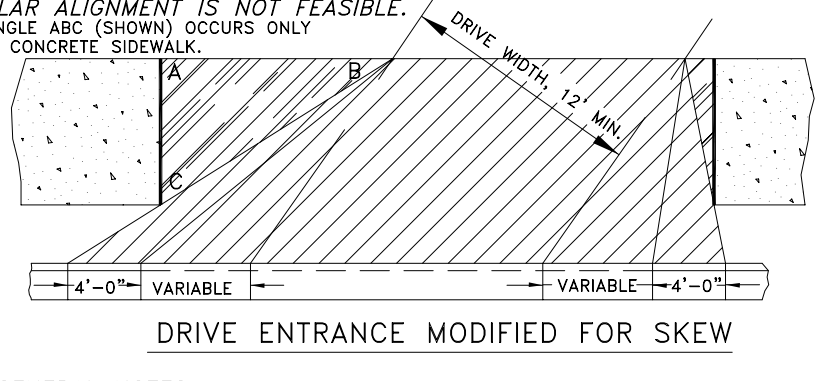
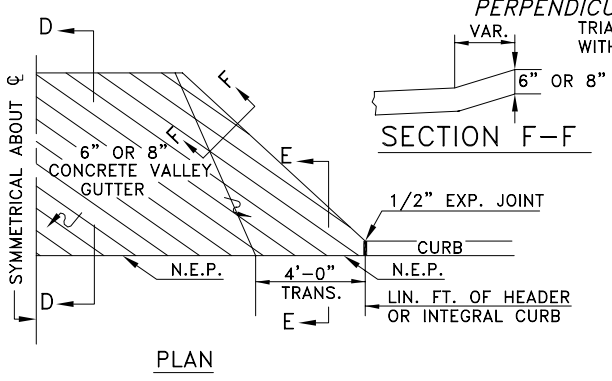
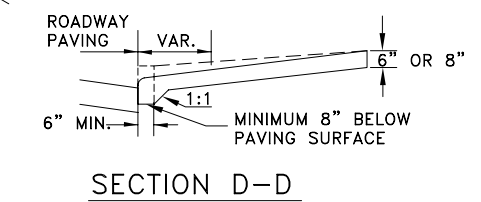
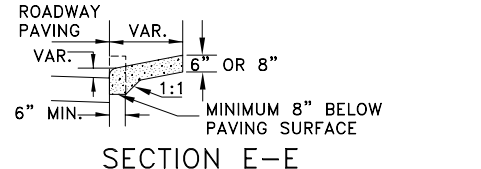
