ELEVATION "D"

DATE: PROJ. NO.

SHEET NO:

A-201

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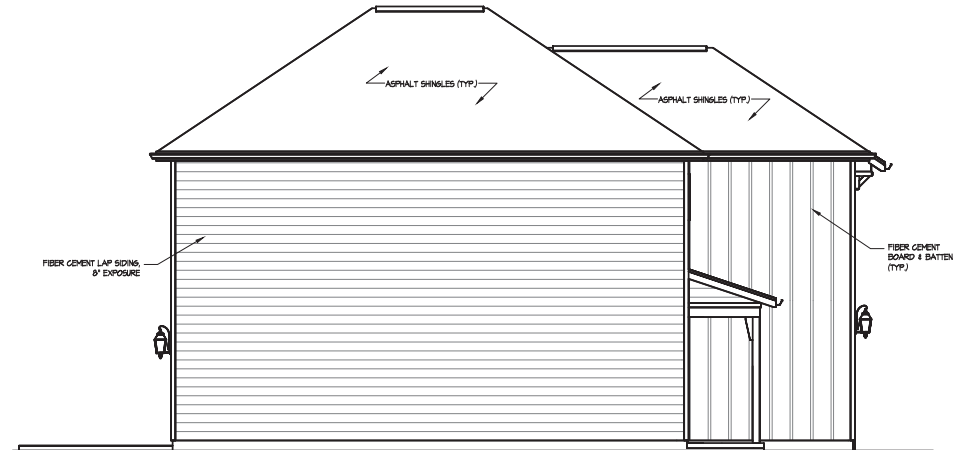
MATERIAL HATCHING TABLE

| | |
|--|---|
| | -FIBER CEMENT LAP SIDING 8' EXP. TYP. |
| | -FIBER CEMENT LAP SIDING 8' EXP. TYP. |
| | -FIBER CEMENT BOARD AND BATTEN |
| | -ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL |
| | -ALL ROOFING TO BE ASPHALT SHINGLE |



1 FRONT ELEVATION "E"
SCALE 1/8"=1'-0"

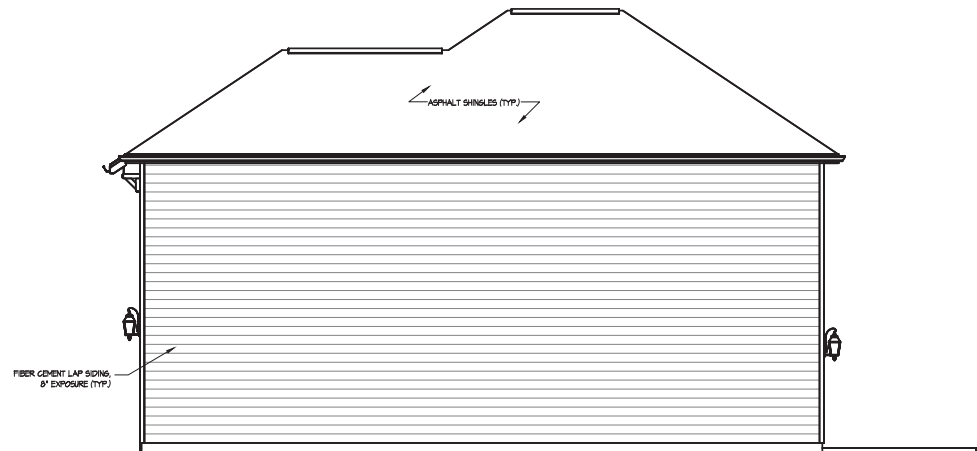
ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL



2 LEFT ELEVATION "E"
SCALE 1/8"=1'-0"



3 REAR ELEVATION "E"
SCALE 1/8"=1'-0"



4 RIGHT ELEVATION "E"
SCALE 1/8"=1'-0"

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Architecture/Planning
Buckhead City and Taylor, Georgia
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RECORDING, OR BY ANY INFORMATION
SYSTEM.

| NO. | ISSUES & REVISIONS | DATE |
|-----|--------------------|----------|
| 1 | PERMIT SET | 8/1/2024 |

PROJECT:
FATHOM ELEVATION "E"
26' WIDE SINGLE FAMILY

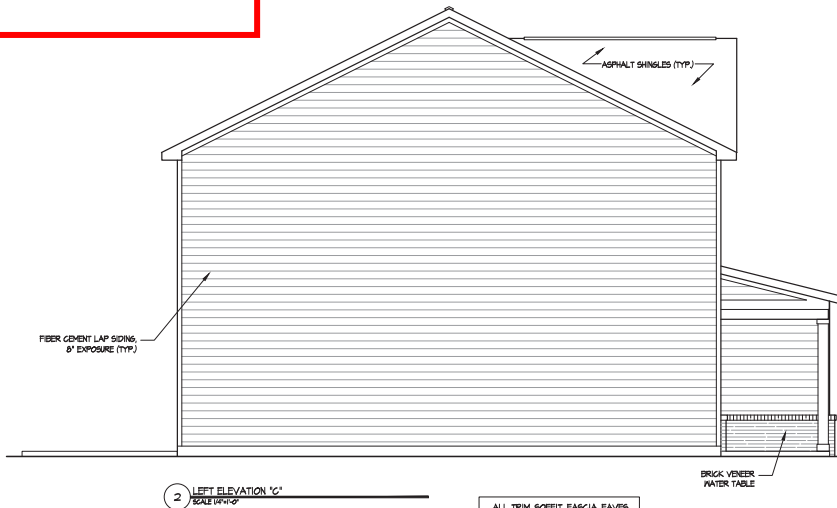
CONTENTS:
ELEVATION "E"

DATE: PROJ. NO.

SHEET NO:

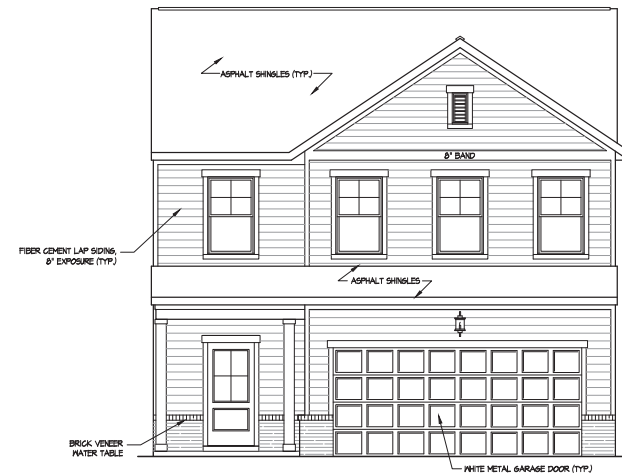
A-201

9.12.2024



2 LEFT ELEVATION "C"
SCALE 1/4"=1'-0"





ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL

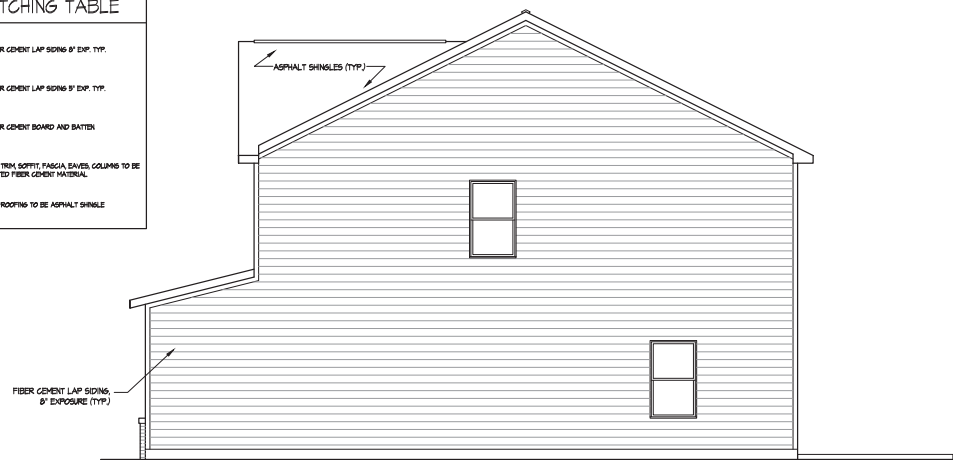


1 FRONT ELEVATION "C"
SCALE 1/4"=1'-0"

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL

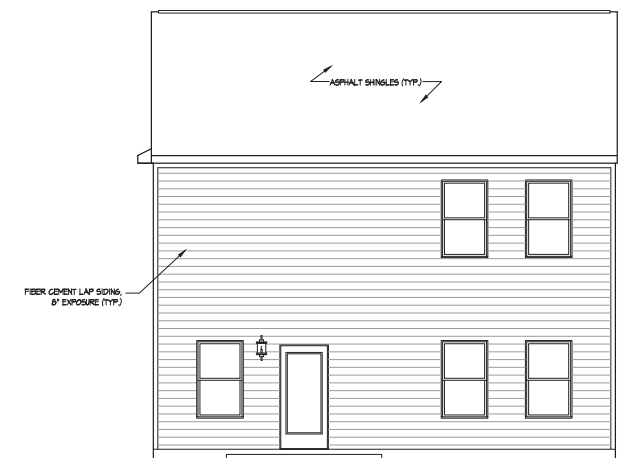
MATERIAL HATCHING TABLE

| | |
|--|---|
|  | -FIBER CONCRETE LAP SIDING 8" EXP. TYP. |
|  | -FIBER CONCRETE LAP SIDING 8" EXP. TYP. |
|  | -FIBER CONCRETE BOARD AND BATT |
|  | -ALL TRIM, GYPSUM, PLASTER, SLAT, COLUMNS TO BE PAINTED FIBER CONCRETE MATERIAL |
| | -ALL ROOFING TO BE ASPHALT SHINGLE |



4 RIGHT ELEVATION "C"
SCALE 1/4"=1'-0"

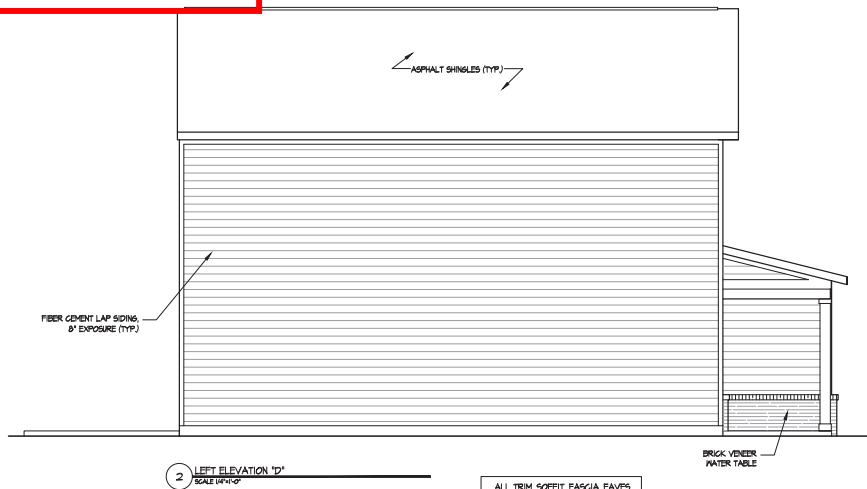
ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL



3 REAR ELEVATION "C"
SCALE 1/4"=1'-0"

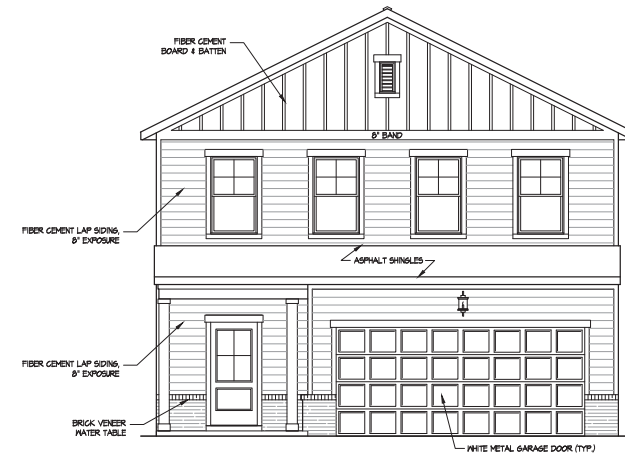
ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL

9.12.2024



2 LEFT ELEVATION 'D'
SCALE 1/4"=1'-0"





ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL

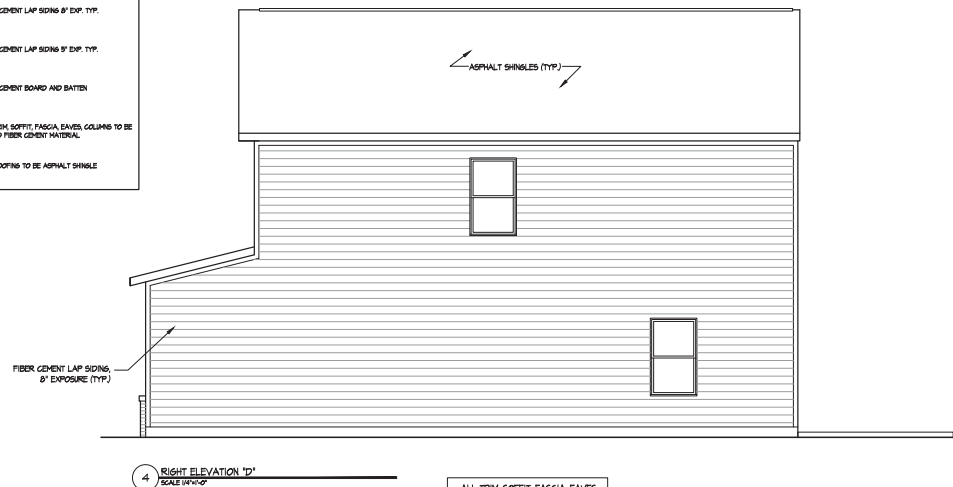


1 FRONT ELEVATION "D"
SCALE 1/4"=1'-0"

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL

MATERIAL HATCHING TABLE

| | |
|--|--|
|  | -FIBER CEMENT LAP SIDING BY EXP. TYP. |
|  | -FIBER CEMENT LAP SIDING BY EXP. TYP. |
|  | -FIBER CEMENT BOARD AND BATTEN |
|  | -ALL TRIM, SOFFIT, FASCIA, EAVES, COLUMNS TO BE PAINTED FIBER CEMENT MATERIAL. |
| | -ALL ROOFING TO BE ASPHALT SHINGLE |



4 RIGHT ELEVATION 'D'
SCALE 1/4"=1'-0"

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL



2 REAR ELEVATION 'D'
SCALE 1/4"=1'-0"

ALL TRIM, SOFFIT, FASCIA, EAVES,
COLUMNS TO BE PAINTED FIBER
CEMENT MATERIAL.

