



Planning Commission Public Hearing Agenda

Tuesday, April 14, 2026 at 7:00pm

75 Langley Drive, Lawrenceville, Georgia 30046

- A. Call To Order, Moment of Silence, Pledge to Flag
- B. Opening Remarks by Chairman and Rules of Order
- C. Approval of Agenda
- D. Approval of Minutes:
 - March 3, 2026
- E. Announcements
- F. New Business

1. Case Number: **REZ2026-00007**
 Applicant: Sullins Engineering
 Contact: Jakob Sullins
 Phone Number: 706.773.3849
 Zoning Change: C-2 to M-1
 Location: 833 Hurricane Shoals Road
 Map Number: R5208 004
 Acreage: 3.01
 Square Footage: 41,000
 Proposed Development: Warehouse
 Additional Requests: Variances
 Commission District: (4) Holtkamp
 Department Recommendation: **APPROVAL WITH CONDITIONS**

2. Case Number: **REZ2026-00011**
 Applicant: Jupiter Development, Inc.
 Contact: c/o Mahaffey Pickens Tucker, LLP
 Gabrielle Schaller
 Phone Number: 770.232.0000
 Zoning Change: HRR to C-2
 Location: 4400 Satellite Boulevard
 Map Number: R6210 034
 Acreage: 2.04
 Square Footage: 24,841
 Proposed Development: Shopping Center
 Commission District: (1) Carden
 Department Recommendation: **APPROVAL WITH CONDITIONS**

3. Case Number: **REZ2026-00013**
Applicant: DP2 Holdings, LLC
Contact: Jakob Sullins
Phone Number: 706.773.3849
Zoning Change: RA-200 to R-100
Location: 1790 Whitley Road
Map Number: R5358 001
Acreage: 13.14
Units: 6
Proposed Development: Single-Family Detached Subdivision
Commission District: (3) Watkins
Department Recommendation: **APPROVAL WITH CONDITIONS**
4. Case Number: **REZ2026-00015**
Applicant: One Street Residential, LLC
c/o Mahaffey Pickens Tucker, LLP
Contact: Shane Lanham
Phone Number: 770.232.0000
Zoning Change: C-2 to R-IF
Location: 3175 Loganville Highway
Map Number: R5157 341
Acreage: 16.78
Units: 139
Proposed Development: Mixed-Residential Development (Age-Restricted)
Additional Requests: Variances
Commission District: (3) Watkins
Department Recommendation: **APPROVAL WITH CONDITIONS**
5. Case Number: **REZ2026-00016**
Applicant: Simes Signature Properties, Inc.
c/o Mahaffey Pickens Tucker, LLP
Contact: Gabrielle Schaller
Phone Number: 770.232.0000
Zoning Change: RA-200 to OSC
Location: 2538 and 2570 Kilgore Road
Map Numbers: R7184 005 and 014
Acreage: 30.28
Units: 75
Proposed Development: Single-Family Detached Subdivision
Commission District: (4) Holtkamp
Department Recommendation: **APPROVAL WITH CONDITIONS**

6. Case Number:	REZ2026-00017
Applicant:	Balsam Green, LLC c/o Mahaffey Pickens Tucker, LLP
Contact:	Shane Lanham
Phone Number:	770.232.0000
Zoning Change:	RA-200 to MRR
Location:	2730 McGinnis Ferry Road
Map Number:	R7152 002
Acreage:	6.45
Units:	174
Proposed Development:	Apartments
Additional Requests:	Variance and Waiver
Commission District:	(4) Holtkamp
Department Recommendation:	APPROVAL WITH CONDITIONS

G. Audience Comments

H. Committee Reports

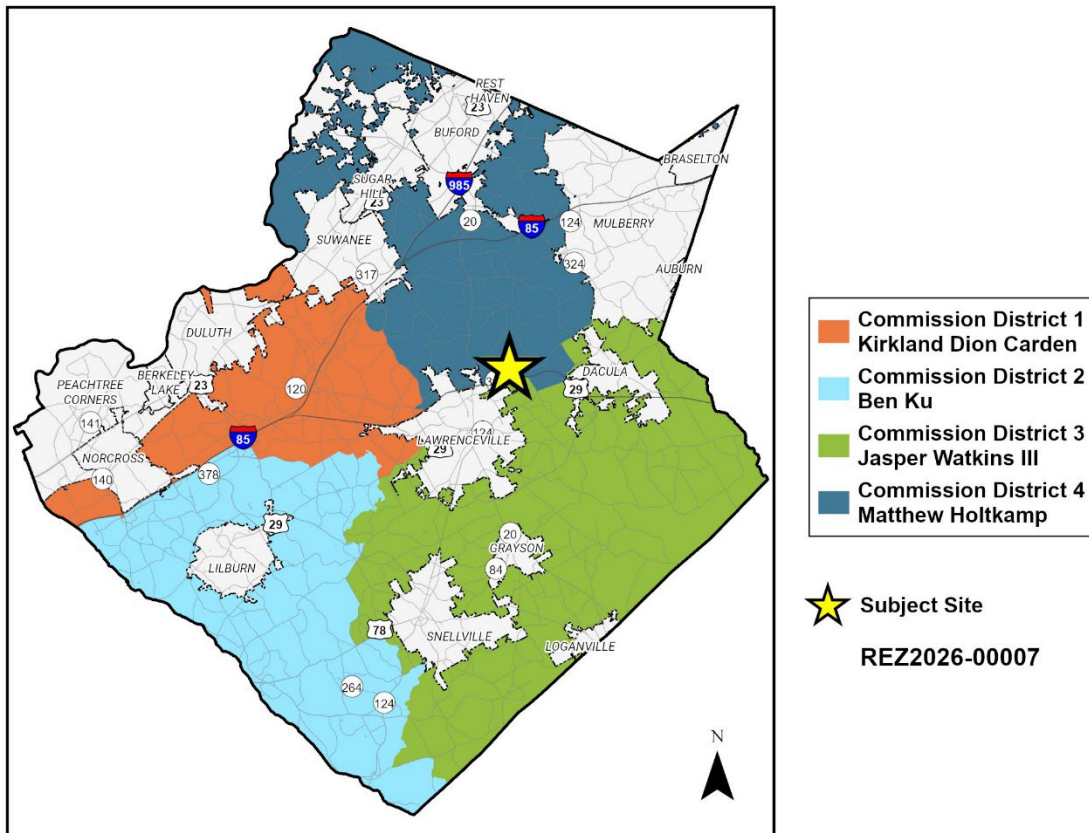
I. Comments By Staff and Planning Commission

J. Adjournment

PLANNING AND DEVELOPMENT DEPARTMENT CASE REPORT

Case Number: REZ2026-00007
Current Zoning: C-2 (General Business District)
Request: Rezoning to M-1 (Light Industry District)
Additional Requests: Variances
Address: 833 Hurricane Shoals Road
Map Number: R5208 004
Site Area: 3.01 acres
Square Feet: 41,000
Proposed Development: Warehouse
Commission District: District 4 – Commissioner Holtkamp
Future Development Type: Workplace Center – Light

Staff Recommendation: APPROVAL WITH CONDITIONS



Applicant: Sullins Engineering
302 West May Street
Winder, GA 30680

Owner: International Complete Services Inc.
833 Hurricane Shoals Road
Lawrenceville, GA 30680