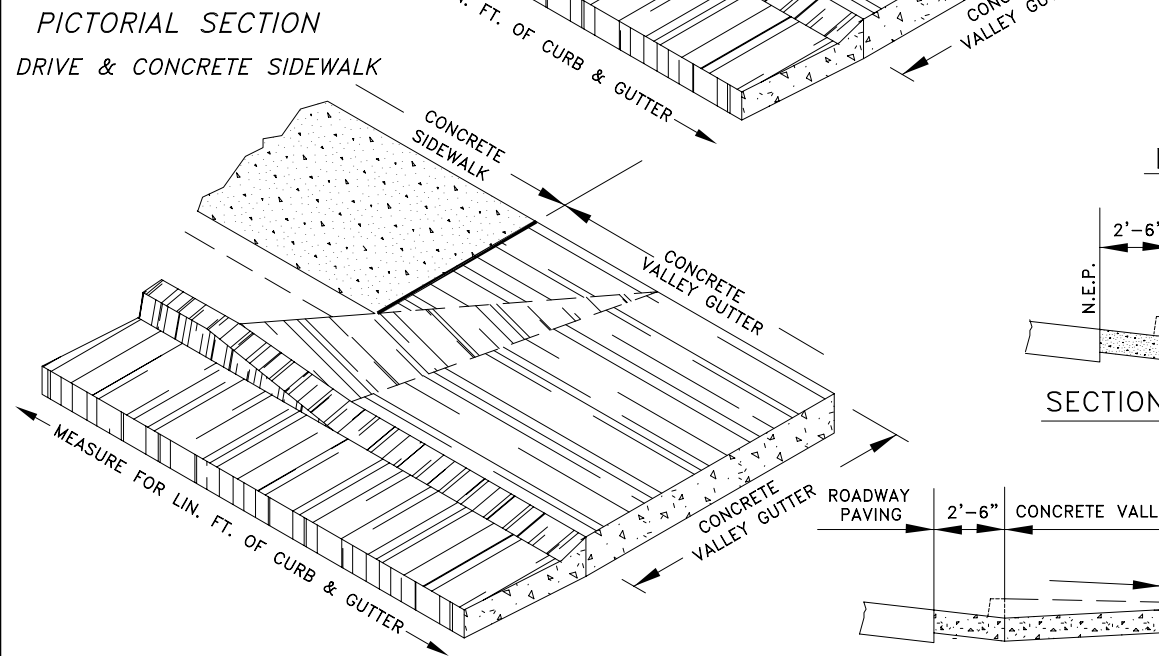
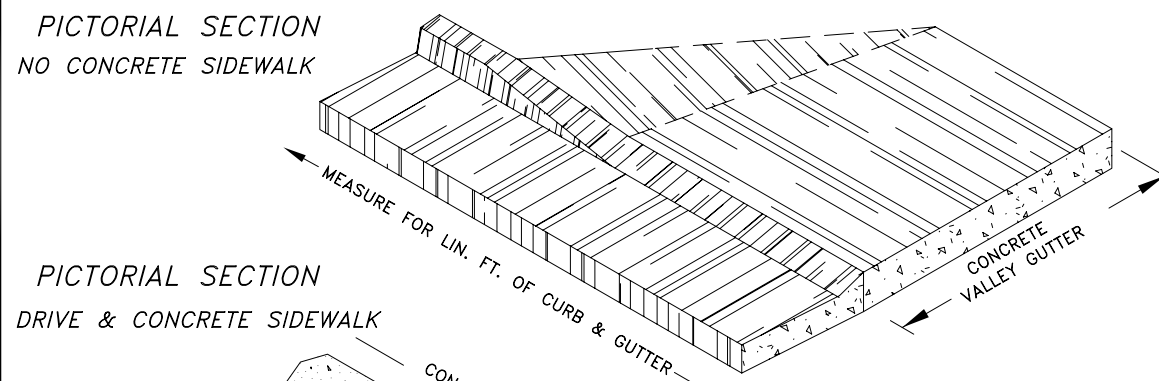
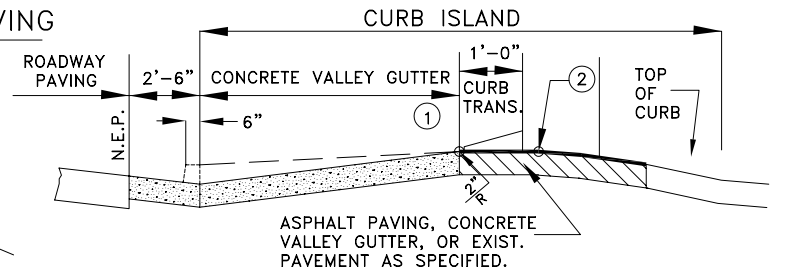
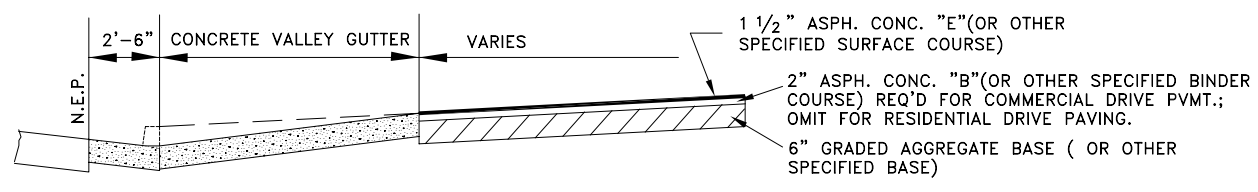