Exhibit D: Letter of Intent and Applicant's Response to Standards

[attached]

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Applicant's Letter of Intent

Havenn Homes LLC

Rezoning R-100 to RIF

The Applicant, Havenn Homes, LLC, requests to rezone a 12.154 acre parcel from R-100 to RIF (Residential Infill District) to construct a residential community with a townhome and single-family detached unit mix. The subject site is located on the south side of Russell Road and close to the west side of Buford Drive (GA-20).

The site is situated to the west of a commercial center at the intersection of Russell Road and Buford Drive (zoned C2 and OI), to the south of the Waverly Village Townhome development (zoned RTH), and to the east and north of single-family homes (zoned R100). The parcel contains a single-family home which will be demolished to make way for the proposed development.

As proposed, the community will offer a total of 105 units, resulting in a density of 8.61 units per acre. The Townhomes labeled "Autumn Leaf" on the site plan will feature front-entry garages facing internal streets. The homes labeled "Dogwood" will have rear-entry garages accessible via internal alleys; the front facade of these units will face internal streets. The "Vanderbilt" units will have rear-entry garages accessible by internal alleys, and the front façade of these units will face Russell Road. The single-family detached homes labeled "Pilot and Fathom" will have front entry garage to internal streets.

As shown on the submitted elevations, some Vanderbilt units fronting Russell Road will be 3 stories and will have exterior materials consisting of a mixture of brick and fiber cementitious siding. The remaining units will be 2 stories and will have a similar exterior composition of brick and cementitious siding. All units will feature a covered front porch and a patio.

The Vanderbilt and single-family detached units will include a two-car garage for private parking; the Dogwood and Autumn Leaf units will include a single-car garage. All will feature private driveways with additional parking space. Guest parking will be provided as parallel spaces along the interior streets. Additional site improvements will include usable open space in the form of linear parks, mews, planted areas, and sidewalks along the internal streets. Notable open space features include a dog park and gathering area with remnant fireplace.

Our plan includes a minimum 20-foot buffer along all property lines that border R100 zoning to provide space and noise reduction for adjoining single-family homes. Additionally, the single-family detached units of the proposed development will be adjacent to the neighboring R-100. As proposed the rezoning and development meets the intent of the 2045 Unified Plan. The site is located in the Ridgewood Village daily community, as part of a Low-Mix neighborhood. This neighborhood type encourages missing middle housing and easy access to the village center. Havenn Homes believes that the proposed development will serve as an ideal transition from the single-family homes in the neighboring Traditional and Traditional+ Future Development Categories to the Medium Village center. The proposed development will suitably

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9.12.2024

increase the density of the parcel in line with the Unified 2045 plan and provide much-needed additional housing to support Gwinnett County's growing population.

Havenn Homes looks forward to working with the Gwinnett County Planning Department and the local community to answer any questions and address any concerns. We are excited to provide highly desirable housing for the future.

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Standards Governing the Exercise of the Zoning Power

To further demonstrate that the proposed rezoning and land use is consistent with the intent of the UDO and 2045 Unified Plan, the applicant submits its response to the Standards Governing Exercise of the Zoning Power as follows:

(A) WHETHER A PROPOSED REZONING WILL PERMIT A USE THAT IS SUITABLE IN VIEW OF THE USE AND DEVELOPMENT OF ADJACENT AND NEARBY PROPERTY:

Rezoning the subject site to RIF for the purpose of constructing townhomes and single-family detached mixed development is suitable in relation to the adjacent and nearby properties. Adjacent properties are either raw land or residential use. Raw land is east of subject property and it has current zoning RTH. Residential property to south and west of property is zoned R100.

Other nearby uses include a large commercial retail center, offices, and churches.

The proposed development will add to the variety of housing in the area, and will act as a quality, low-intensity, infill use that complements the surrounding single-family uses. Further, this development will promote walkability as the area continues to attract new businesses and residents.

(B) WHETHER A PROPOSED REZONING WILL ADVERSELY AFFECT THE EXISTING USE OR USABILITY OF ADJACENT OR NEARBY PROPERTY:

The proposed rezoning will not adversely affect the use of the surrounding properties. Development standards will include buffers that will meet or exceed requirements of the category. Commercial uses nearby will benefit from new residents.

(C) WHETHER THE PROPERTY TO BE AFFECTED BY A PROPOSED REZONING HAS REASONABLE ECONOMIC USE AS CURRENTLY ZONED:

The highest and best economic use is significantly enhanced by the rezoning of the subject property. Current Zoning (R100) would allow approximately 8 single family detached lots with a net economic value (after development) of \$750,000. Property is currently under contract for more than that subject to re-zoning. The applicant submits the rezoning would allow for a use more compatible with the demand and pattern of development in the area.

(D) WHETHER THE PROPOSED REZONING WILL RESULT IN A USE WHICH WILL OR COULD CAUSE AN EXCESSIVE OR BURDENSOME USE OF EXISTING STREETS, TRANSPORTATION FACILITIES, UTILITIES, OR SCHOOLS:

The requested rezoning will not result in excessive or burdensome use of existing streets, transportation facilities, utilities, or schools. See traffic report included with this application.

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**(E) WHETHER THE PROPOSED REZONING IS IN CONFORMITY WITH THE POLICY AND INTENT
OF THE UNIFIED PLAN AND FUTURE DEVELOPMENT MAP:**

The proposed development meets the intent of the 2045 Unified Plan. The subject property is located within the Low Mix future development category, which encourages missing middle housing. The proposed development is designed to fill the need for additional housing types in the Ridgewood Daily Community, as well as to be walkable to the village center. Further, the use serves as a transition from less dense surrounding neighborhoods to the commercial uses of the Center Medium future development category.

**(F) WHETHER THERE ARE OTHER EXISTING OR CHANGING CONDITIONS AFFECTING THE USE
AND DEVELOPMENT OF THE PROPERTY WHICH GIVE SUPPORTING GROUNDS FOR EITHER
APPROVAL OR DISAPPROVAL OF THE PROPOSED REZONING:**

The pattern of residential infill development in the area and the proposed development's compatibility with the land use of the adjacent properties are among reasons for the approval of the proposed rezoning.

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JUSTIFICATION FOR REZONING

The portions of the Gwinnett County Unified Development Ordinance (the “UDO”) which classify or may classify the property which is the subject of this Application (the “Property”) into any less intensive zoning classification other than as requested by the Applicant, are or would be unconstitutional in that they would destroy the Applicant's property rights without first paying fair, adequate and just compensation for such rights, in violation of Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States.

The application of the UDO as applied to the subject Property, which restricts its use to the present zoning classification, is unconstitutional, illegal, null and void, constituting a taking of the Applicant's and the Owner's property in violation of the Just Compensation Clause of the Fifth Amendment and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States, Article I, Section I, Paragraph I, and Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Equal Protection Clause of the Fourteenth Amendment to the Constitution of the United States denying the Applicant an economically viable use of its land while not substantially advancing legitimate state interests.

The Property is presently suitable for development under the R-IF classification as requested by the Applicant, and is not economically suitable for development under the present R-100 zoning classification of Gwinnett County. A denial of this Application would constitute an arbitrary and capricious act by the Gwinnett County Board of Commissioners without any rational basis therefore, constituting an abuse of discretion in violation of Article I, Section I, Paragraph I and Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States.

A refusal by the Gwinnett County Board of Commissioners to rezone the Property to the R-IF classification with such conditions as agreed to by the Applicant, so as to permit the only feasible economic use of the Property, would be unconstitutional and discriminate in an arbitrary, capricious and unreasonable manner between the Applicant and owners of similarly situated property in violation of Article I, Section I, Paragraph II of the Constitution of the State of Georgia

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of 1983 and the Equal Protection Clause of the Fourteenth Amendment to the Constitution of the United States. Any rezoning of the subject Property to the R-IF classification, subject to conditions which are different from the conditions by which the Applicant may amend its application, to the extent such different conditions would have the effect of further restricting the Applicant's and the Owner's utilization of the subject Property, would also constitute an arbitrary, capricious and discriminatory act in zoning the Property to an unconstitutional classification and would likewise violate each of the provisions of the State and Federal Constitutions set forth hereinabove.

Opponents to the request set forth in the Application, or in any amendments to the Application, have waived their rights to appeal any decision of the Gwinnett County Board of Commissioners because they lack standing, have failed to exhaust administrative remedies, and/or because they failed to assert any legal or constitutional objections.

Accordingly, the Applicant respectfully requests that the rezoning application submitted by the Applicant relative to the Property be granted and that the Property be rezoned to the zoning classification as shown on the respective application.

This 29th day of August, 2024.

Respectfully submitted,

MAHAFFEY PICKENS TUCKER, LLP

Gabrielle Schaller

Gabrielle H. Schaller
Attorneys for Applicant

1550 North Brown Road
Suite 125
Lawrenceville, Georgia 30043
(770) 232-0000

Exhibit E: Application and Disclosure of Campaign Contributions

[attached]

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10.18.2024

REZONING APPLICATION

AN APPLICATION TO AMEND THE OFFICIAL ZONING MAP OF GWINNETT COUNTY, GA.

| APPLICANT INFORMATION | | PROPERTY OWNER INFORMATION | |
|--|--|------------------------------------|--|
| Name: _____ | | Name: _____ | |
| Address: _____ | | Address: _____ | |
| City: _____ | | City: _____ | |
| State: _____ ZIP: _____ | | State: _____ ZIP: _____ | |
| Phone: _____ | | Phone: _____ | |
| Email: _____ | | Email: _____ | |
| Contact Person: _____ Phone: _____ | | | |
| Contact's Email: _____ | | | |
| APPLICANT IS THE: | | | |
| <input type="checkbox"/> Owner's Agent <input type="checkbox"/> Property Owner <input type="checkbox"/> Contract Purchaser | | | |
| Current Zoning District(s): _____ Requested Zoning District: _____ | | | |
| Parcel Number(s): _____ Acreage: _____ | | | |
| Property Address(es): _____ | | | |
| Proposed Development: _____ | | | |
| Variance(s): _____ Waiver(s): _____ | | | |
| RESIDENTIAL DEVELOPMENT | | NON-RESIDENTIAL DEVELOPMENT | |
| No. of Dwelling Units: _____ | | No. of Buildings: _____ | |
| Dwelling Unit Sq. Ft.: _____ | | Total Building Sq. Ft.: _____ | |
| Density: _____ | | Floor Area Ratio: _____ | |
| Floor Area Ratio (LRR, MRR, HRR): _____ | | | |
| MIXED-USE DEVELOPMENT | | | |
| No. of Dwelling Units: _____ Dwelling Unit Sq. Ft.: _____ | | | |
| Total Non-Residential Sq. Ft.: _____ Floor Area Ratio: _____ | | | |

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REZONING APPLICANT'S CERTIFICATION

The undersigned below is authorized to make this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon within six (6) months from the date of last action by the Board of Commissioners.



8.22.24

Signature of Applicant

Date

MICHAEL LANGELLA

PRESIDENT

Type or Print Name and Title



8/22/24

Signature of Notary Public

Date



Notary Seal


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Gwinnett County Planning Division
Rezoning Application
Last Updated 12/2023

REZONING PROPERTY OWNER'S CERTIFICATION

The undersigned below, or as attached, is the owner of the property considered in this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon within six (6) months from the date of last action by the Board of Commissioners.




Signature of Property Owner

8/20/24

Date

Gregory W Peavy, owner

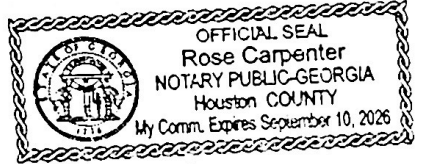
Type or Print Name and Title



Signature of Notary Public

8/20/24

Date



Notary Seal

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CONFLICT OF INTEREST CERTIFICATION FOR REZONING

The undersigned below, making application for a rezoning, has complied with the Official Code of Georgia Section 36-67A-1, et. seq, Conflict of Interest in Zoning Actions, and has submitted or attached the required information on the forms provided.

[Signature] 9/9/24 Gregory W. Peavy, Owner
Signature of Applicant Date Type of Print Name and Title

Signature of Applicant's Attorney or Representative Date Type or Print Name and Title

Sheila Hill 9/9/24 Sheila Hill
Signature of Notary Public Date NOTARY PUBLIC
Houston County, GEORGIA
My Commission Expires 07/19/2025

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

Have you, within the two years immediately preceding the filing of this application, made campaign contributions aggregating \$250.00 or more to a member of the Board of Commissioners?

☐ Yes ☒ No Gregory Peavy (Your Name)

If the answer is yes, please complete the following section:

| NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL | CONTRIBUTIONS (List all which aggregate to \$250 or More) | DATE CONTRIBUTION WAS MADE (Within last two years) |
|---|---|--|
| | | |
| | | |
| | | |
| | | |
| | | |

Attach additional sheets if necessary to disclose or describe all contributions.

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CONFLICT OF INTEREST CERTIFICATION FOR REZONING

The undersigned below, making application for a rezoning, has complied with the Official Code of Georgia Section 36-67A-1, et. seq, Conflict of Interest in Zoning Actions, and has submitted or attached the required information on the forms provided.

Michael Langella 8/22/24 MICHAEL LANGELLA PRESIDENT
Signature of Applicant Date Type or Print Name and Title

Signature of Applicant's Attorney or Representative Date Type or Print Name and Title

R. Langella 8/22/24 Notary Seal
Signature of Notary Public Date Notary Seal



DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

Have you, within the two years immediately preceding the filing of this application, made campaign contributions aggregating \$250.00 or more to a member of the Board of Commissioners?