Contact: Jakob Sullins

Phone: 706.773.3849

Zoning History

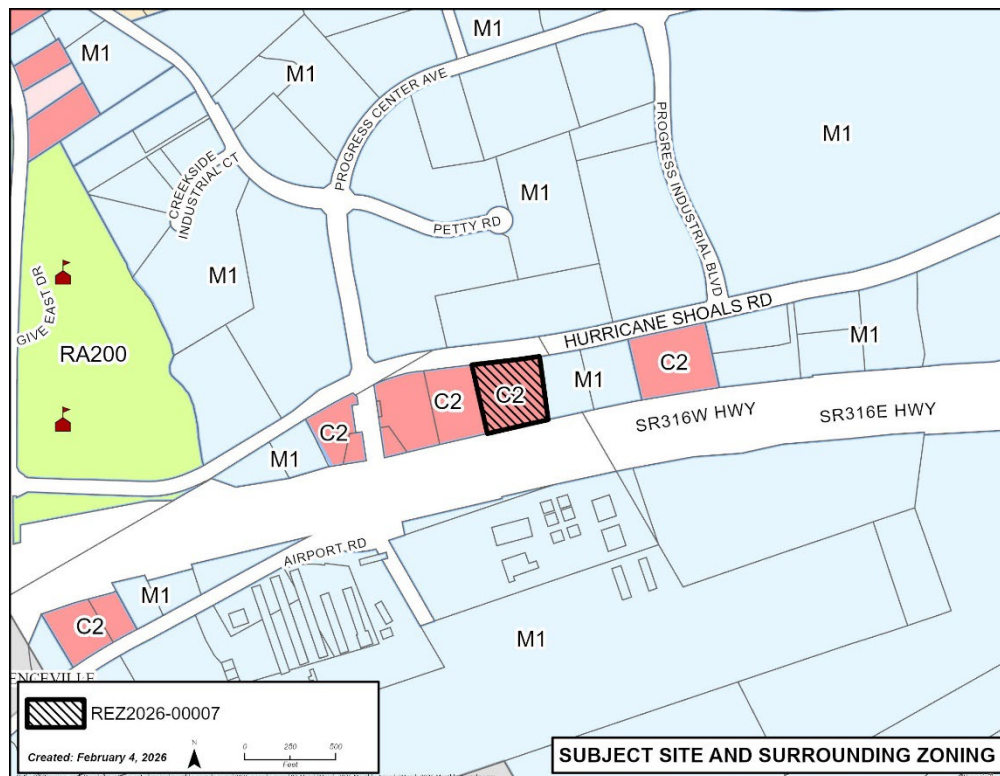
The subject property is zoned C-2 (General Business District). In 1985, the property was included in a larger rezoning from R-100 (Single-Family Residence District), O-I (Office-Institutional District), and M-1 (Light Industry District) to C-2, pursuant to RZ-134-85.

Existing Site Condition

The subject property is a 3.01-acre parcel located along Hurricane Shoals Road, east of its intersection with Progress Center Avenue. The property is undeveloped with dense vegetation. There is no sidewalk along the road frontage. The nearest Ride Gwinnett transit stop is approximately 2.3 miles from the subject property.

Surrounding Use and Zoning

The property is located in the Gwinnett Progress Center Business Park and is surrounded by light industrial and office uses. An undeveloped light industrial parcel is to the north, across Hurricane Shoals Road. Contractor offices with outdoor storage are located to the east. The Gwinnett County Airport is to the south, across University Parkway (SR 316). An office is located to the west. The following is a summary of surrounding uses and zoning:



Location	Land Use	Zoning
Proposed	Warehouses	M-1
North	Light Industrial (undeveloped)	M-1
East	Light Industrial (contractor offices)	M-1
South	Gwinnett County Airport	M-1
West	Commercial (office)	C-2

Project Summary

The applicant requests rezoning from C-2 to M-1 for warehouses, including:

- Three, one-story buildings with 24 tenant suites totaling 41,000 square feet, yielding a floor area ratio (FAR) of 0.31.
- Anticipated users include small businesses, service providers, and individuals for personal storage or hobby space, with all operations conducted indoors.
- Exterior building materials:
 - Buildings A and B: metal siding with brick accents on all sides.
 - Building C: metal siding with brick accents on the front and side façades, and brick on the rear façade facing SR 316.
 - Each suite includes an overhead bay door.
- 78 vehicle parking spaces including one EV charging station, and four bike parking spaces.
- A stormwater management facility located in the rear of the development.
- A 30-foot-wide landscaped buffer along the western property line and a 10-foot-wide landscape strip along Hurricane Shoals Road and SR 316.
- One full-access entrance with a deceleration lane and a 5-foot-wide sidewalk along Hurricane Shoals Road.

Zoning and Development Standards

The following is a summary of applicable standards from the Unified Development Ordinance (UDO):

Standard	Required	Proposed	Meets Standard?
FAR	Maximum 1.0	0.31	YES
Lot Size	Minimum 1 acre	3.01	YES
Lot Width	Minimum 150'	Minimum 150'	YES
Lot Coverage	Maximum 80%	67%	YES
Front Setback	Minimum 50'	Minimum 50'	YES
Side Setback	Minimum 25'	Minimum 25'	YES
Rear Setback	Minimum 50'	Minimum 50'	YES
Building Height	Maximum 45'	Maximum 45'	YES
Parking	Minimum 14 spaces Maximum 27 spaces	78 spaces	NO*
Loading Spaces	3	0	NO*
Zoning Buffer	30' Undisturbed	30' Graded and Replanted	NO*

* The applicant is requesting variances from these standards.

Public Participation

The applicant held a community meeting on February 12, 2026, at Rabbit Hill Park in Dacula. There were no community members in attendance.

Staff Analysis

Rezoning Request Analysis: The standards governing the exercise of zoning power are outlined in Section 270-20.6 of the UDO. Staff makes the following findings related to the zoning requests:

A. Whether the proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

The surrounding area contains a mixture of light industrial and office uses. The proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

B. Whether the proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property.

The proposed warehouses are located within an existing light industrial business park. Staff has recommended conditions to ensure compatibility with existing development, including requiring the front façades of Buildings A and B to be at least 50 percent brick, enhanced landscaping along Hurricane Shoals Road, and that all business activities occur indoors. With proper conditions, the proposed rezoning will not adversely affect the existing use or usability of adjacent or nearby property.