NOTE: DETAILS NOT SHOWN FOR MODIFIED DRIVES WILL BE SIMILAR TO THAT SHOWN FOR STANDARD DRIVEWAYS AT BOTTOM.

NOTE: DRIVES WILL NOT BE SKEWED EXCEPT WHERE PERPENDICULAR ALIGNMENT IS NOT FEASIBLE. TRIANGLE ABC (SHOWN) OCCURS ONLY WITH CONCRETE SIDEWALK.

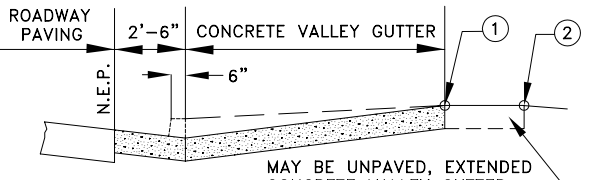


DRIVE MODIFIED FOR HEADER OR INTEGRAL CURB ALONG ROADWAY

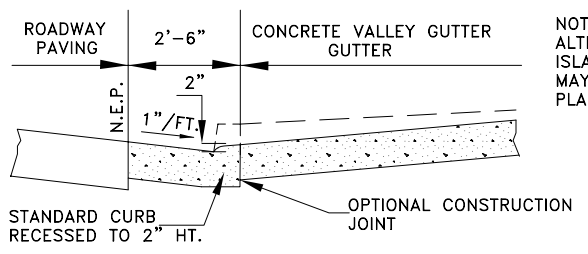


SECTION C-C

(SPECIAL CASE FOR SECTION B-B OR SECTION C-C) WHERE THE SHOULDER OR SIDEWALK IS SLOPED AWAY FROM THE ROAD.

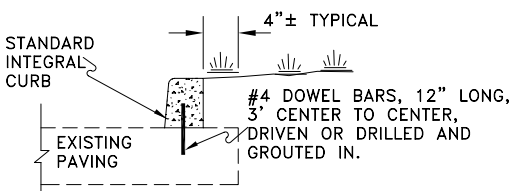


SECTION B-B



SECTION THRU GUTTER

NOTE: ALTERNATES TO THE CURB ISLAND DESIGN SHOWN HERE MAY BE SPECIFIED IN THE PLANS.



DOWELED CURB DETAIL

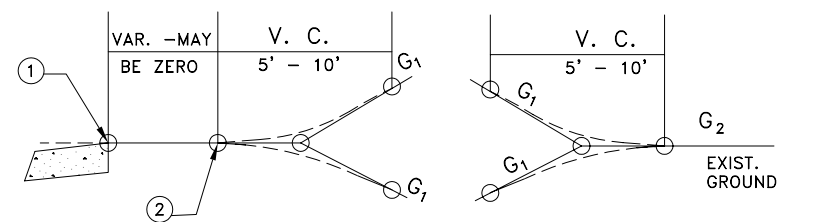
① IS THE BACK OF SIDEWALK SURFACE GRADE OR THE SHOULDER SURFACE GRADE 7'-0" BACK OF THE CURB. SLOPE OF THE CONCRETE VALLEY GUTTER WILL BE SUCH THAT THE BACK OF THE DRIVEWAY TAPER WILL BE THE SAME ELEVATION.

② IS BEGINNING OF DRIVEWAY TIE-IN.