☐ Yes ☒ No Michael Langella (Your Name)

If the answer is yes, please complete the following section:

| NAME AND OFFICAL POSITION OF GOVERNMENT OFFICIAL | CONTRIBUTIONS (List all which aggregate to \$250 or More) | DATE CONTRIBUTION WAS MADE (Within last two years) |
|--|---|--|
| | | |
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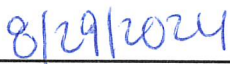
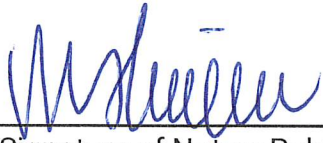

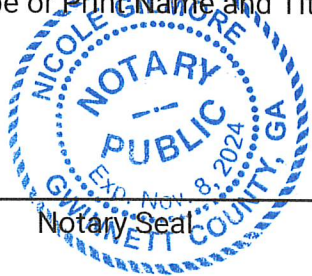
Attach additional sheets if necessary to disclose or describe all contributions.

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8.29.2024

CONFLICT OF INTEREST CERTIFICATION FOR REZONING

The undersigned below, making application for a rezoning, has complied with the Official Code of Georgia Section 36-67A-1, et. seq, Conflict of Interest in Zoning Actions, and has submitted or attached the required information on the forms provided.

| | | |
|---|---|--|
| Signature of Applicant | Date | Type of Print Name and Title |
|  |  | Gabrielle Schaller, attorney for Applicant |
| Signature of Applicant's Attorney or Representative | Date | Type or Print Name and Title |
|  |  |  |
| Signature of Notary Public | Date | Notary Seal |

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

Have you, within the two years immediately preceding the filing of this application, made campaign contributions aggregating \$250.00 or more to a member of the Board of Commissioners?

☒ Yes
 ☐ No
 Gabrielle Schaller, attorney for Applicant (Your Name)

If the answer is yes, please complete the following section:

| NAME AND OFFICAL POSITION OF GOVERNMENT OFFICIAL | CONTRIBUTIONS (List all which aggregate to \$250 or More) | DATE CONTRIBUTION WAS MADE (Within last two years) |
|--|---|--|
| Please see attached | | |
| | | |
| | | |
| | | |
| | | |

Attach additional sheets if necessary to disclose or describe all contributions.

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**CONFLICT OF INTEREST CERTIFICATION FOR REZONING/CHANGE IN
CONDITIONS AND SPECIAL USE PERMIT APPLICATIONS**

MAHAFFEY PICKENS TUCKER, LLP

| NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL | CONTRIBUTIONS | DATE |
|--|---------------|------------|
| Ben Ku, Commissioner | \$1,500 | 10/03/2022 |
| Matt Holtkamp, Commissioner | \$1,000 | 04/13/2023 |
| Nicole Love Hendrickson, Chairwoman | \$2,500 | 08/11/2023 |
| Matt Holtkamp, Commissioner | \$3,300 | 09/18/2023 |
| Kirkland Carden, Commissioner | \$3,300 | 09/27/2023 |

Lee Tucker, Attorney at Law, P.C.

| NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL | CONTRIBUTIONS | DATE |
|--|---------------|------------|
| Kirkland Carden, Commissioner | \$2,000 | 10/25/2023 |

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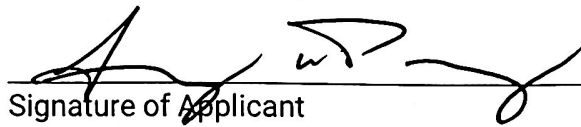
Gwinnett County Planning Division
Rezoning Application
Last Updated 12/2023

VERIFICATION OF CURRENT PAID PROPERTY TAXES FOR REZONING

The undersigned below is authorized to make this application. The undersigned certifies that all Gwinnett County property taxes billed to date for the parcel listed below have been paid in full to the Gwinnett County Tax Commissioner. In no case shall an application or reapplication for rezoning be processed without such property verification.

A SEPARATE VERIFICATION FORM MUST BE COMPLETED FOR EACH TAX PARCEL INCLUDED IN THE REZONING REQUEST.

Parcel I.D. Number: R7066 002
(Map Reference Number)


Signature of Applicant

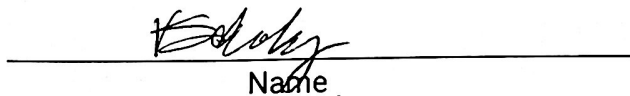
8/26/24
Date

Gregory W Peavy, owner
Type or Print Name and Title

PLEASE TAKE THIS FORM TO THE TAX COMMISSIONERS OFFICE AT THE GWINNETT JUSTICE AND ADMINISTRATION CENTER, 75 LANGLEY DRIVE. THIS FORM MUST BE SIGNED BY A REPRESENTATIVE OF THE TAX COMMISSIONER'S OFFICE.

TAX COMMISSIONERS USE ONLY

Payment of all property taxes billed to date for the above referenced parcel has been verified as paid current and confirmed by the signature below.


Name

TSA - II
Title

8/26/2024
Date

Exhibit F: DWR Sewer Capacity Certification Letter

[attached]



September 11, 2024

Jonathan Love
Havenn Homes, LLC
1100 Circle 75 Pkwy, Suite 270
Atlanta, GA 30339

☐ **APPROVED**
☐ **DENIED**
☒ **CONDITIONALLY APPROVED**
Sewer Capacity Request #C2024-224-09
Expiration Date: 09/11/2025
Tie-In Manhole FID: 207495

RE: Sewer Availability for Proposed Development – Summer Haven – Russell Road Tract
Parcel ID 7066 002

Dear Mr. Love:

This letter supersedes the earlier Sewer Capacity Certification C2021-01-011 dated January 14, 2021 addressed to Brian Taylor of Taylor Morrison.

Gwinnett County Department of Water Resources (GCDWR) has reviewed the impact of the proposed development consisting of 85 townhomes and 20 single-family homes on the above parcel and determined:

County can provide sewer service for the proposed development only upon completion of the following conditions:

- Completion by Gwinnett County of the Ridge Road Pump Station Decommissioning project with approximate completion date of 2026
- Coordination on the schedule, phasing, and design of the referenced development in relation to GCDWR's capacity expansion project in the area and the anticipated sewer connection

This confirmation is based on your anticipated annual average daily flow of **18.23 gpm** discharging to the sewer tie-in manhole at Facility ID **207495**.

Capacity Allocation: Sewer capacities are allocated only upon the issuance of a Development Permit, by Gwinnett County Planning & Development and after all applicable conditions established in this Sewer Capacity Certification, if any, are met.

Certification Expiration: The Sewer Capacity Certification is valid for 12 months from the date of this letter. If rezoning is approved, GCDWR will issue a revised Sewer Capacity Certification based on the approved units, as needed. The capacity request can be renewed for an additional 12 months. If a Development Permit has not been obtained by the expiration date above the developer may apply for renewal by providing evidence of active rezoning, plan review, or permitting efforts.

GWINNETT COUNTY
PLANNING AND DEVELOPMENT

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RZR2024-00051
9.12.2024



Once the Sewer Capacity Certification expires, the capacity is no longer allocated, and a new Sewer Capacity Certification request must be submitted for re-evaluation with no guarantee of available capacity. The evaluation will be based on the updated conditions of the sewer system.

Certification Invalidation: This certification is not valid if there are proposed changes to your development that could impact downstream sewer capacity (e.g. land use density, sewer tie-in manhole, parcel changes, and/or rezoning). In this case the development must resubmit for a new Sewer Capacity Request with updated information prior to the expiration of the previous one.

Please contact us at 678-376-7026 if you have any questions.

Sincerely,

GWINNETT COUNTY DEPARTMENT OF WATER RESOURCES

Lorraine Campagne, PE
Section Manager, Development Support
678.376.7058

C: Gwinnett County DWR – Della Taylor, Division Director; Raghu Vemuru, Engineer V; Mikala Weston, Engineer III
Gwinnett County P&D – Charli Young, Planning Manager; Geniva Sylvain, Engineer I; Christina Dejarnette, Engineer I;
Wendell Tumale Engineer III

Exhibit G: Traffic Impact Study

[attached]

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**TRAFFIC IMPACT STUDY
FOR
PROPOSED RESIDENTIAL DEVELOPMENT
AT 598 RUSSELL ROAD**

GWINNETT COUNTY, GEORGIA



Prepared for:

***Havenn Homes
1100 Circle 75 Parkway, Suite 270
Havenn Homes, GA, 30339***

Prepared By:



A&R Engineering Inc.

2160 Kingston Court, Suite O
Marietta, GA 30067
Tel: (770) 690-9255 Fax: (770) 690-9210
www.areneg.com

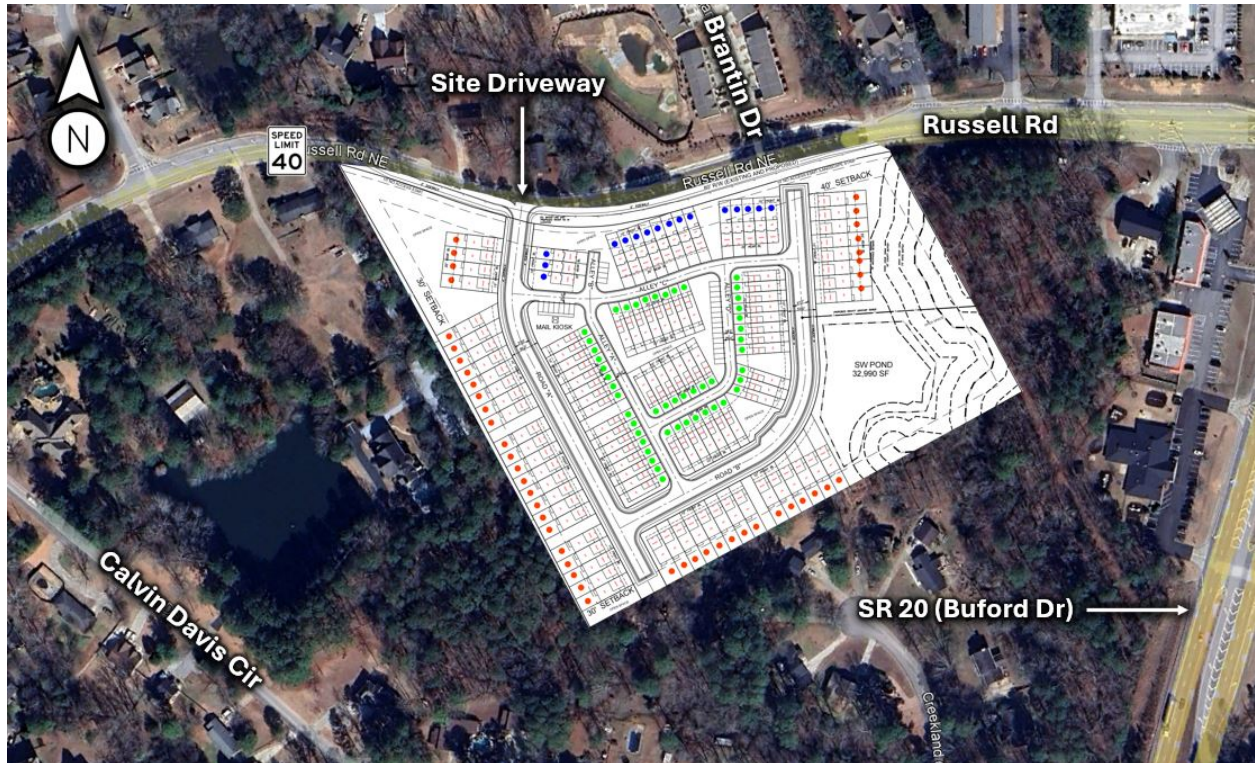
August 07, 2024
A & R Project # 24-147

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1.0 INTRODUCTION

The purpose of this study is to determine the traffic impact from the proposed residential development that will be located at 598 Russell Road in Gwinnett County, Georgia. The traffic analysis evaluates the current operations and future conditions with the traffic generated by the development. The development will consist of 112 townhomes and proposes one full access driveway on Russell Road.



The AM, school dismissal, and PM peak hours have been analysed in this study. This study includes the evaluation of traffic operations at the intersections of:

1. SR 20 (Buford Drive) at Russell Road / Ridge Road
2. Russell Road at Brantin Drive
3. Russell Road at Calvin Davis Circle

Recommendations to improve traffic operations have been identified as appropriate and are discussed in detail in the following sections of the report. The location of the development and the surrounding roadway network are shown in Figure 1.

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Study Intersection



LOCATION MAP

FIGURE 1
A&R Engineering Inc.

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2.0 EXISTING FACILITIES / CONDITIONS

2.1 Roadway Facilities

The following is a brief description of each of the roadway facilities located in proximity to the site:

2.1.1 SR 20 (Buford Drive)

SR 20 (Buford Drive) is a north-south, four-lane, median-divided roadway with a posted speed limit of 50 mph in the vicinity of the site. Georgia Department of Transportation (GDOT) traffic counts (Station ID: 135-0125) indicate that the daily traffic volume on SR 20 (Buford Drive) in 2023 was 34,500 vehicles per day north of Russell Road. Both GDOT and Gwinnett County classify SR 20 (Buford Drive) as a principal arterial roadway.

2.1.2 Russell Road

Russell Road is an east-west, two-lane, undivided roadway with a posted speed limit of 40 mph in the vicinity of the site. Georgia Department of Transportation (GDOT) traffic counts (Station ID: 135-8025) indicate that the daily traffic volume on Russell Road in 2023 was 8,220 vehicles per day west of Calvin Davis Drive. Russell Road changes names to the Ridge Road to the east of its intersection with SR 20. Gwinnett County classifies Russell Road as a major collector roadway.

2.1.3 Ridge Road

Ridge Road is an east-west, two-lane, undivided roadway with a posted speed limit of 40 mph in the vicinity of the site. Ridge Road changes names to Russell Road to the west of its intersection with SR 20.

2.1.4 Calvin Davis Circle

Calvin Davis Circle is a two-lane undivided roadway with a posted speed limit of 25 mph in the vicinity of the site.

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3.0 STUDY METHODOLOGY

In this study, the methodology used for evaluating traffic operations at each of the subject intersections is based on the criteria set forth in the Transportation Research Board Highway Capacity Manual, 6th edition (HCM 6). Synchro software, which utilizes the HCM methodology, was used for the analysis. The following is a description of the methodology employed for the analysis of unsignalized and signalized intersections.