C. Whether the property to be affected by the proposed rezoning has a reasonable economic use as currently zoned.

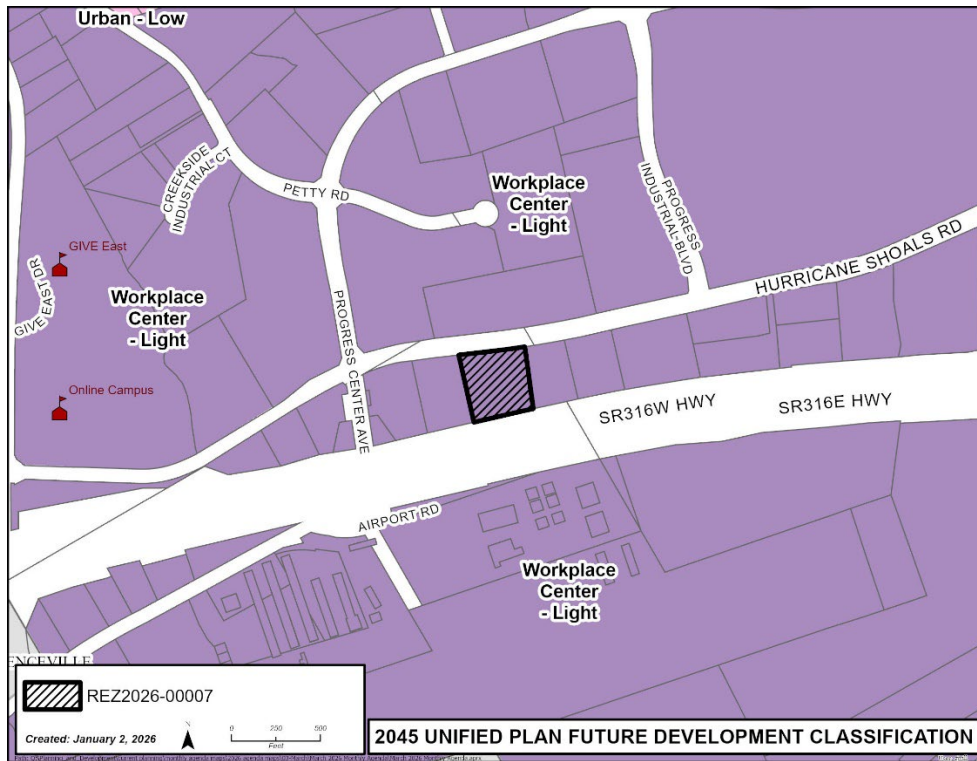
The property has a reasonable economic use as currently zoned.

D. Whether the proposed rezoning will result in a use which will or could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools.

An increase in impacts on public facilities would be anticipated in the form of traffic, utility demand, and stormwater runoff; however, these impacts would be mitigated with appropriate conditions, site development requirements, and planning. The traffic impact study estimated that the warehouses would generate 20 morning and 20 evening peak hour trips. The traffic impact study did not recommend any roadway improvements. No impact is anticipated on school enrollment.

E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map; and

The Unified Plan's Future Development Map indicates "Workplace Center – Light" for the subject property, which envisions non-residential uses, including flex/light industrial, office, and public/institutional. Warehouses are a recommended land use and M-1 is a recommended zoning district. The proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map.



F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning.

There are no other conditions giving support for approval or disapproval of the proposal.

Variance Requests and Analyses:

The applicant is seeking variances from the following provisions of the UDO:

1. **Section 230-100.1.D Zoning Buffer.** The applicant is requesting to replace the 30-foot-wide zoning buffer along the western property line with a landscaped buffer.
Analysis: The applicant is proposing to grade and replant the area to buffer standards. This request meets the spirit of the UDO while allowing for development of the property.

2. **Section 240-20.2 Maximum Parking Requirements.** The applicant is requesting to increase the maximum parking from 27 to 78 spaces.
Analysis: The proposal includes 24 tenant suites which supports the need for additional parking. Approval of the variance request would not nullify the intent of the UDO.

3. **Section 240-120.3 Off-Street Loading Standards.** The applicant is requesting to eliminate the required three loading spaces.
Analysis: Eliminating all three required loading spaces could cause vehicular circulation issues during deliveries. Staff has recommended a condition to construct one loading space. With proper conditions, approval of the variance request would not nullify the intent of the UDO.

Staff Recommendation

Based on the staff's evaluation of the request and the standards governing the exercise of zoning power, the Department of Planning and Development recommends **APPROVAL WITH CONDITIONS** of the rezoning request and **APPROVAL** of the following variances:

1. To replace the zoning buffer along the western property line with a landscaped buffer.
2. To increase the maximum parking from 27 to 78 spaces.
3. To eliminate the required loading spaces.

Staff Recommended Conditions

Approval as M-1 (Light Industry District) for warehouses, subject to the following conditions:

1. The property shall be limited to uses allowed in M-1, not exceeding 41,000 square feet.
2. The proposed development shall be constructed in general conformance with Exhibit B: Site Plan dated received January 20, 2026, and Exhibit C: Building Elevations dated received March 17, 2026, with revisions required by zoning conditions and the Unified Development Ordinance, subject to the review and approval of the Department of Planning and Development.
3. All business activities including storage shall take place indoors.
4. The front façades of Buildings A and B shall contain at least 50 percent brick.
5. One loading space shall be provided on-site.
6. The 10-foot-wide landscape strip along Hurricane Shoals Road and the 30-foot-wide landscaped buffer along the western property line shall be planted to buffer standards, prior to the issuance of a Certificate of Occupancy, subject to the review and approval of the Department of Planning and Development.
7. A full-access entrance, deceleration lane, and a 5-foot-wide sidewalk shall be provided along Hurricane Shoals Road, prior to the issuance of a Certificate of Occupancy, subject to the review and approval of the Gwinnett County Department of Transportation.

Exhibits:

- A. Site Visit Photos
- B. Site Plan
- C. Building Elevations
- D. Letter of Intent and Applicant's Response to Standards
- E. Application and Disclosure of Campaign Contributions
- F. DWR Sewer Capacity Certification Letter
- G. Traffic Impact Study
- H. Public Participation Plan and Report
- I. Internal and External Agency Review Comments
- J. Maps

Exhibit A: Site Visit Photos



Hurricane Shoals Road frontage



Contractor's office to the east



Contractor's office to the east



Office building to the west

Exhibit B: Site Plan

[attached]

GWINNETT COUNTY
PLANNING AND DEVELOPMENT

RECEIVED

1.20.2026

FLOOD NOTE:

- FLOOD HAZARD AREA DOES NOT EXIST ON SITE AS PER F.I.R.M. COMMUNITY PANEL No. 13135C0074F, DATED 9/29/2006

BUILDING SETBACKS:

- FRONT: 50' FROM R/W OF ROADS
- SIDE: 25' FROM PROPERTY LINES
- REAR: 50' FROM PROPERTY LINES

GENERAL NOTES:

- CURRENT ZONING: C-2
- TO BE REZONED: M-1
- WORKPLACE CENTER - LIGHT FUTURE DEVELOPMENT AREA
- 3,005 ACRE LOT SIZE
- MAX 1.0 FAR
- MAX 80% LOT COVERAGE (PROPOSED 68.8% COVERAGE, 2.03/3.05AC)
- DOMESTIC WATER SUPPLY TO BE GWINNETT COUNTY
- SANITARY SEWER TO BE GWINNETT COUNTY

SURVEY:

- BOUNDARY REFERENCED FROM RETRACEMENT & TOPOGRAPHIC SURVEY FOR "STARK PROPERTIES" BY GEORGIA PREMIER LAND SERVICES, INC. DATED 10/28/25

FAR:

- TOTAL LOT AREA: 3,005 ACRES
- TOTAL PROPOSED BUILDING COVERAGE: 41,000SF
- TOTAL FAR: (41,000) / (3,005*43,560) = 0.313

PARKING:

- TOTAL MINIMUM PARKING REQUIRED: 41,000/3,000 = 13.67
- TOTAL MAXIMUM PARKING REQUIRED: 41,000/1,500 = 27.33
- TOTAL PARKING SPACES PROVIDED: 41,000/526 = 78
- PARKING SPACES PER SF = 41,000/78 - TOTAL 1 SPACE PER 526SF

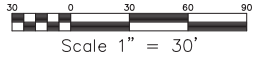
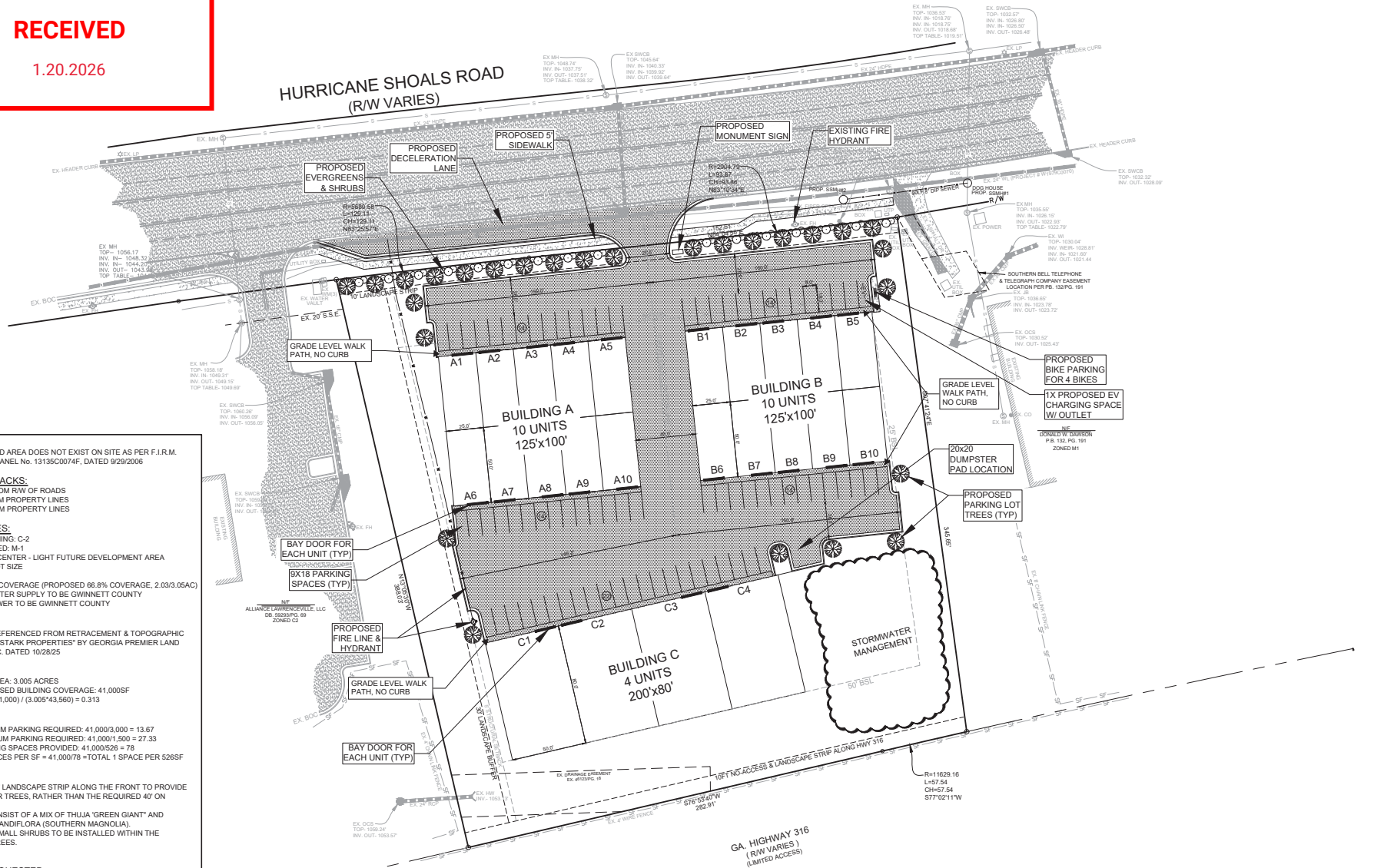
LANDSCAPING:

- ENHANCED 10' LANDSCAPE STRIP ALONG THE FRONT TO PROVIDE 20' ON CENTER TREES, RATHER THAN THE REQUIRED 40' ON CENTER.
- TREES TO CONSIST OF A MIX OF THUJA 'GREEN GIANT' AND MAGNOLIA GRANDIFLORA (SOUTHERN MAGNOLIA).
- VARIETY OF SMALL SHRUBS TO BE INSTALLED WITHIN THE PROPOSED TREES.

VARIANCES REQUESTED:

- Variance from UDO Section 230-100.1(B) to eliminate the required 30 foot zoning buffer, and to provide a 30 foot landscaped buffer that may be graded and replanted to meet the permanent buffer screening standards of UDO Section 610.20 upon completion of construction.
- Variance to Table 240.1 to increase the maximum number of parking spaces allowed, permitting a total of 78 parking spaces as shown on the concept plan.
- Variance from UDO Sections 240-120 to modify the off-street loading requirement so that required loading activity can occur inside the individual suites at the overhead bay doors, rather than in separate exterior loading bays.

HURRICANE SHOALS ROAD
(R/W VARIES)



REVISION:

ALL UTILITIES, CONDUITS, AND MANHOLES ON THIS PROJECT SHALL BE AS SHOWN ON THE ATTACHED UTILITY DRAWINGS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL UTILITY DEPTHS, LOCATIONS, AND TYPES. ANY CHANGES TO UTILITIES SHALL BE MADE PRIOR TO CONSTRUCTION AND SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND EASEMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND EASEMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND EASEMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND EASEMENTS.

26 EAST ATLANTA STREET
WINDY, GA. 30308
PHONE: (404) 887-6719
Sullins Engineering, LLC
CONTACT: MATTHEW SULLINS
CIVIL ENGINEERS - LAND PLANNERS