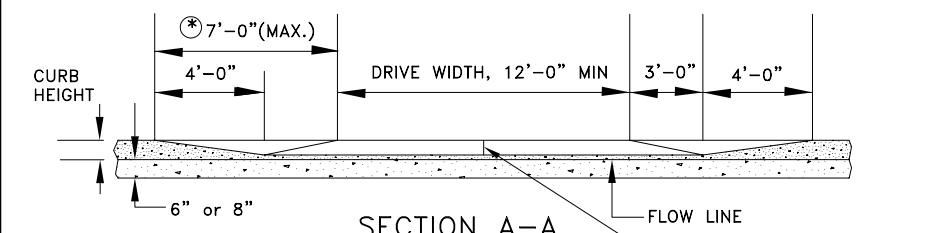
① AND ② MAY COINCIDE

V.C.	MAXIMUM G ₁	
	CUT	FILL
5'	27%	16.67%
10'	28%	27%

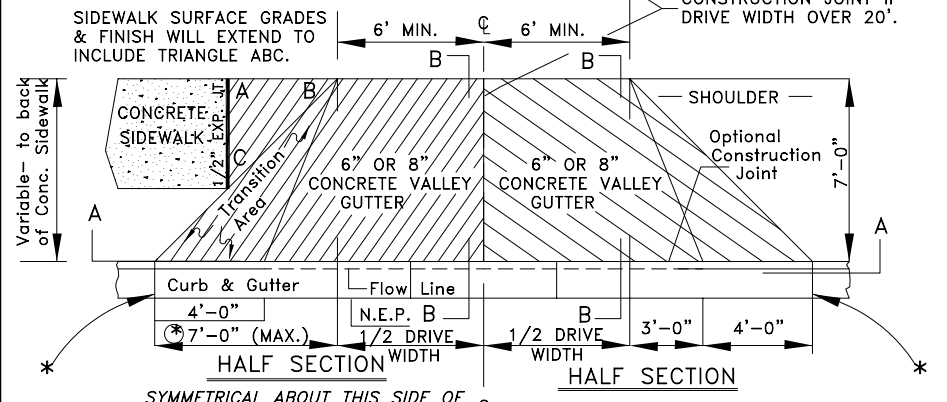
V.C.	G ₁ - G ₂ (MAX. ALGEBRAIC GRADE CHANGE)	
	SAG	CREST
5'	21%	25%
10'	25%	36%



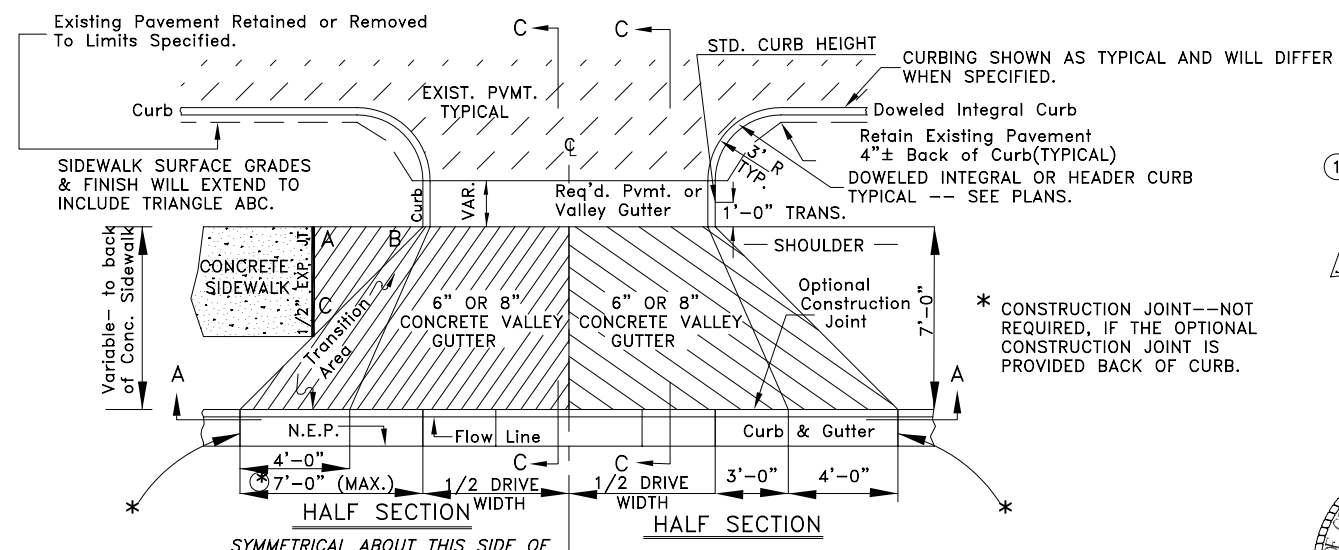
MAXIMUM DRIVEWAY GRADES (SEE GENERAL NOTE NO. 7)



SECTION A-A



PLAN RESIDENTIAL, COMMERCIAL OR NON-COMMERCIAL DRIVE



PLAN DRIVEWAY WITH CURB ISLAND (Shown Adjacent To Existing Paved Parking Area)



GWINNETT COUNTY DEPARTMENT OF PLANNING & DEVELOPMENT STANDARD DRAWING

Driveways with Tapered Entrances Concrete Valley Gutters

DATE: JANUARY 1988

SHEET: 314