3.1 Unsignalized Intersections

For unsignalized intersections controlled by a stop sign on minor streets, the level of service (LOS) for motor vehicles with controlled movements is determined by the computed control delay according to the thresholds stated in Table 1 below. LOS is determined for each minor street movement (or shared movement), as well as major street left turns. LOS is not defined for the intersection as a whole or for major street approaches. The LOS of any controlled movement which experiences a volume-to-capacity ratio greater than 1 is designated as “F” regardless of the control delay.

Control delay for unsignalized intersections includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. Several factors affect the control delay for unsignalized intersections, such as the availability and distribution of gaps in the conflicting traffic stream, critical gaps, and follow-up time for a vehicle in the queue.

Level of service is assigned a letter designation from “A” through “F”. Level of service “A” indicates excellent operations with little delay to motorists, while level of service “F” exists when there are insufficient gaps of acceptable size to allow vehicles on the side street to cross the main road without experiencing long delays.

| TABLE 1 — LEVEL-OF-SERVICE CRITERIA FOR UNSIGNALIZED INTERSECTIONS | | |
|--|----------------------------------|-----------|
| Control Delay (sec/vehicle) | LOS by Volume-to-Capacity Ratio* | |
| | v/c ≤ 1.0 | v/c > 1.0 |
| ≤ 10 | A | F |
| > 10 and ≤ 15 | B | F |
| > 15 and ≤ 25 | C | F |
| > 25 and ≤ 35 | D | F |
| > 35 and ≤ 50 | E | F |
| > 50 | F | F |

*The LOS criteria apply to each lane on a given approach and to each approach on the minor street. LOS is not calculated for major-street approaches or for the intersection.

Source: Highway Capacity Manual, 6th edition, Exhibit 20-2 *LOS Criteria: Motorized Vehicle Mode*

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3.2 Signalized Intersections

According to HCM procedures, LOS can be calculated for the entire intersection, each intersection approach, and each lane group. HCM uses control delay alone to characterize LOS for the entire intersection or an approach. Control delay per vehicle is composed of initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. Both control delay and volume-to-capacity ratio are used to characterize LOS for a lane group. A volume-to-capacity ratio greater than 1.0 for a lane group indicates failure from capacity perspective. Therefore, such a lane group is assigned LOS F regardless of the amount of control delay.

Table 2 below summarizes the LOS criteria from HCM for motorized vehicles at signalized intersection.

| TABLE 2 — LEVEL-OF-SERVICE CRITERIA FOR SIGNALIZED INTERSECTIONS | | |
|--|---|-----------|
| Control Delay (sec/vehicle) * | LOS for Lane Group by Volume-to-Capacity Ratio* | |
| | v/c ≤ 1.0 | v/c > 1.0 |
| ≤ 10 | A | F |
| > 10 and ≤ 20 | B | F |
| > 20 and ≤ 35 | C | F |
| > 35 and ≤ 55 | D | F |
| > 55 and ≤ 80 | E | F |
| > 80 | F | F |

*For approach-based and intersection wide assessments, LOS is defined solely by control delay

Source: Highway Capacity Manual, 6th edition, Exhibit 19-8 *LOS Criteria: Motorized Vehicle Mode*

LOS A is typically assigned when the volume-to-capacity (v/c) ratio is low and either progression is exceptionally favourable, or the cycle length is very short. LOS B is typically assigned when the v/c ratio is low and either progression is highly favorable, or the cycle length is short. However, more vehicles are stopped than with LOS A. LOS C is typically assigned when progression is favorable, or the cycle length is moderate. Individual *cycle failures* (one or more queued vehicles are not able to depart because of insufficient capacity during the cycle) may begin to appear at this level. Many vehicles still pass through the intersection without stopping, but the number of vehicles stopping is significant. LOS D is typically assigned when the v/c ratio is high and either progression is ineffective, or the cycle length is long. There are many vehicle-stops and individual cycle failures are noticeable. LOS E is typically assigned when the v/c ratio is high, progression is very poor, the cycle length is long, and individual cycle failures are frequent. LOS F is typically assigned when the v/c ratio is very high, progression is very poor, the cycle length is long, and most cycles fail to clear the queue.

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4.0 ADDITIONAL INFORMATION – WINNETT COUNTY LEVEL 2 TRAFFIC IMPACT STUDY

The following sections have been included per Gwinnett County's Traffic Impact Study Guidelines (updated June 30, 2023) to meet the requirements of a Level 2 traffic study. Additional information regarding access management/spacing, intersection sight distance, or connectivity and circulation should be requested from the civil site engineer.

4.1 Future ADT Volumes

As shown in section 2 (Page 3), the roads within the study network with AADT volumes recorded by GDOT are SR 20 (Buford Drive) and Russell Road. The daily traffic volume on SR 20 (Buford Drive) in 2023 was 34,500 vehicles per day north of Russell Road. Based on the proposed site traffic generation/distribution, it is estimated that the AADT on SR 20 (Buford Drive) would be approximately 35,390 vehicles per day following the completion of the development in 2026. The AADT on Russell Road in 2023 was 8,220 vehicles per day west of Calvin Davis Drive, and it is estimated that this volume will increase to approximately 8,670 vehicles per day at this location following the completion of the development in 2026. Given how these estimated increases also factor in an assumed annual growth rate of 1% from 2024 to 2026, it can be concluded that the impact of the proposed site on the daily traffic for the roadways within the study network will be minimal.

4.2 Truck Volumes and Circulation

Since the proposed site will be residential, it is not expected to have any notable impact on truck volumes/circulation in the study network.

4.3 Summary of Existing Pedestrian and Bicycle Facilities and Connectivity

Pedestrian crosswalks exist at the signalized intersection of SR 20 (Buford Drive) and Russell Road. Sidewalks exist along all roadways within the vicinity of the site, except for the section of Calvin Davis Circle to the south of Russell Road. This study does not include any recommendations for new pedestrian and/or bicycle facilities within or near the study network. Any additional information regarding planned pedestrian and/or bicycle facilities within the proposed residential development itself should be requested from the civil site engineer.

4.4 Existing Transit Routes and Stops/Proposed Transit Routes

None of Gwinnett County's current transit routes or stops are located on near the SR 20 (Buford Drive) corridor that falls within the study network. There are no bus stops located within a 1-mile radius of the proposed development. No transit routes have been proposed to service this development in the future.

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4.5 Traffic Operation Analysis Requirements

A detailed description of the study methodology is available in section 3 on pages 4-5. A description of the site trip generation and distribution estimations is available on page 15.

4.6 Future Identified Projects (i.e., GCCTP, GDOT, SPLOST)

There are no planned/scheduled GDOT/Gwinnet County projects that will be completed within the study network or that will have any notable impact on the study network.

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5.0 EXISTING 2024 TRAFFIC ANALYSIS

5.1 Existing Traffic Volumes

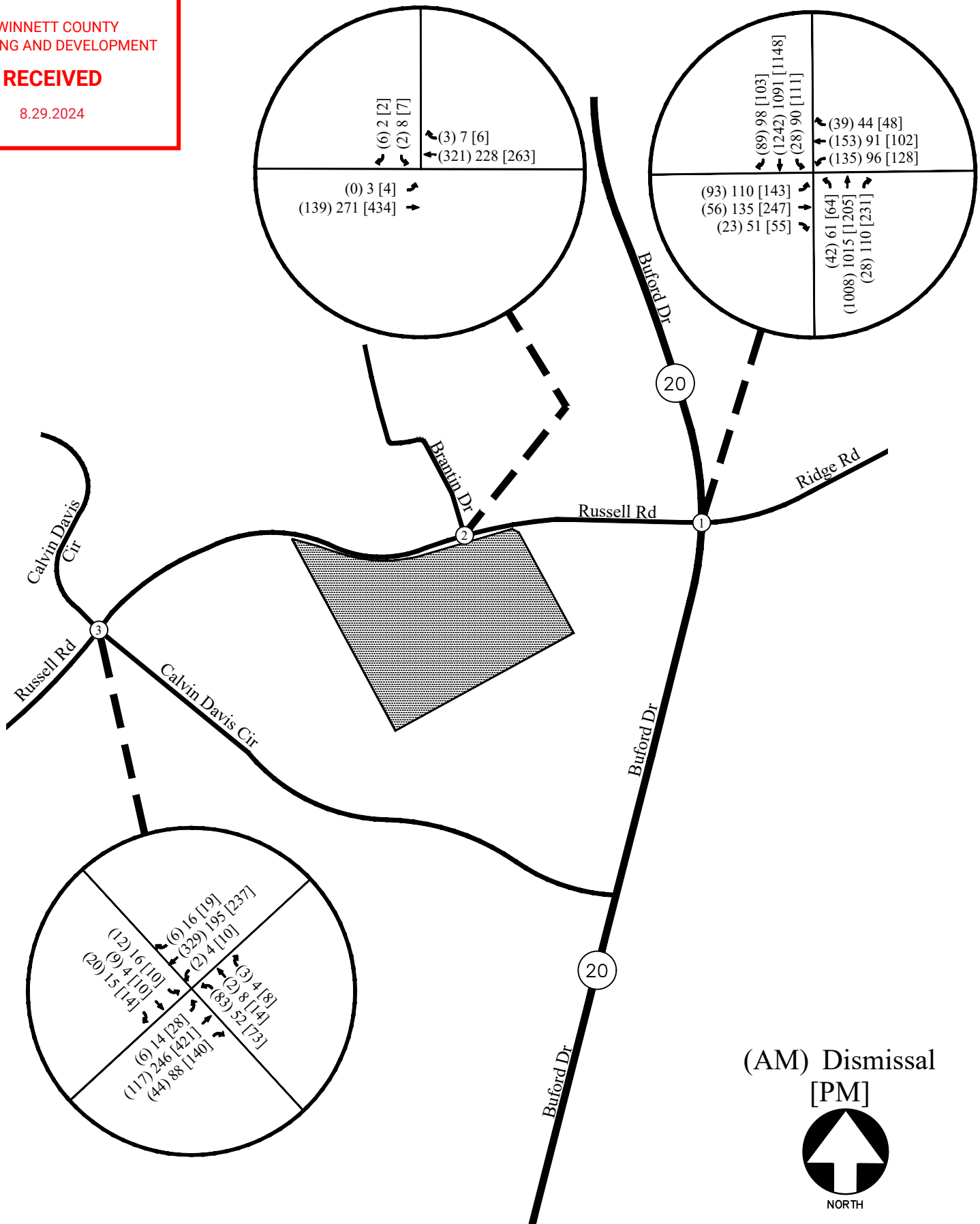
Existing traffic counts were obtained at the following study intersections:

1. SR 20 (Buford Drive) at Russell Road / Ridge Road
2. Russell Road at Brantin Drive
3. Russell Road at Calvin Davis Circle

Turning movement counts were collected on Wednesday, July 24, 2024. All turning movement counts were recorded during the AM and PM peak hours between 7:00 AM to 9:00 AM and 2:00 PM to 6:00 PM, respectively. The four consecutive 15-minute interval volumes that produced the highest volume at the intersections were then determined. These counted volumes make up the peak hour traffic volumes for the intersection and are shown in Figure 2. The existing AM and PM traffic volumes were increased by 10% (and the afternoon dismissal peak hour volumes were increased by 15%) to account for the fact that schools were not in session when the data was collected. These adjusted volumes are shown in Figure 3 and were used in the existing traffic operations analysis. The existing traffic control and lane geometry for the study intersections are shown in Figure 4.

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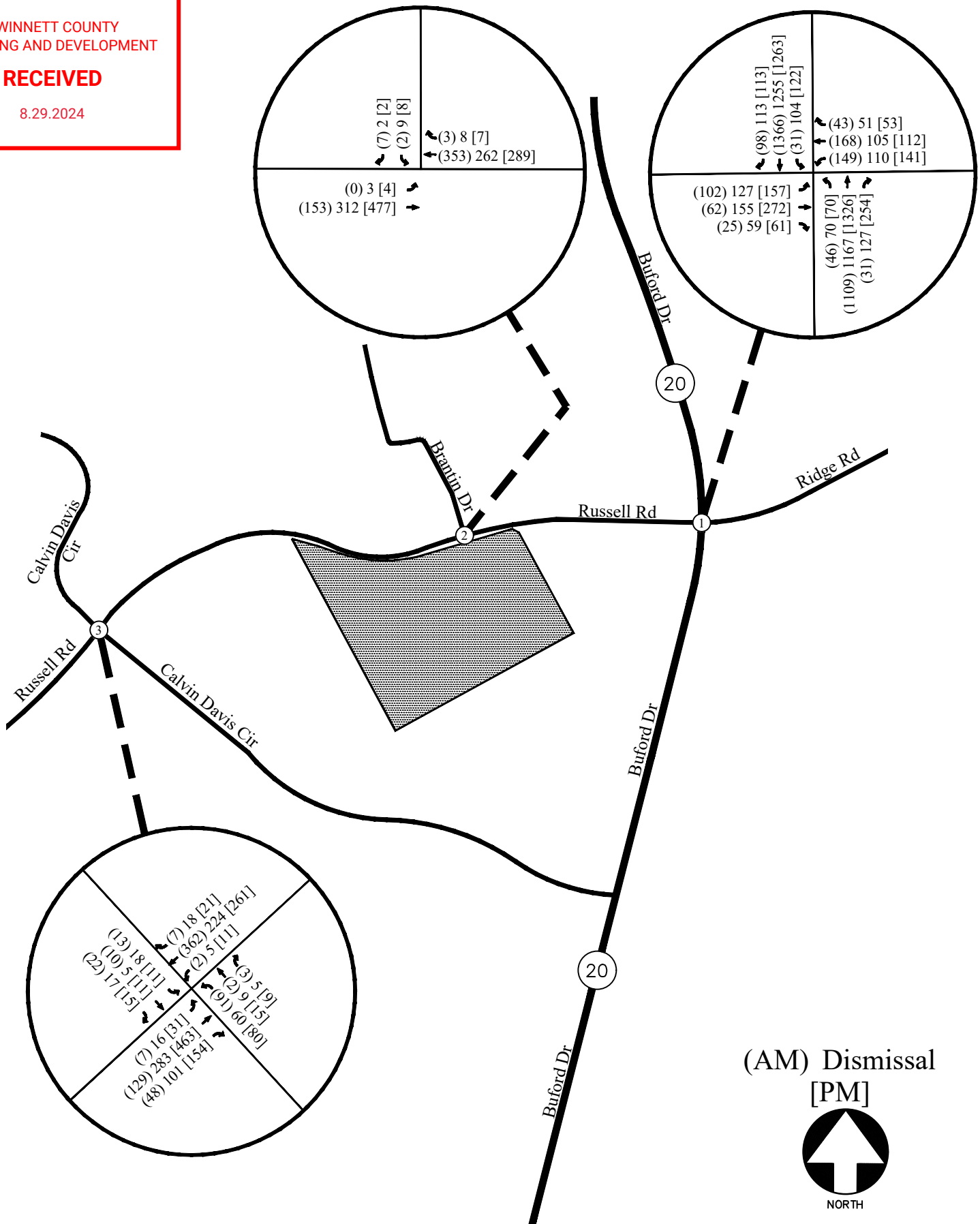