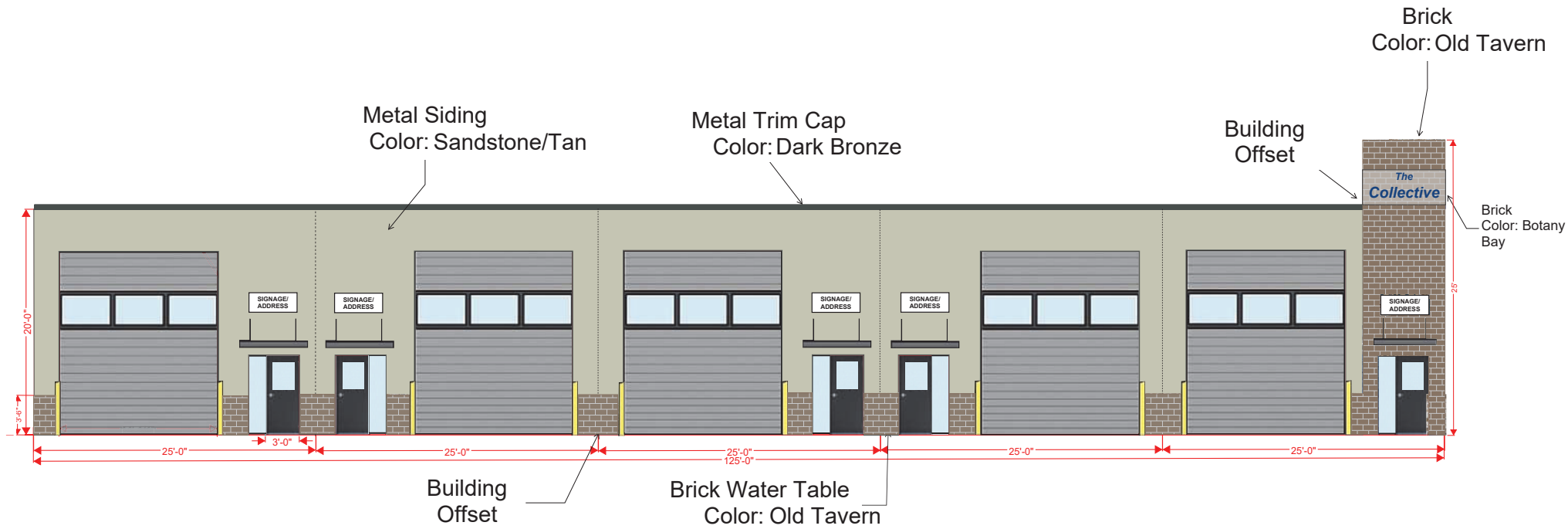
RESUBMIT PLAN FOR:
833 HURRICANE SHOALS ROAD
LOCATED IN:
PARCEL 5208/004
GWINNETT COUNTY, GEORGIA

DATE: 1/13/2026 JOB #: 4021
SHEET 1

Exhibit C: Building Elevations

[attached]

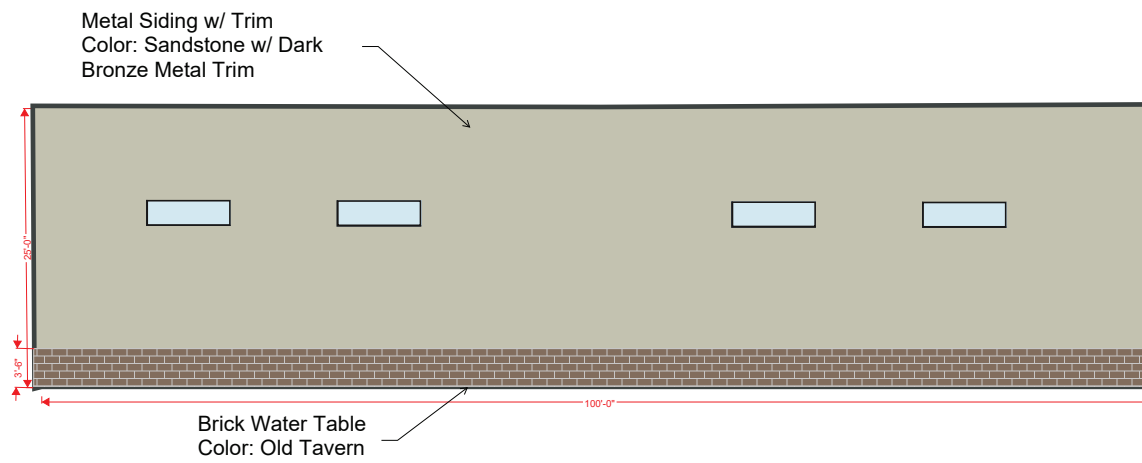
Building A & B: Front (north) and Rear (south) Elevations



RECEIVED

3/17/2026

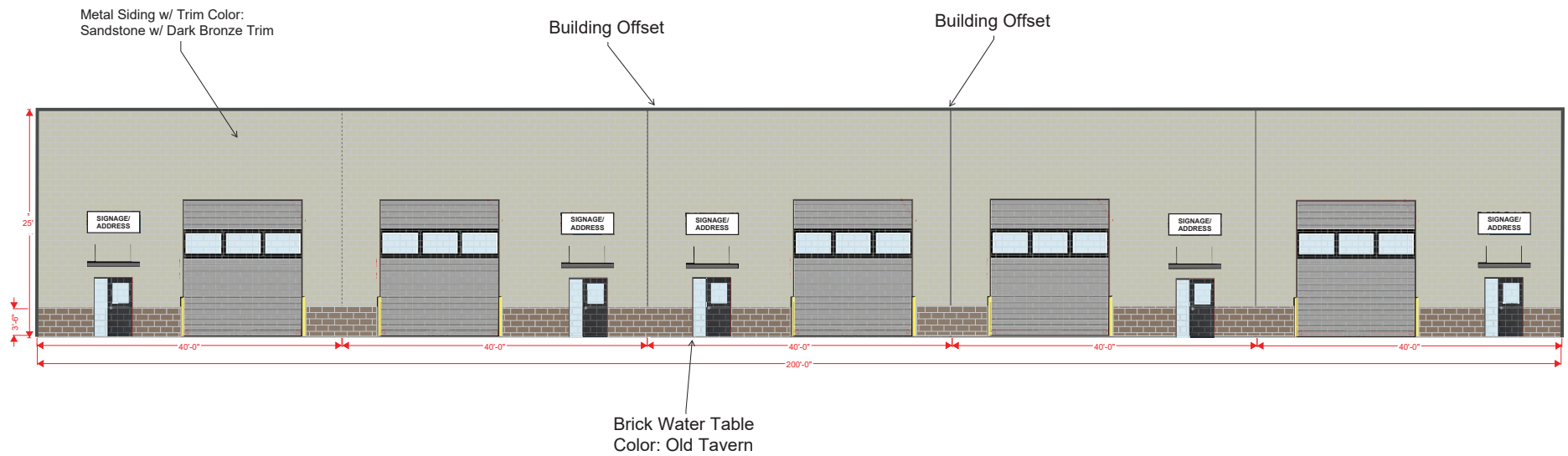
Building A & B: Side (east & west) Elevation



RECEIVED

3/17/2026

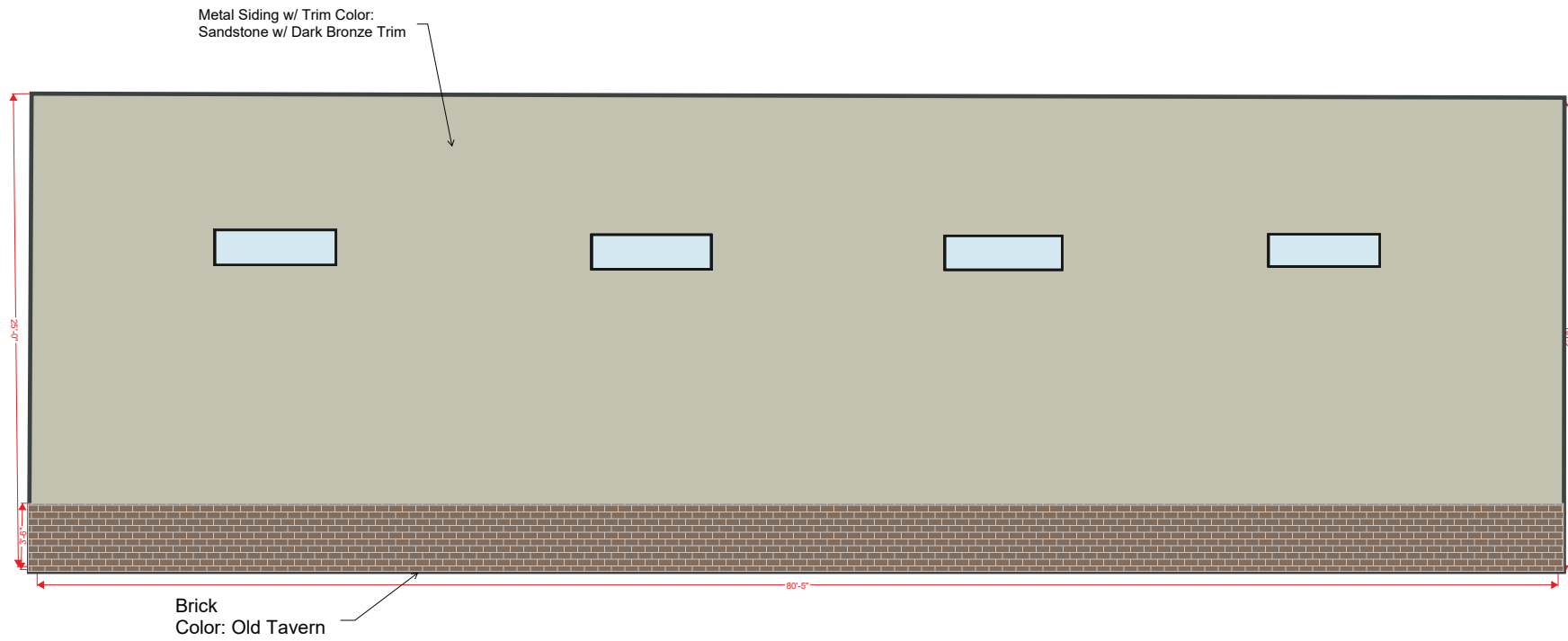
Building C: Front (north) Elevation



RECEIVED

3/17/2026

Building C: Side (east & west) Elevation



RECEIVED

3/17/2026

Building C: Rear (south) Elevation

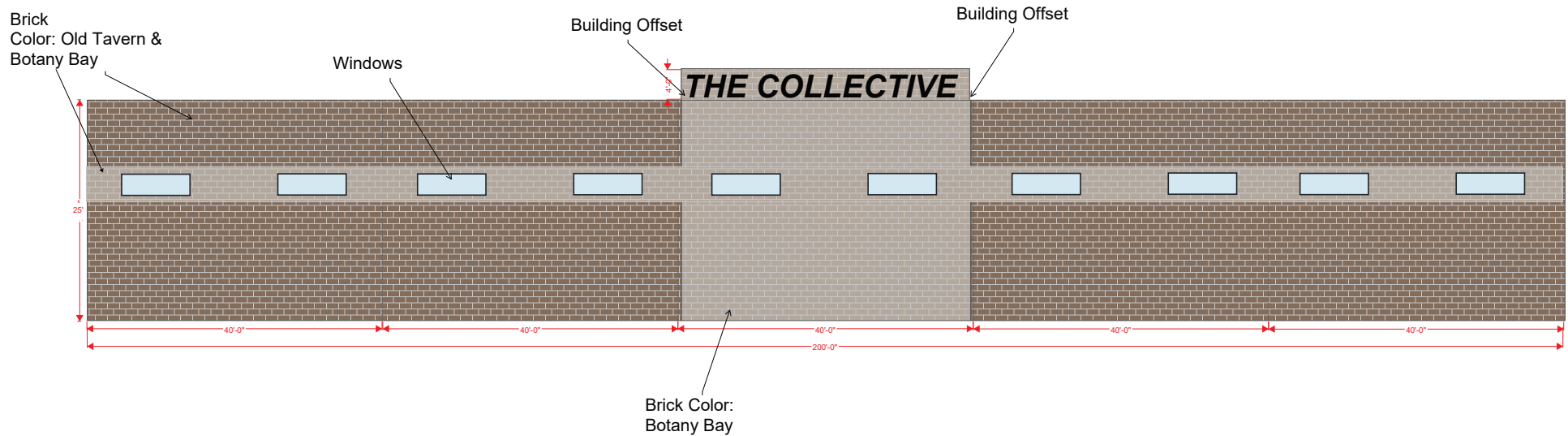


Exhibit D: Letter of Intent and Applicant's Response to Standards

[attached]



26 East Athens Street, Winder, GA 30680 * mattsullins@gmail.com * 678-687-6219

December 13, 2025

RE: Application for Rezone & Concurrent Variances for Collective at 316

Letter of Intent

On behalf of the property owner, International Complete Services Inc, and the contract purchaser, Stark Properties, I am submitting this Letter of Intent to formally request approval for the rezoning and concurrent variances for the property located at 833 Hurricane Shoals Road, identified as Tax Map and Parcel 5208 004. The property contains approximately 3.005 acres, is currently zoned C 2 (General Business District), and lies within the Workplace Center Light future development area as designated by Gwinnett County. The site is also part of the Business Progress Center, a larger commercial and industrial business park along the SR 316 corridor.

The request includes:

- Rezoning approximately 3.005 acres from C-2 to M-1;
- Variance from UDO Section 230-100.1(B) to eliminate the required 30 foot zoning buffer, and to provide a 30 foot landscaped buffer that may be graded and replanted to meet the permanent buffer screening standards of UDO Section 610 20 upon completion of construction.
- Variance to Table 240.1 to increase the maximum number of parking spaces allowed, permitting a total of 78 parking spaces as shown on the concept plan.

- Variance from UDO Sections 240-120 to modify the off-street loading requirement so that required loading activity can occur inside the individual suites at the overhead bay doors, rather than in separate exterior loading bays.

This application seeks to rezone the property to allow development of The Collective at 316, a high quality small bay light industrial and warehouse project. As shown on the concept plan, the proposal includes three one story buildings totaling approximately 41,000 square feet. Building A and Building B will each contain ten suites, for a total of twenty suites in these two buildings, with each suite being approximately 1,250 square feet in size (twenty-five feet by fifty feet). Building C will contain four larger suites, each approximately 4,000 square feet in size (eighty feet by fifty feet). In total, the project will provide twenty-four individual suites, and each suite will have its own overhead bay door, for a total of twenty-four bay doors across the development. Anticipated users include local small businesses, trades, service providers, and personal storage or hobby space, with all operations conducted indoors.

The scale and character of the proposed development are similar to the small bay project located a few doors down at 1000 Hurricane Shoals Road within the same Business Progress Center. Like that project, Collective at 316 is intended to serve small businesses and entrepreneurs that benefit from a combination of office space and individual bay doors in a clean, modern business park setting.

Access to the site will be provided from Hurricane Shoals Road via a single driveway with required turn lane improvements, internal drive aisles, and onsite parking and loading. The site geometry and drive aisles are designed to accommodate passenger vehicles, service vans, and small box trucks. The site is not designed for circulation or loading by eighteen-wheel tractor trailer trucks, and no such truck traffic is anticipated or proposed. All loading and deliveries will occur at the individual bay doors serving each suite, and any short-term staging of delivery vehicles will occur within the suites themselves. No exterior loading docks or outdoor loading areas are proposed.