EXISTING WEEKDAY PEAK-HOUR VOLUMES
(WITHOUT SCHOOL TRAFFIC)

FIGURE 2
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EXISTING WEEKDAY PEAK-HOUR VOLUMES
(ADJUSTED FOR SCHOOL TRAFFIC)

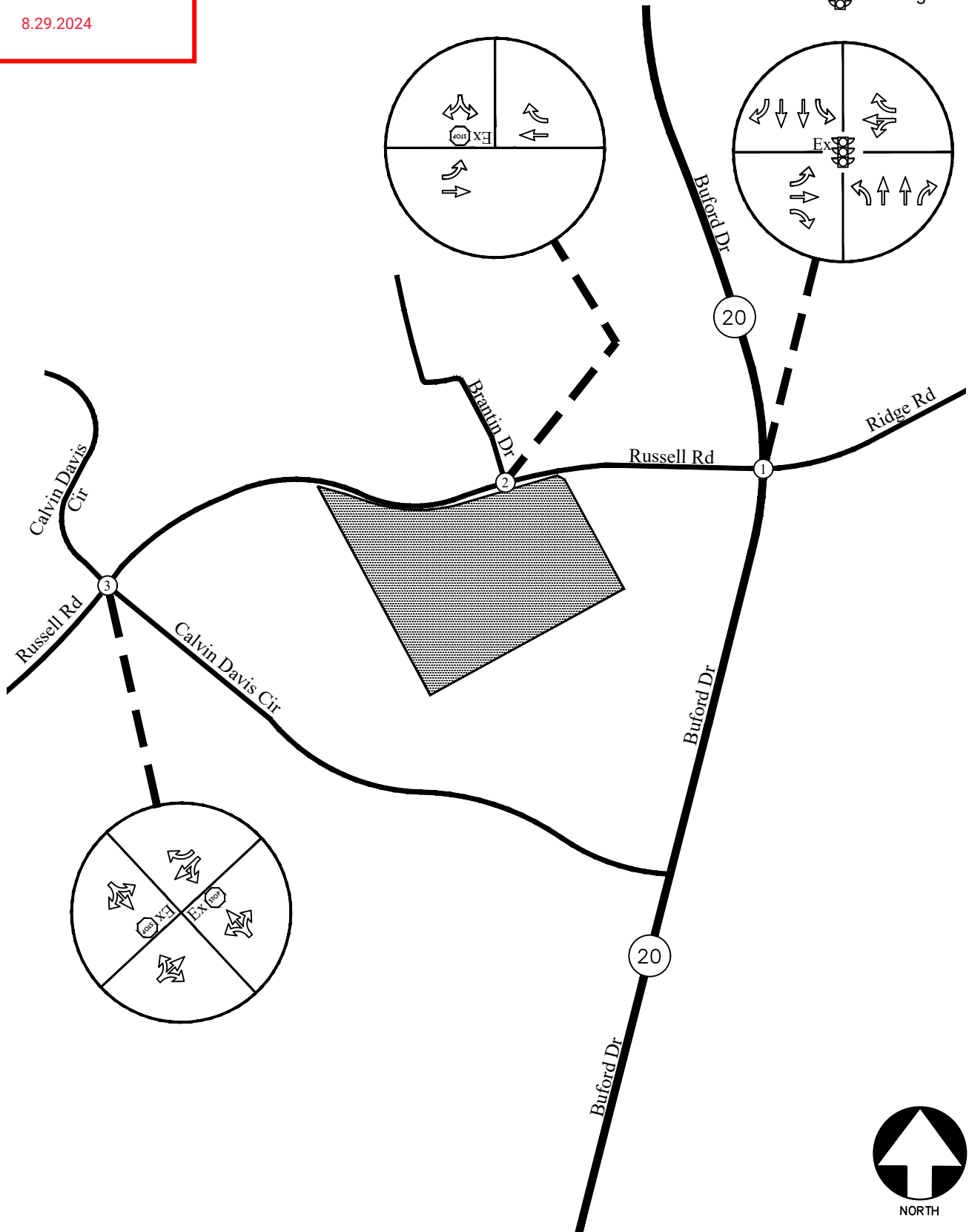
FIGURE 3
A&R Engineering Inc.

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LEGEND

- Ex  Existing Signed Approach
-  Existing Lane Geometry
- Ex  Existing Traffic Signal



EXISTING TRAFFIC CONTROL AND LANE GEOMETRY

FIGURE 4

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5.2 Existing Traffic Operations

Existing 2024 traffic operations were analyzed at the study intersections in accordance with the HCM methodology. The results of the analysis are shown in Table 3.

| TABLE 3 — EXISTING INTERSECTION OPERATIONS | | | | | |
|--|--|---|------------------------|------------------------|------------------------|
| Intersection | | Traffic Control | LOS (Delay) | | |
| | | | AM Peak | School Dismissal | PM Peak |
| 1 | <u>SR 20 (Buford Drive) @ Russell Road / Ridge Road</u> | Signalized | <u>C (32.2)</u> | <u>C (29.3)</u> | <u>D (44.8)</u> |
| | -Eastbound Approach | | E (64.3) | E (56.2) | E (68.7) |
| | -Westbound Approach | | E (64.9) | E (56.6) | E (74.9) |
| | -Northbound Approach | | C (23.1) | C (32.2) | D (39.6) |
| | -Southbound Approach | | C (27.5) | C (23.8) | D (36.6) |
| 2 | <u>Russell Road @ Brantin Drive</u> | Stop Controlled on SB Approach | A (8.2) | A (7.9) | A (7.9) |
| | -Eastbound Left -Southbound Approach | | B (11.2) | B (13.0) | B (14.9) |
| 3 | <u>Russell Road @ Calvin Davis Circle</u> | Stop Controlled on NB and SB Approaches | A (8.2) | A (7.8) | A (8.0) |
| | -Eastbound Left | | A (7.7) | A (8.1) | A (9.0) |
| | -Westbound Left | | C (20.0) | C (17.2) | E (38.0) |
| | -Northbound Approach | | B (13.7) | B (13.3) | B (19.1) |
| | -Southbound Approach | | | | |

The results of the existing traffic operations analysis indicate that the signalized study intersection of SR 20 (Buford Drive) is operating at an overall level of service “D” or better in the AM, school dismissal, and PM peak hours. The stop-controlled approaches at the unsignalized study intersections are operating at a level of service “D” or better with peak hour traffic, except for the northbound (Calvin Davis Circle) approach at intersection 3, which will operate at a level of service “E” in the PM peak hour.

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6.0 PROPOSED DEVELOPMENT

The proposed residential development will be located at 598 Russell Road in Gwinnett County and will consist of 112 townhomes.



The development proposes one full access driveway on Russell Road.

A site plan is shown in Figure 5.

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6.1 Trip Generation

Trip generation estimates for the project were based on the rates and equations published in the 11th edition of the Institute of Transportation Engineers (ITE) Trip Generation report. This reference contains traffic volume count data collected at similar facilities nationwide. The trip generation was based on the ITE land use category 215 – *Single-Family Attached Housing*. The calculated total trip generation for the proposed development is shown in Table 4.

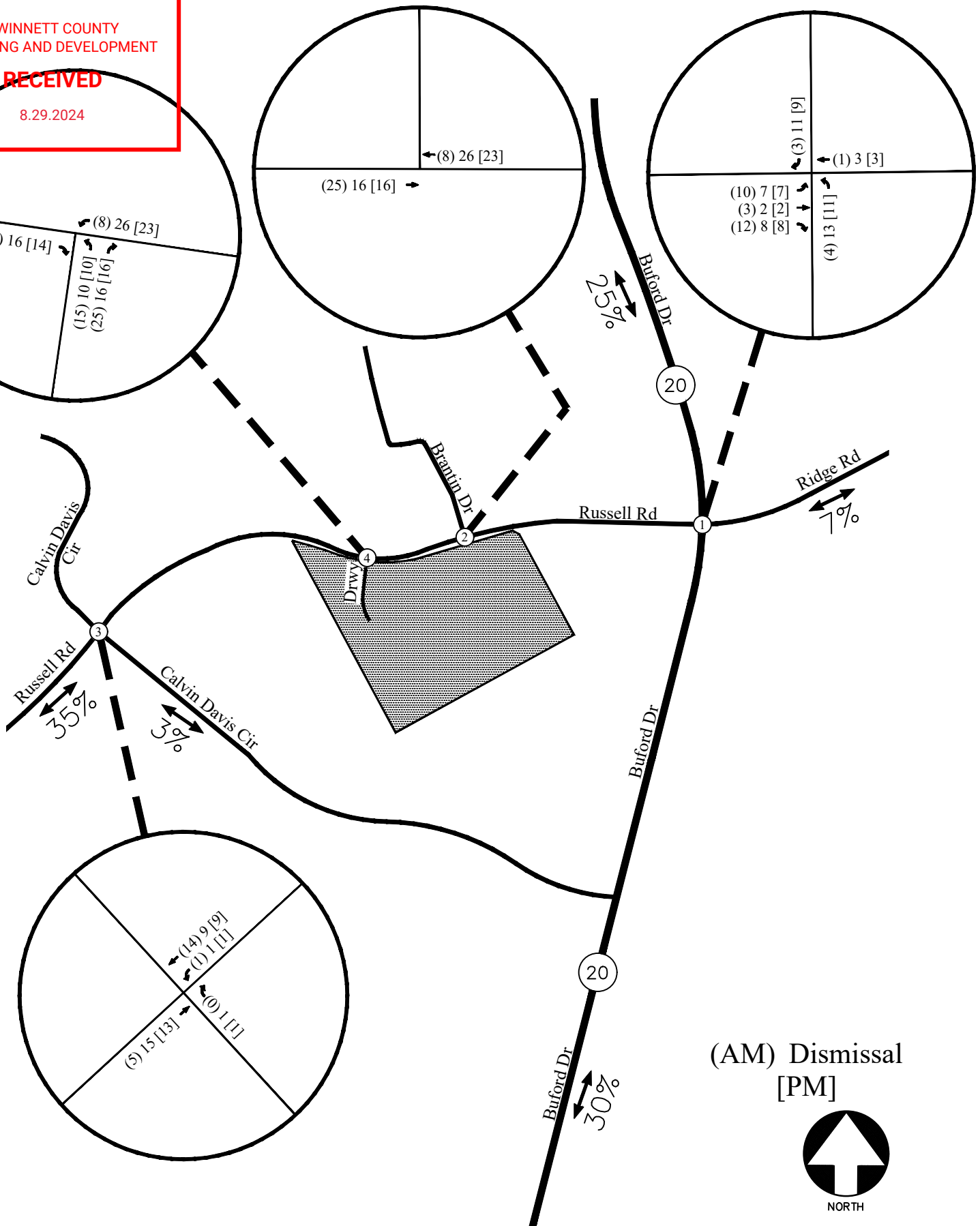
| TABLE 4 – TRIP GENERATION | | | | | | | | | | | |
|---|--------------|--------------|------|-------|-------------------------------|------|-------|--------------|------|-------|---------|
| Land Use | Size | AM Peak Hour | | | School Dismissal Peak Hour | | | PM Peak Hour | | | 24 Hour |
| | | Enter | Exit | Total | Enter | Exit | Total | Enter | Exit | Total | Two-Way |
| ITE 215 – Single-Family Attached Housing | 112 Units | 13 | 40 | 53 | 42 | 26 | 68 | 37 | 26 | 63 | 803 |

6.2 Trip Distribution

The trip distribution describes how traffic arrives and departs from the site. An overall trip distribution was developed for the site based on a review of the existing travel patterns in the area and the locations of major roadways and highways that will serve the development. The site-generated peak hour traffic volumes, shown in Table 4, were assigned to the study area intersections based on this distribution. The outer-leg distribution and AM, school dismissal, and PM peak hour traffic volumes generated by the site are shown in Figure 6.

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TRIP DISTRIBUTION AND NEW SITE-GENERATED
WEEKDAY PEAK HOUR VOLUMES

FIGURE 6
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7.0 FUTURE 2026 TRAFFIC ANALYSIS

The future 2026 traffic operations are analyzed for the “Build” and “No-Build” conditions.

7.1 Future “No-Build” Conditions

The “No-Build” (or background) conditions provide an assessment of how traffic will operate in the study horizon year without the study site being developed as proposed, with projected increases in through traffic volumes due to normal annual growth. The future “No-Build” volumes consist of the existing traffic volumes (Figure 2) plus increases due to the annual growth of through traffic.

7.1.1 Annual Traffic Growth

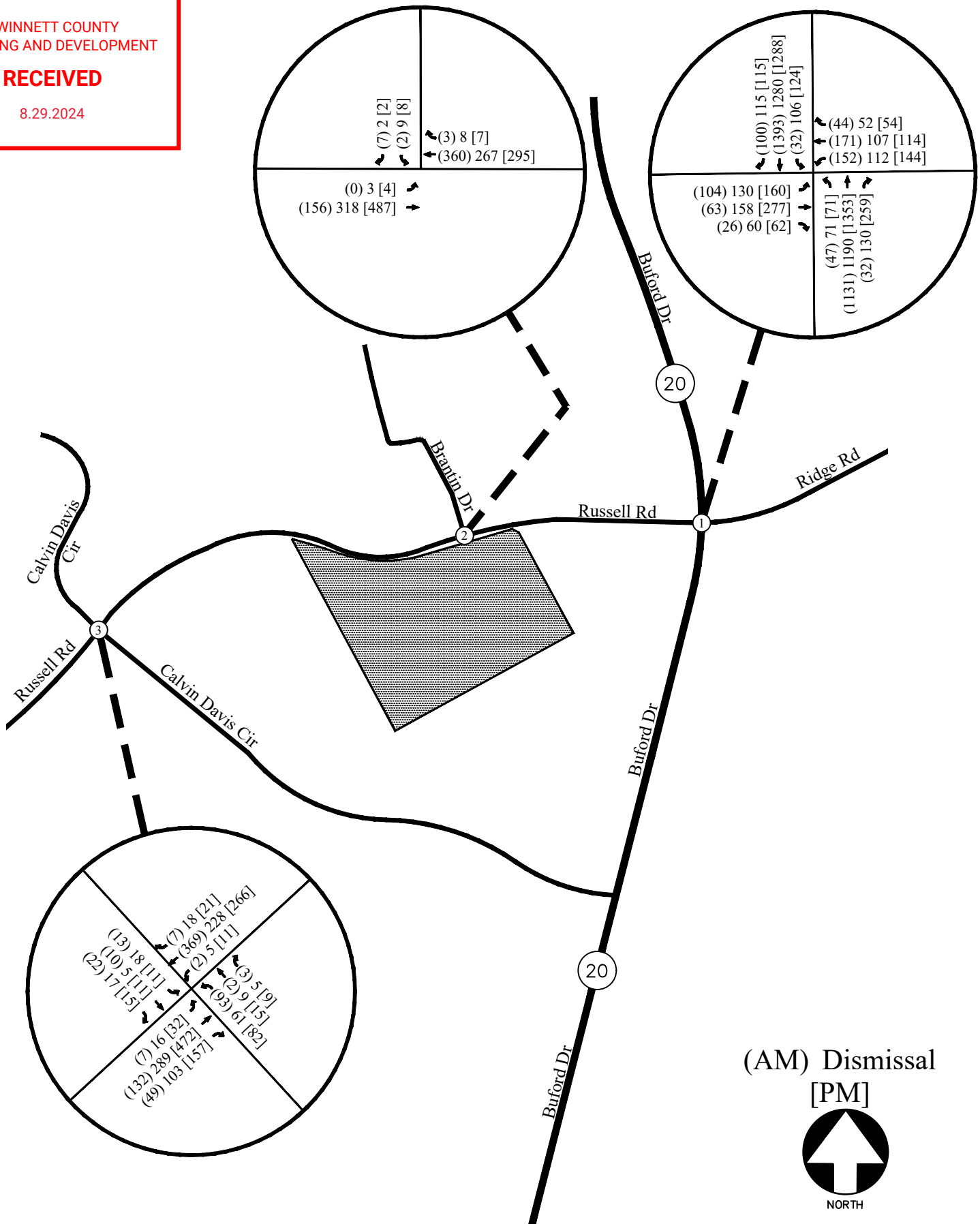
To evaluate future traffic operations in this area, a projection of normal traffic growth was applied to the existing volumes. The Georgia Department of Transportation recorded average daily traffic volumes at several locations in the vicinity of the site. Reviewing the growth over the last five years (2018-2019 & 2021-2023) revealed a traffic volume increase of approximately 1% in the area. This growth factor was applied to the existing traffic volumes between collector and arterial roadways to estimate the future year traffic volumes prior to the addition of site-generated traffic. The resulting future “No-Build” volumes on the roadway are shown in Figure 7.

7.2 Future “Build” Conditions

The “Build” or development conditions include the estimated background traffic from the “No-Build” conditions plus the added traffic from the proposed development. To evaluate future traffic operations in this area, the additional traffic volumes from the site (Figure 6) were added to base traffic volumes (Figure 7) to calculate the future traffic volumes after the construction of the development. These total future “Build” traffic volumes are shown in Figure 8.

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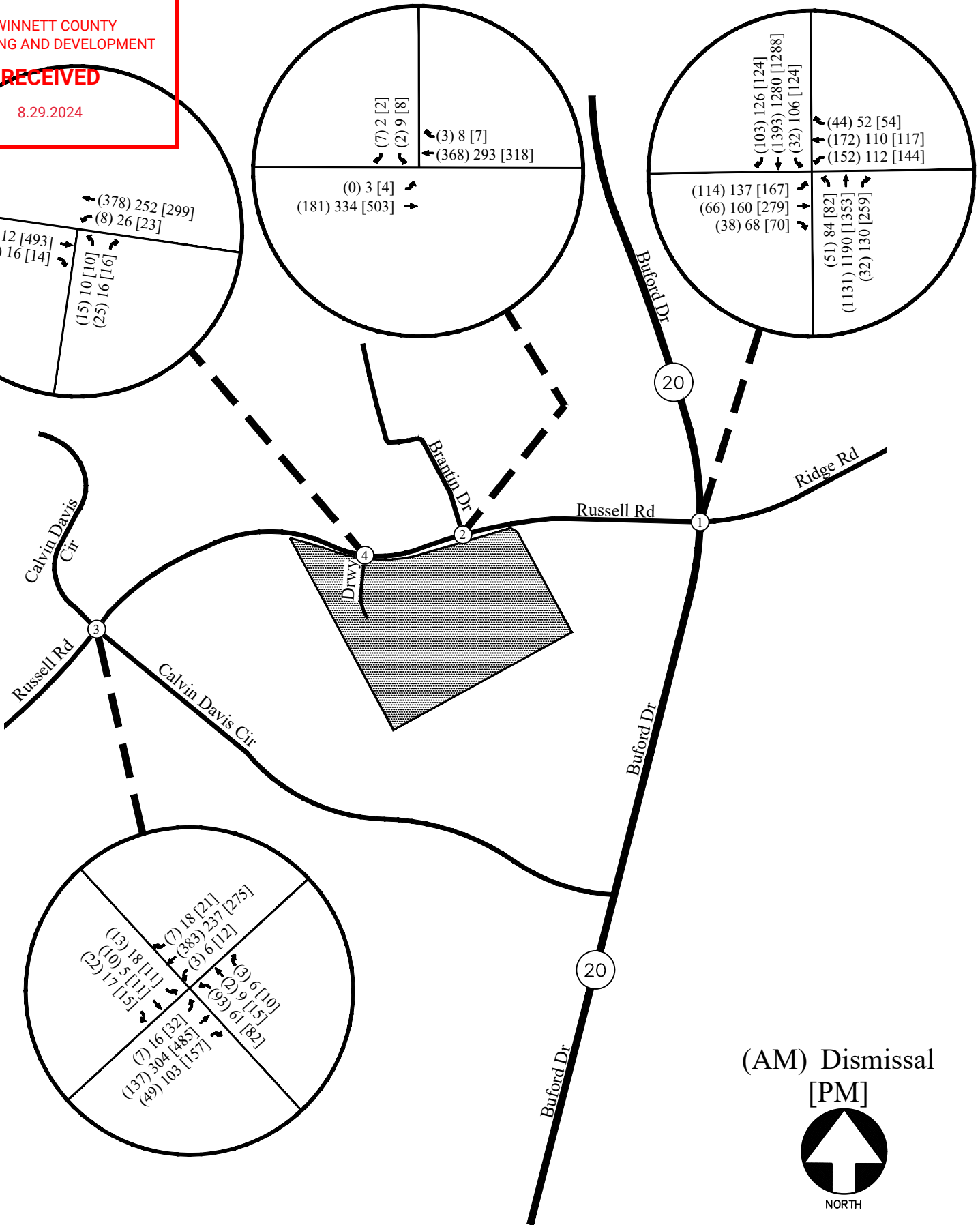
FUTURE (NO-BUILD) WEEKDAY PEAK HOUR VOLUMES

FIGURE 7

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FUTURE (BUILD) WEEKDAY PEAK HOUR VOLUMES

FIGURE 8
A&R Engineering Inc.

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7.3 Auxiliary Lane Analysis

Included below are analyses for right turn lanes for the site driveway. The analyses below are based off the trip distribution included in Section 5.2. According to the trip distribution, the overall 24-hour two-way volume for traffic entering and exiting the site are 803 vehicles.

7.3.1 Left Turn Lane Analysis

According to Gwinnett County Criteria and Guidelines for Left Turn Lanes, left turn lanes must be provided at each subdivision street that accesses a minor/major collector street or major thoroughfare if the characteristics of the major street and site density exceed certain thresholds. Per Gwinnett County left turn lane standards (Table 1 – Residential Developments), the minimum threshold for a left turn lane on a 40-mph road with an ADT above 6,000 is 65 lots/units. Since the proposed development will have 112 townhome units and will therefore exceed the threshold, a left turn lane is warranted at the site driveway.

7.3.2 Deceleration Turn Lane Analysis

According to Gwinnett County Unified Development Ordinance (UDO) Standards, a right turn deceleration lane shall be required at each subdivision street entrance that is provided street access to a Minor/Major Collector Street or Major Thoroughfare (900-30.2). Since Russell Road is classified by Gwinnett County as a Major Collector Street, a right turn lane is warranted at the site driveway.

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7.4 Future Traffic Operations

The future “No-Build” and “Build” traffic operations were analyzed using the volumes in Figures 7 and 8, respectively. The results of the future traffic operations analyses are shown below in Table 5. Recommendations on traffic control and lane geometry are shown in Figure 9.







| TABLE 5 — FUTURE INTERSECTION OPERATIONS | | | | | | | |
|--|--|-------------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| Intersection | | Future Condition: LOS (Delay) | | | | | |
| | | NO-BUILD | | | BUILD | | |
| | | AM | Dismissal | PM | AM | Dismissal | PM |
| 1 | <u>SR 20 (Buford Drive) @ Russell Road / Ridge Road</u> | <u>C (33.4)</u> | <u>C (30.3)</u> | <u>D (47.4)</u> | <u>C (34.6)</u> | <u>C (30.9)</u> | <u>D (48.2)</u> |
| | -Eastbound Approach | E (65.7) | E (57.5) | E (70.7) | E (65.9) | E (58.2) | E (71.0) |
| | -Westbound Approach | E (67.3) | E (58.8) | E (77.4) | E (68.9) | E (59.8) | E (78.2) |
| | -Northbound Approach | C (23.9) | C (24.0) | D (42.4) | C (24.6) | C (24.4) | D (43.1) |
| | -Southbound Approach | C (28.6) | C (24.7) | D (39.0) | C (29.7) | C (25.1) | D (40.1) |
| 2 | <u>Russell Road @ Brantin Drive</u> | | | | | | |
| | -Eastbound Left | A (8.2) | A (7.9) | A (7.9) | A (8.2) | A (8.0) | A (8.0) |
| | -Southbound Approach | B (11.3) | B (13.1) | C (15.1) | B (11.5) | B (13.6) | C (15.7) |
| 3 | <u>Russell Road @ Calvin Davis Circle</u> | | | | | | |
| | -Eastbound Left | A (8.3) | A (7.8) | A (8.0) | A (8.3) | A (7.8) | A (8.0) |
| | -Westbound Left | A (7.7) | A (8.2) | A (9.0) | A (7.7) | A (8.2) | A (9.1) |
| | -Northbound Approach | C (20.7) | C (17.6) | E (41.2) | C (21.7) | C (18.2) | E (44.5) |
| | -Southbound Approach | B (13.9) | B (13.5) | C (19.5) | B (14.2) | B (13.8) | C (20.2) |
| 4 | <u>Russell Road @ Site Driveway</u> | | | | | | |
| | -Westbound Left | - | - | - | A (7.6) | A (8.1) | A (8.6) |
| | -Northbound Approach | - | - | - | B (10.8) | B (11.8) | B (14.4) |

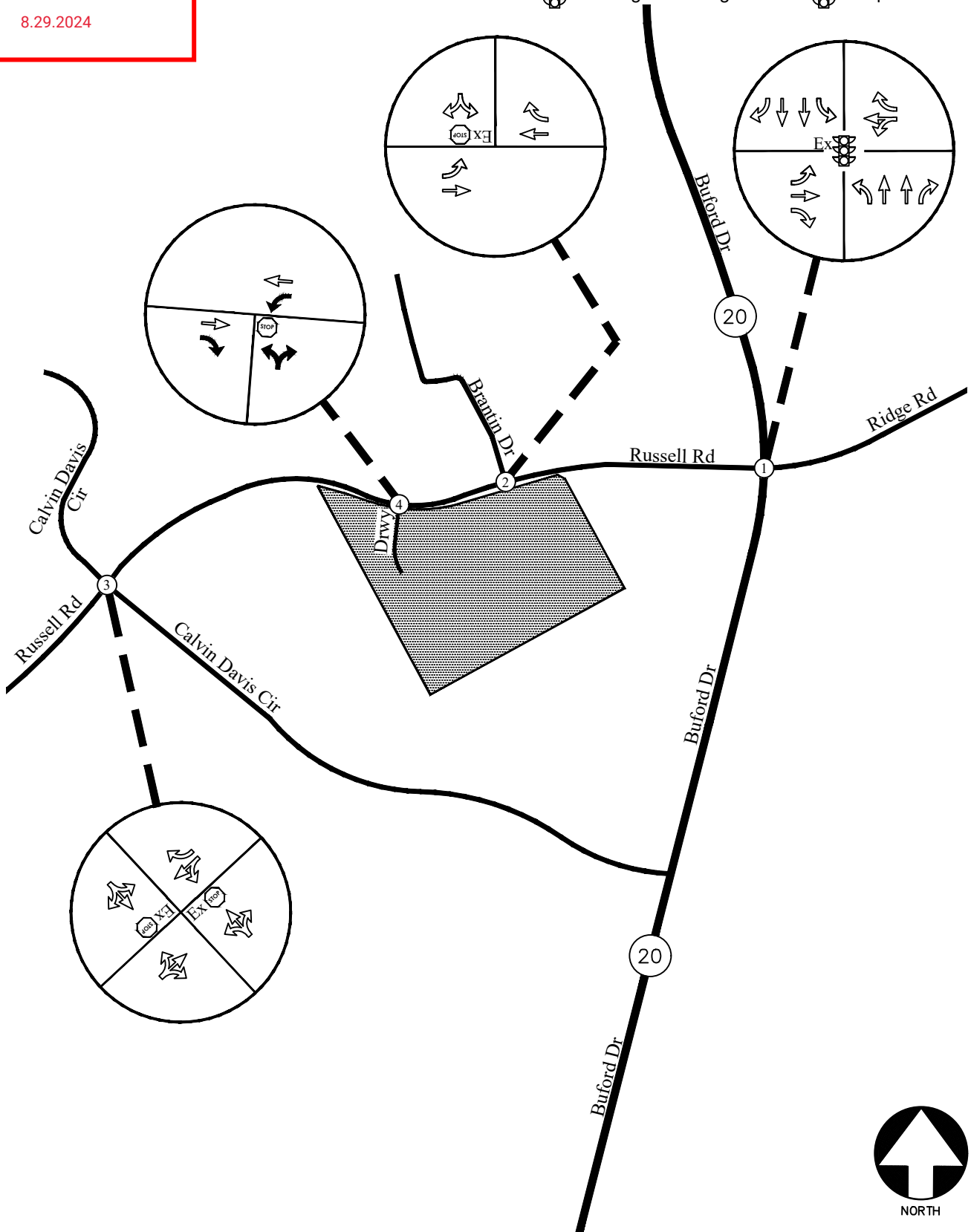
The results of the “No Build” and “Build” traffic operations analysis indicate that the signalized study intersection of SR 20 (Buford Drive) will continue to operate at an overall level of service “D” or better in the AM, school dismissal, and PM peak hours. The stop-controlled approaches at the unsignalized study intersections will operate at a level of service “D” or better with peak hour traffic, except for the northbound (Calvin Davis Circle) approach at intersection 3, which will operate at a level of service “E” in the PM peak hour under both “No-Build” and “Build” conditions. It is not unusual for minor side streets that are stop-controlled to experience higher delays due to the time gap required for vehicles to make turning movements on a busy collector roadway.