For the 41,000 square feet of small bay space, the UDO parking standards for warehouse use establish a parking range based on floor area. The required minimum parking is 1 space per 1,500 square feet, which equals 41,000 divided by 1,500 equals 27.33, rounded up to 28 spaces. The required maximum parking is 1 space per 3,000 square feet, which equals 41,000 divided by 3,000 equals 13.67, rounded up to 14 spaces. This higher parking provision is intended to accommodate the combined parking needs of multiple small tenants, visitors, and small service vehicles, as well as occasional personal storage users, while avoiding spillover parking onto Hurricane Shoals Road or adjacent properties. For this reason, a variance to Table 240.1 is requested to increase the maximum number of parking spaces allowed to permit 78 parking spaces, while still meeting or exceeding the minimum required parking.

To address the parking lot tree requirements in UDO Section 620-30, the plan provides ten interior parking lot trees within and adjacent to the parking field, together with an enhanced evergreen landscape strip along Hurricane Shoals Road that contains a total of seventeen evergreen trees. For a seventy eight space parking lot, the UDO would normally require twelve parking lot trees. By providing seventeen trees associated with the parking area, the project exceeds the numerical parking lot tree requirement while concentrating many of the trees along the Hurricane Shoals Road frontage in an enhanced landscape strip. The landscape strip trees are proposed at approximately twenty feet on center, which is twice the density of the standard forty foot spacing. This approach provides continuous evergreen screening of the parking lot and buildings, improves the streetscape along Hurricane Shoals Road, and reduces the need for additional interior planting islands that would interrupt drive aisles, fire access, and on site circulation. To support this enhanced but slightly different configuration, a variance from UDO Section 620-30 is requested so that the combination of interior trees and the enhanced frontage landscape strip can satisfy the off-street surface parking lot planting requirement.

The rear of the property fronts SR 316, a major commercial and industrial corridor, and will be treated with appropriate setbacks, landscaping, and screening consistent with the development standards for this

area. The project will provide a 10-foot landscape strip along Hurricane Shoals Road and required buffers where the site adjoins other properties. Building elevations will incorporate articulated facades and durable materials, including metal siding in a sandstone tan color with dark bronze metal trim and cap, brick water tables, and brick accent elements in Old Tavern and Botany Bay colors as shown on the concept elevations. The front elevations include periodic building offsets, brick base courses, entry features with signage for The Collective, and grouped windows at the office entries to break up the mass of the buildings and create a pedestrian scale along Hurricane Shoals Road. Side elevations use the same metal siding and brick base treatments, and the elevation facing SR 316 incorporates brick in Old Tavern and Botany Bay colors with windows to provide visual interest toward the highway. These materials and facade treatments ensure compatibility with surrounding commercial and industrial development within the Business Progress Center and present a cohesive architectural theme for the project.

In conjunction with this rezoning and variance application, the applicant agrees to the following condition of zoning:

- 1. The front 10-foot landscape strip along Hurricane Shoals Road shall comply with the Type 2 10-foot landscape strip requirements and shall be enhanced with evergreen trees planted 20 feet on center at a minimum height of 6 feet at the time of planting, rather than the standard 40-foot spacing.**

Legal Rights Reservation

This letter is submitted without prejudice to any legal or equitable rights that the property owner may have. By submitting this application, the property owner does not waive any rights to seek further remedies, administrative appeals, or legal relief under applicable law should the rezone and variances be denied. All rights are expressly reserved.

Conclusion

The proposed development is in alignment with the spirit and intent of the County ordinances, the Workplace Center Light future development designation, and the established commercial and industrial character of the Business Progress Center and SR 316 corridor. We appreciate your consideration of this request and respectfully ask for your approval of the rezoning and concurrent variance applications. Should you require additional information or have any questions, please do not hesitate to contact me.

Sincerely,

Matthew Sullins



GA Registration No. 28445

Pursuant to requirements of the United Development Ordinance, the Board of Commissioners finds that the following standards are relevant in balancing the interest in promoting the public health, safety, morality, or general welfare against the right to the unrestricted use of property and shall govern the exercise of the zoning power.

- A. Whether a proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property:

The subject property is currently zoned C-2 and is surrounded almost entirely by other C-2 and M-1 properties, with the closest non-commercial/industrial zoning located roughly 1,600 feet away. The site fronts Hurricane Shoals Road and has rear frontage on SR 316, which functions as a major commercial and industrial corridor. The property is also part of the larger Business Progress Center development, a master-planned area specifically created to accommodate commercial and industrial uses. Rezoning the site to M-1 for small-bay, personal and business warehouse units is consistent with the surrounding development pattern and the established character of the Business Progress Center and SR 316 corridor.

- B. Whether a proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property:

The surrounding properties are already developed or zoned for commercial and light industrial uses, and the proposed project is compatible with those uses. Site design will incorporate the required landscape strips and buffers to visually screen the development and soften its impact on adjacent properties. Building orientation, internal drive aisles, and controlled access will keep activity internal to the site and away from property lines. As a result, the project should not adversely affect the existing use or usability of nearby properties and will instead complement the established commercial/industrial environment.

- C. Whether the property to be affected by a proposed rezoning has reasonable economic use as currently zoned:

The property has some economic potential under its existing C-2 zoning; however, its configuration between Hurricane Shoals Road and SR 316 and its location within the Business Progress Center make it better suited to light industrial and warehouse/flex uses than to traditional retail or general commercial uses. The M-1 district allows a modern, small-bay warehouse project that aligns with the Business Progress Center's purpose as a commercial/industrial business park and responds to market demand for secure, small-scale storage and workspace. Rezoning to M-1 will therefore provide a more reasonable and efficient economic use of the property than the existing C-2 zoning.

- D. Whether the proposed rezoning will result in a use which could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools:

The proposed personal/business warehouse use is a low-to-moderate traffic generator compared to many C-2 commercial uses, with most trips limited to unit owners, small businesses, and occasional visitors rather than high-turnover retail traffic. The project will provide required transportation improvements, including a deceleration lane and sidewalk along Hurricane Shoals Road, and will comply with any driveway and access management requirements. The site can be served by existing public water, sewer, and other utilities, and stormwater facilities will be designed to meet County standards. Because the project is non-residential, it will not impact local schools. For these reasons, the rezoning should not create an excessive or burdensome demand on public infrastructure.

- E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map:

The Gwinnett County Unified Plan and Future Development Map identify this property within a workplace-oriented future development area that contemplates employment uses such as light industrial, flex space, and supporting commercial. The subject property's inclusion in the Business Progress Center, a County-approved commercial and industrial park, further demonstrates that the long-term policy intent for this tract is

employment-based development rather than conventional retail. The proposed M-1 zoning and small-bay warehouse concept directly implement this vision by providing high-quality, light industrial/flex space in a cohesive, campus-style setting. Accordingly, the request is in conformity with the policy direction of the Unified Plan and its Future Development Map.

- F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning:

The subject tract is part of the established Business Progress Center, which was planned and entitled by Gwinnett County as a commercial and industrial business park. Much of the surrounding area has already developed under that framework, with similar commercial and light industrial buildings using shared access and infrastructure. The proposed project will complete the build-out of this portion of the Business Progress Center with a master-planned, architecturally cohesive development that adds tax base and employment opportunities while maintaining appropriate buffers and design standards consistent with the rest of the park. These existing conditions provide strong grounds for approval of the requested rezoning from C-2 to M-1.