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LEGEND

- | | | | |
|--|--------------------------|---|--------------------------|
| Ex  | Existing Signed Approach |  | Proposed Signed Approach |
|  | Existing Lane Geometry |  | Proposed Lane Geometry |
| Ex  | Existing Traffic Signal |  | Proposed Traffic Signal |



FUTURE TRAFFIC CONTROL AND LANE GEOMETRY

FIGURE 9
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8.0 CONCLUSIONS AND RECOMMENDATIONS

Traffic impacts were evaluated for the proposed residential development that will be located at 598 Russell Road in Gwinnett County, Georgia. The development will consist of 112 townhomes and proposes one full access driveway on Russell Road.

Existing and future operations after completion of the project were analyzed at the intersections of:

1. SR 20 (Buford Drive) at Russell Road / Ridge Road
2. Russell Road at Brantin Drive
3. Russell Road at Calvin Davis Circle
4. Russell Road at Site Driveway

The analysis included the evaluation of future operations for “No-Build” and “Build” conditions, with the differences between “No-Build” and “Build” accounting for increases in traffic due to the proposed development. The results of the “No Build” and “Build” traffic operations analysis indicate that the signalized study intersection of SR 20 (Buford Drive) will continue to operate at an overall level of service “D” or better in the AM, school dismissal, and PM peak hours. The stop-controlled approaches at the unsignalized study intersections will operate at a level of service “D” or better with peak hour traffic, except for the northbound (Calvin Davis Circle) approach at intersection 3, which will operate at a level of service “E” in the PM peak hour under both “No-Build” and “Build” conditions. It is not unusual for minor side streets that are stop-controlled to experience higher delays due to the time gap required for vehicles to make turning movements on a busy collector roadway. Based on the analysis, the proposed development will have minimal impact on traffic operations in the study network.

8.1 Recommendations for Site Access Configuration

The following access configuration is recommended for the proposed site driveway intersection:

- Site Driveway: Full Access Driveway on Russell Road
 - One entering lane and one exiting lane
 - Stop-sign controlled on the driveway approach with Russell Road remaining free flow
 - A right turn lane and a left turn lane for entering traffic
 - Provide/confirm adequate sight distance per AASHTO standards

Exhibit H: Public Participation Plan and Report

[attached]



Public Participation Plan

1. Who do you intend to contact in addition to property owners within 1,000 feet of the site, if any?

District 4 Commissioner Matthew Holtkamp

2. Where do you plan to host the Public Participation Meeting? Ensure that the meeting is held in person at a convenient location for interested parties.

The Meeting Room at Rhodes Jordan Park Community Recreation Center

3. What date and time do you plan to host the Public Participation Meeting? Ensure that the meeting is held at a convenient time for interested parties, e.g. not during typical work hours on weekdays.

September 26th from 6 to 8 p.m.

4. What is your method for providing opportunities for discussion with interested parties at the Public Participation Meeting?

Attendees will be given the opportunity to ask questions before the conclusion of the meeting. There will be a box to collect written questions or concerns.



Public Participation Report

1. List all groups that you notified of the requested application. Include a copy of the notification package and stamped Postal Service Form 3877 (attached) with itemized name and address of each addressee.

Please see attached.

2. Provide the date, time, and location of all meeting(s) that the applicant and/or representative attended to discuss an application with interested parties.

Rhodes Jordan Park, 100 E Crogan Street, Lawrenceville, Georgia 30046, 6 p.m. September 26, 2024

3. Provide the number of people who participated in the meeting(s). Include the sign-in sheet(s) with meeting date, time, location, and attendee names.

5 neighbors attended the meeting. See sign-in sheet attached. 5 applicant representatives attended the meeting as well: Mike Langella, Len Meyer, Bradley Dunckel, Tess Shaheen, and Gabrielle Schaller.

4. What issues and concerns were expressed by attendees at the meeting(s)?

Please see attached.

5. What are the applicant's responses to the issues and concerns that were expressed at the meetings? What changes to the development will be made?

Please see attached.

David Belle Isle
Matthew P. Benson
Catherine W. Davidson
Gerald Davidson, Jr.*
Brian T. Easley
Rebecca B. Gober
John D. Hipes*
Christopher D. Holbrook
Jessica R. Kelly
Shane M. Lanham

Julia A. Maxwell
Jeffrey R. Mahaffey
Steven A. Pickens
Jack M. Ryan
Gabrielle H. Schaller
S. Tess Shaheen
Andrew D. Stancil
R. Lee Tucker, Jr.

*Of Counsel

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10.1.2024

PUBLIC PARTICIPATION REPORT FOR
CASE NUMBER RZR2024-00051

The purpose of this letter is to provide more detail to the Gwinnett County Planning and Development Department about the discussions the Applicant had with the community during the public participation meeting for case number RZR2024-00051.

Overall, the Applicant was pleased with the outcome of the meeting. The Applicant appreciated the opportunity to meet members of the community and hear about their feelings regarding the proposed development.

4. What issues and concerns were expressed by attendees at the meeting?

Here is a non-exhaustive list of the attendees' comments: questions: if applicant had a landscape plan; general questions regarding ownership and development operations; sewer capacity in the area; why applicant not developing as currently zoned; provide larger buffer; elevation variety; could applicant do less townhomes? concerns: deer habitat being disturbed; traffic in the area; creek in the back of the property attracting children; noise; light pollution; water runoff.

5. What are the applicant's responses to the issues and concerns that were expressed at the meetings? What changes to the development will be made?

The Applicant intends to make a good faith effort to address all the attendees' concerns and will provide a detailed update to the attendees as soon as practicable but no later than the planning commission public hearing.

Applicant is open to providing a larger buffer between the proposed development and neighbors. Applicant explained to attendees that there is no required buffer between the proposed zoning district, R-IF, and the neighbors' property, R-100. However, Applicant believes a buffer is beneficial to its future residents as well as the neighbors and shows a 20-foot buffer on the current site plan. Applicant is looking into providing an even larger buffer than what is shown on the current site plan. Applicant is also looking into removing some of the townhome units and

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replacing them with single-family detached units in response to an attendee's request for less townhome units. Applicant will provide a revised site plan to staff and neighbors when available.

At the public participation meeting, Applicant was able to provide helpful information to the attendees and answer some of their questions. Applicant displayed a site plan and elevations on presentation boards at the meeting. Applicant also explained the development will have a minimum of three elevations each for the three floor plans. Streetscapes will be composed to ensure varied elevations. The color palette of the elevations is complementary to homes in the surrounding area. Applicant explained to the attendees that Applicant will be required to provide a landscape plan and lighting plan during the permitting phase. Applicant also mentioned that Applicant will have to meet county landscaping, architecture, and lighting requirements. Regarding sewer capacity, Applicant has a condition approval sewer capacity certification. Sewer capacity certification is included in the application package. The Applicant listened to the attendees' concerns about the traffic in the area. Applicant informed the attendees that they provided a traffic impact study with the application submittal, and that the study will become public record with the application. Applicant read some of the results of the traffic study to the attendees at the meeting – particularly the numbers on how the development will affect the level of service at the intersections that were studied.

Regarding the attendees' concern about displacement of wildlife from the property, Applicant intends to enhance the stormwater detention pond to make it a sanctuary for birds and other wildlife, subject to county department planning and development approval. The Applicant will strive to minimize the impact to the wildlife as much as possible. Applicant pitched the idea of creating a stormwater management pond that will serve as a habitat for birds and other fauna rather than an eyesore to the attendees. The attendees seemed receptive to the idea.

The Applicant explained why the proposed development is appropriate on the subject property in response to an attendee's question why rezone the property. The Applicant discussed a variety of reasons including compatibility with the policies of the 2045 Comprehensive Plan and surrounding uses, and the need for missing middle homes in Gwinnett. The Applicant briefly explained the 2045 Comprehensive Plan and its designation of future development types on areas of the county and its role in rezoning decisions. Applicant expressed his opinion that the proposed development is the best and highest use of the property. Applicant also read a statement from the property owner that expressed his approval of the proposed development. The property owner and applicant team all stated that the proposed development will be of high quality and will improve the area. Applicant also explained the economics behind why the proposed development works better than leaving the property as R-100.

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10.1.2024

Exhibit I: Internal and External Agency Review Comments

[attached]



Department of Planning and Development
TECHNICAL REVIEW COMMITTEE

| | | | |
|---------------------------------------|--|--|-------------------------------------|
| TRC Meeting Date: | | 10.04.2024 | |
| Department/Agency Name: | | Transportation | |
| Reviewer Name: | | Brent Hodges | |
| Reviewer Title: | | Construction Manager 2 | |
| Reviewer Email Address: | | Brent.Hodges@gwinnettcountry.com | |
| Case Number: | | RZR2024-00051 | |
| Case Address: | | 598 Russell Road, Lawrenceville, 30043 | |
| Comments: | | <input checked="" type="checkbox"/> X | <input type="checkbox"/> YES |
| | | <input type="checkbox"/> | <input type="checkbox"/> NO |
| 1 | Russell Road is a major collector. The Average Daily Traffic (ADT) volume is 10,976. | | |
| 2 | The nearest Ride Gwinnett facility is located 2.4 miles away at the Buford Drive (SR 20) and Old Peachtree Road Publix. | | |
| 3 | Traffic calming shall be provided for any internal street exceeding 500' without a control point. | | |
| 4 | | | |
| 5 | | | |
| 6 | | | |
| 7 | | | |
| Recommended Zoning Conditions: | | <input checked="" type="checkbox"/> X | <input type="checkbox"/> YES |
| | | <input type="checkbox"/> | <input type="checkbox"/> NO |
| 1 | The site driveway shall align with Brantin Drive and include an eastbound right-turn deceleration lane as well as a westbound left-turn lane along Russell Road into the site. | | |
| 2 | | | |
| 3 | | | |
| 4 | | | |
| 5 | | | |
| 6 | | | |
| 7 | | | |

Note: Attach additional pages, if needed

Revised 7/26/2021

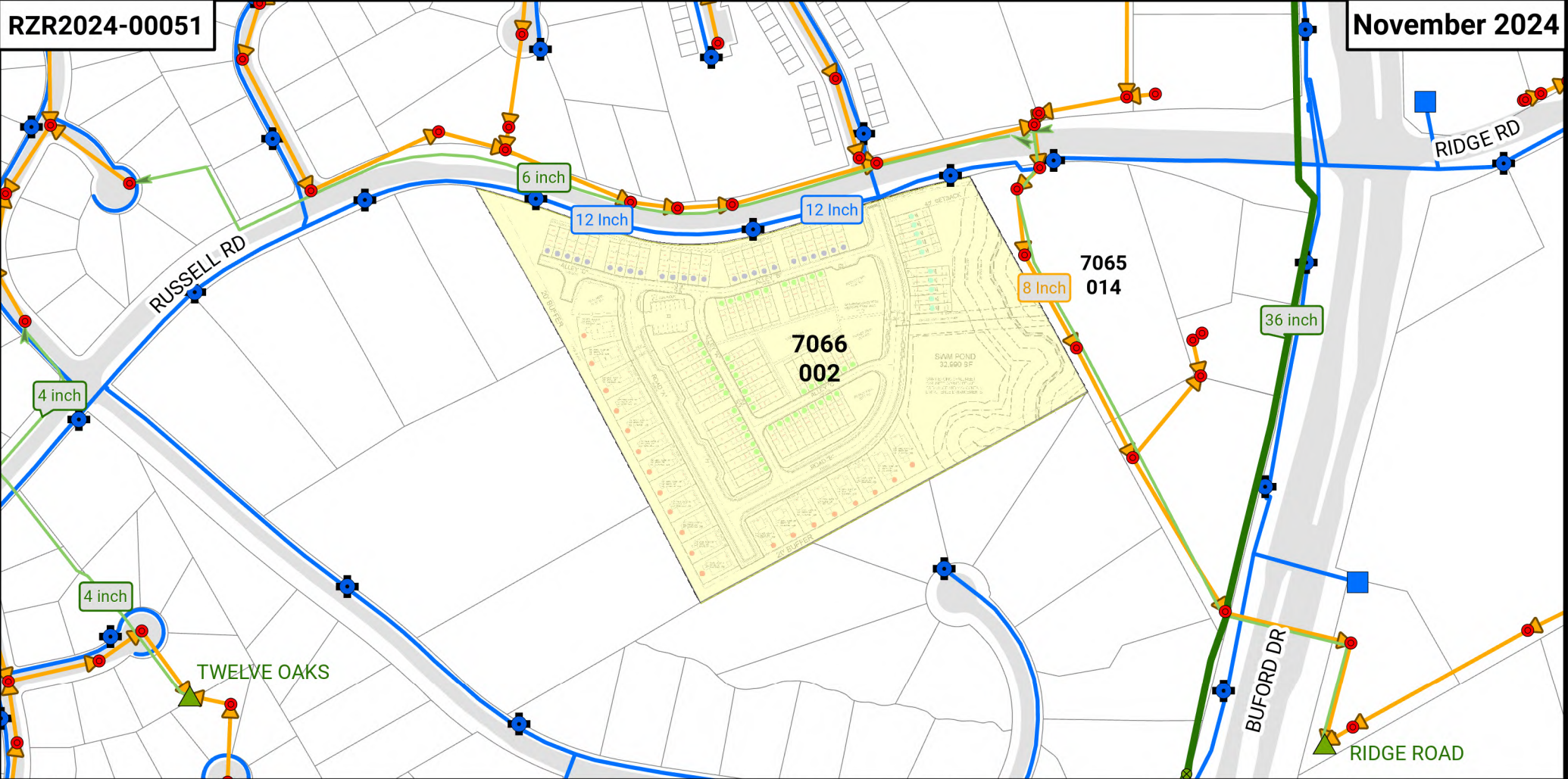


Department of Planning and Development
TECHNICAL REVIEW COMMITTEE

| | | | | |
|--------------------------------|---|--|-----|--|
| TRC Meeting Date: | | | | |
| Department/Agency Name: | | DWR | | |
| Reviewer Name: | | Mike Pappas | | |
| Reviewer Title: | | GIS Planning Manager | | |
| Reviewer Email Address: | | Michael.pappas@gwinnettcountry.com | | |
| Case Number: | | RZR2024-00051 | | |
| Case Address: | | 598 Russel Road | | |
| Comments: | | <input checked="" type="checkbox"/> | YES | <input type="checkbox"/> NO |
| 1 | Water: The proposed development may connect to an existing 12-inch water main along the southern right-of-way of Russell Road. | | | |
| 2 | Sewer: A Sewer Capacity Certification (C2024-224-09) has been conditionally approved for 85 townhomes and 20 single family homes. The proposed development plans to connect to an existing 8-inch gravity sewer located on parcel 7065 014. | | | |
| 3 | | | | |
| 4 | | | | |
| 5 | | | | |
| 6 | | | | |
| 7 | | | | |
| Recommended Zoning Conditions: | | <input type="checkbox"/> | YES | <input checked="" type="checkbox"/> NO |
| 1 | | | | |
| 2 | | | | |
| 3 | | | | |
| 4 | | | | |
| 5 | | | | |
| 6 | | | | |
| 7 | | | | |

Note: Attach additional pages, if needed

Revised 7/26/2021



LEGEND

Water Main

Hydrant

Master Vault

Sewer Collector

Sewer Force Main

Pump Station

Manhole

598 Russell Rd
R-100 to R-IF

**Water & Sewer
Utility Map**

0 160 320
Feet

LOCATION

Water Comments: The proposed development may connect to an existing 12-inch water main along the southern right-of-way of Russell Road.

Sewer Comments: A Sewer Capacity Certification (C2024-224-09) has been conditionally approved for 85 townhomes and 20 single family homes. The proposed development plans to connect to an existing 8-inch gravity sewer located on parcel 7065 014.

Water Availability: Water demands imposed by the proposed development may require upsizing or extensions of existing water mains in order to meet Gwinnett County Standards and fire flow demands. Any cost associated with such required improvements will be the responsibility of the development. Current Gwinnett County Standards require a minimum of 12" pipe size for commercial developments and a minimum of 8" pipe size for residential developments. Additionally, connection to a minimum of 12" and 8" mains are required for commercial and residential developments, respectively. It is the responsibility of the developer's engineer to confirm pressure and volumes are available for the development.

Sewer Availability: A Sewer Capacity Certification must be obtained from Gwinnett County to confirm the existing system can serve the development. Sewer demands imposed by the proposed development may require upsizing and/or extensions of existing sewer mains, and/or upsizing of an existing pump station, and/or installation of a new pump station. Any cost associated with such required improvements will be the responsibility of the development. The developer shall provide easements for future sewer connection to all locations designated by Gwinnett County during plan review.

Water and Sewer Design and Construction Requirements: Extensions of the water and/or sanitary sewer systems within the subject development must conform to this department's policies, Gwinnett County's ordinances, and the Water Main and Sanitary Sewer Design and Construction Standards and Specifications, dated April 5th, 2016. Subsequent to design, construction, inspection, and final acceptance of the required utilities, service would then become available under the applicable utility permit rate schedules.

Private Road Developments: Any development with private roads must comply with the Standard Policy Requirement for the Installation of Water and Sanitary Sewer Mains within Private Developments. This policy stipulates minimum easement requirements and location of public mains and appurtenances, among other requirements.

Residential Rezoning Impact on Local Schools
Prepared for Gwinnett County BOC, November, 2024

| | | | | | | | | | | | Proposed Zoning |
|---------------|---------------------------------|--------------|----------|----------|--------------|----------|----------|--------------|----------|----------|---|
| | School | 2024-25 | | | 2025-26 | | | 2026-27 | | | Approximate Student Projections from Proposed Developments |
| | | Forecast | Capacity | +/- Cap. | Forecast | Capacity | +/- Cap. | Forecast | Capacity | +/- Cap. | |
| RZR2024-00051 | Collins Hill High School | 2,500 | 2,625 | -125 | 2,458 | 2,625 | -167 | 2,402 | 2,625 | -223 | 18 |
| | Creekland Middle School | 1,677 | 2,100 | -423 | 1,662 | 2,100 | -438 | 1,679 | 2,100 | -421 | 12 |
| | Taylor Elementary School | 872 | 1,300 | -428 | 881 | 1,300 | -419 | 894 | 1,300 | -406 | 23 |
| RZR2024-00056 | Seckinger High School | 2,326 | 2,800 | -474 | 2,452 | 2,800 | -348 | 2,550 | 2,800 | -250 | 1 |
| | Jones Middle School | 1,820 | 1,575 | 245 | 1,893 | 1,575 | 318 | 1,969 | 1,575 | 394 | 1 |
| | Ivy Creek Elementary School | 1,583 | 1,275 | 308 | 1,614 | 1,275 | 339 | 1,646 | 1,275 | 371 | 2 |

Exhibit J: Maps

[attached]



MONTRACHET DR

WYNARD PL

BRANTIN DR

RUSSELL RD

CREEKLAND CV

BUFORD DR
BUFORD DR

IN DAVIS CIR

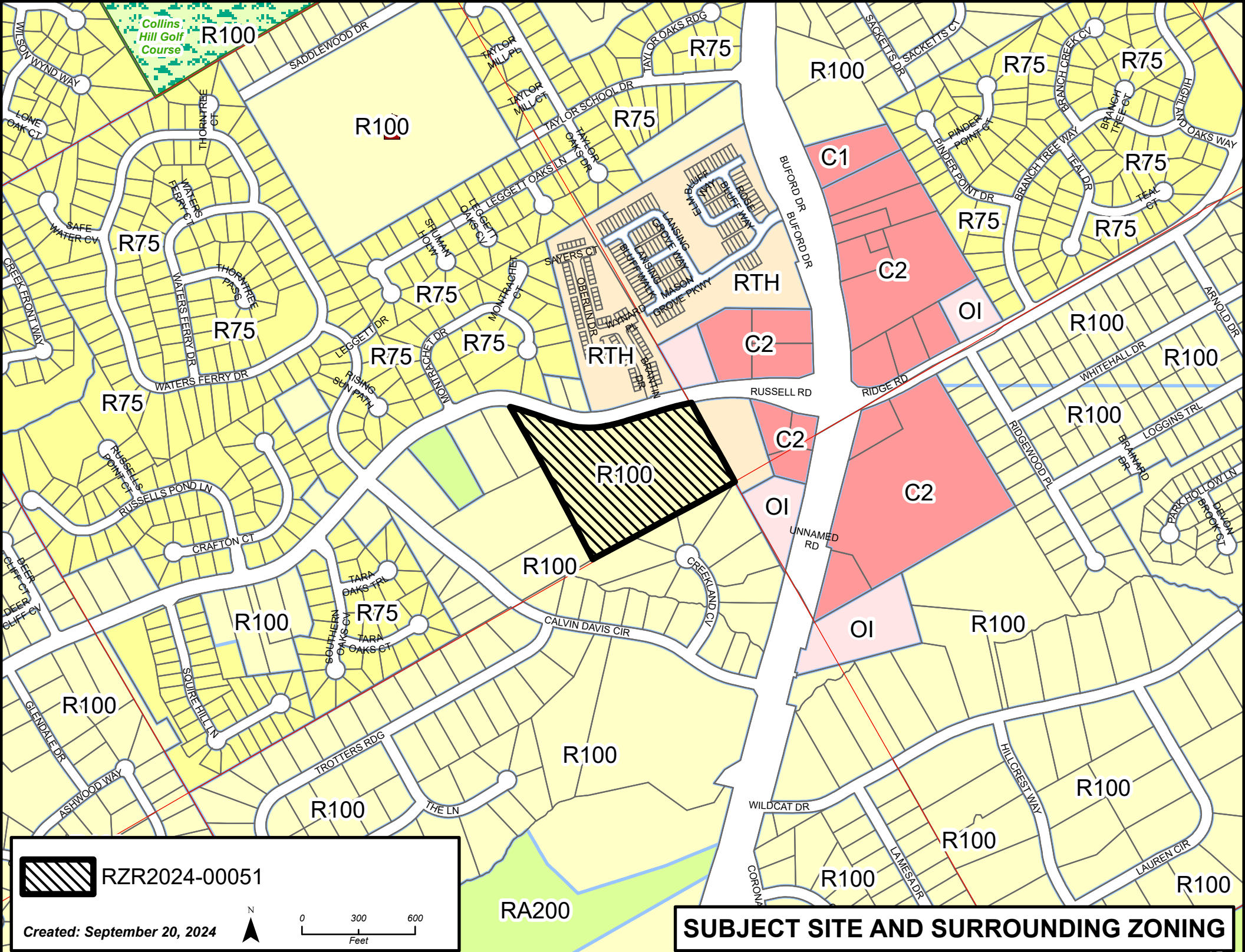
 RZR2024-00051

Created: September 20, 2024



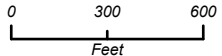
0 100 200
Feet

Gwinnett County GIS

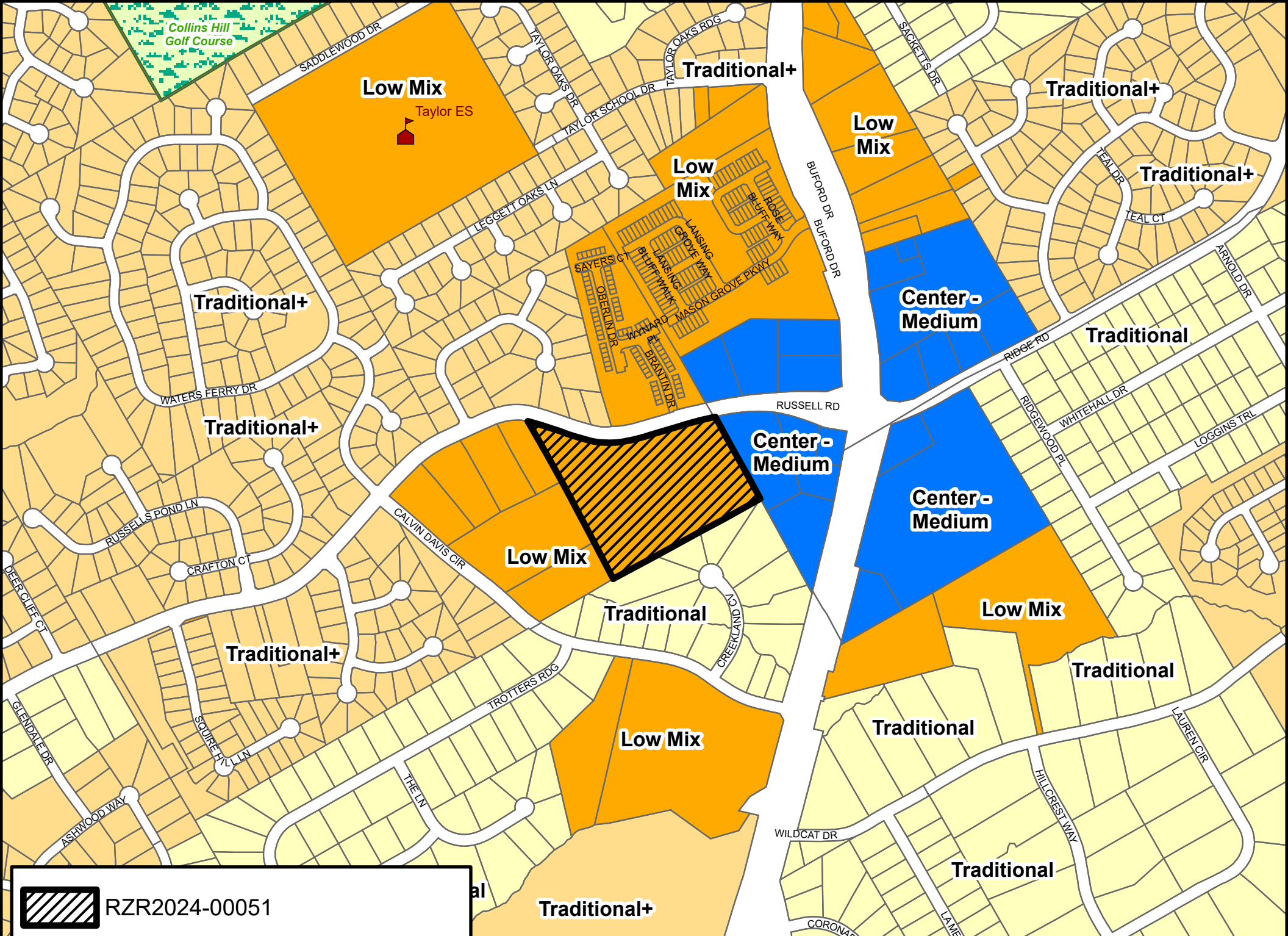


 RZR2024-00051

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SUBJECT SITE AND SURROUNDING ZONING



 RZR2024-00051

Created: September 20, 2024



2045 UNIFIED PLAN FUTURE DEVELOPMENT CLASSIFICATION

**Exhibit K: Fence Exhibit Presented at the November 4, 2024
Planning Commission Public Hearing**

[attached]

RECEIVED

11.4.2024