Exhibit E: Application and Disclosure of Campaign Contributions

[attached]

REZONING APPLICATION

AN APPLICATION TO AMEND THE OFFICIAL ZONING MAP OF GWINNETT COUNTY, GA.

APPLICANT INFORMATION	PROPERTY OWNER INFORMATION
Name: <u>Sullins Engineering</u>	Name: <u>INTERNATIONAL COMPLETE SERVICES INC</u>
Address: <u>302 West May Street</u>	Address: <u>833 Hurricane Shoals</u>
City: <u>Winder</u>	City: <u>Lawrenceville</u>
State: <u>GA</u> ZIP: <u>30680</u>	State: <u>GA</u> ZIP: <u>30043</u>
Phone: <u>706.773.3849</u>	Phone: _____
Email: <u>jakesullins@gmail.com</u>	Email: _____
Contact Person: <u>Jakob Sullins</u> Phone: <u>706.773.3849</u>	
Contact's Email: <u>jakesullins@gmail.com</u>	
APPLICANT IS THE:	
<input checked="" type="checkbox"/> Owner's Agent <input type="checkbox"/> Property Owner <input type="checkbox"/> Contract Purchaser	
Current Zoning District(s): <u>C-2</u> Requested Zoning District: <u>M-1</u>	
Parcel Number(s): <u>5208 004</u> Acreage: <u>3.01</u>	
Property Address(es): <u>833 Hurricane Shoals Lawrenceville GA 30043</u>	
Proposed Development: <u>Warehouses with offices</u>	
Variance(s): <u>230-100.1B, 240.1, 240-120</u> Waiver(s): _____	
<p style="text-align: center;">RESIDENTIAL DEVELOPMENT</p> No. of Dwelling Units: _____ Dwelling Unit Sq. Ft.: _____ Density: _____ Floor Area Ratio (LRR, MRR, HRR): _____	<p style="text-align: center;">NON-RESIDENTIAL DEVELOPMENT</p> No. of Buildings: <u>3</u> Total Building Sq. Ft.: <u>41,000</u> Floor Area Ratio: <u>.31</u>
MIXED-USE DEVELOPMENT	
No. of Dwelling Units: _____ Dwelling Unit Sq. Ft.: _____	
Total Non-Residential Sq. Ft.: _____ Floor Area Ratio: _____	

REZONING PROPERTY OWNER'S CERTIFICATION

The undersigned below, or as attached, is the owner of the property considered in this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon within 12 months from the date of last action by the Board of Commissioners unless waived by the Board of Commissioners. In no case shall an application or reapplication be acted upon in less than six months from the date of last action by the Board of Commissioners.

International Complete Services, Inc.

Signature of applicant

12/10/2025

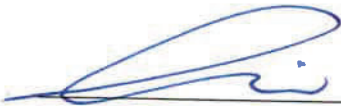
Date

Tony HAKSOO Kim, President

Type or print name and title

12/10/2025

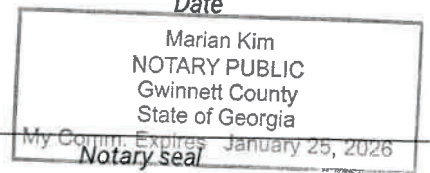
Date



Signature of notary public

12/10/2025

Date





Gwinnett

GWINNETT COUNTY
PLANNING AND DEVELOPMENT
PROPERTY TAX VERIFICATION

The undersigned certifies that all Gwinnett County property taxes are current. In no such case shall an application or reapplication be processed without such property tax verification.

Applicant Information

Name: Stark Properties

Signature: [Handwritten Signature]

Date: 12/09/25

Property Information

Enter all parcel IDs in the table below. Search for Parcel ID

Parcel ID(s) (by Applicant)	Property Taxes Paid (Yes or No) (Tax Commissioner Use Only)
PS208 004	2025 - YES.

Tax Commissioner Affirmation

All Gwinnett County property taxes are current for the above referenced parcel(s) and confirmed by the signature below.

Name: Marche E.

Title: TSA

Signature: [Handwritten Signature]

Date: 12/09/25



Disclosure of campaign contributions aggregating to \$250.00 or more to a member of the Board of Commissioners is required pursuant to Section 270-25.E. of the Unified Development Ordinance.

Name: Sullins Engineering

Relationship to Application (Applicant or Representative of Applicant): Representative of applicant

Campaign Contributions? NO

Contributions:

Official Name	Contribution Amount	Date of Contribution	Contributor



Disclosure of campaign contributions aggregating to \$250.00 or more to a member of the Board of Commissioners is required pursuant to Section 270-25.E. of the Unified Development Ordinance.

Name: Stark Properties

Relationship to Application (Applicant or Representative of Applicant): Applicant

Campaign Contributions? NO

Contributions:

Official Name	Contribution Amount	Date of Contribution	Contributor

Exhibit F: DWR Sewer Capacity Certification Letter

[attached]



December 10, 2025

Christian Stark
Stark Properties
5316 Railroad Avenue
Flowery Branch, GA 30542

<input checked="" type="checkbox"/>	APPROVED
<input type="checkbox"/>	DENIED
<input type="checkbox"/>	CONDITIONALLY APPROVED
Sewer Capacity Request #C2025-311-12	
Expiration Date: 12/10/2026	
Tie-In Manhole FID: 211670	

RE: Sewer Availability for Proposed Development – Collective at 316
Parcel ID 5208 004

Dear Mr. Stark:

Gwinnett County Department of Water Resources (GCDWR) has reviewed the impact of the proposed development consisting of a 50,000 square foot warehouse on the above parcel and determined:

Gwinnett County has adequate sewer capacity to serve the proposed development.

This confirmation is based on your anticipated annual average daily flow of **1.74 gpm** discharging to the sewer tie-in manhole at Facility ID **211670**. **This request is for anticipated flows from the development’s toilet facilities only; no industrial processing discharges are covered as none were identified for this capacity request.**

Capacity Allocation: Sewer capacities are allocated only upon the issuance of a Development Permit, by Gwinnett County Planning & Development and after all applicable conditions established in this Sewer Capacity Certification, if any, are met.

Certification Expiration: The Sewer Capacity Certification is valid for 12 months from the date of this letter. If rezoning is approved, GCDWR will issue a revised Sewer Capacity Certification based on the approved units, as needed. The capacity request can be renewed for an additional 12 months. If a Development Permit has not been obtained by the expiration date above the developer may apply for renewal by providing evidence of active rezoning, plan review, or permitting efforts.

Once the Sewer Capacity Certification expires, the capacity is no longer allocated, and a new Sewer Capacity Certification request must be submitted for re-evaluation with no guarantee of available capacity. The evaluation will be based on the updated conditions of the sewer system.

Certification Invalidation: This certification is not valid if there are proposed changes to your development that could impact downstream sewer capacity (e.g. land use density, sewer tie-in manhole, parcel changes, and/or rezoning). In this case the development must resubmit for a



new Sewer Capacity Request with updated information prior to the expiration of the previous one.

Please contact us at 678-376-7026 if you have any questions.

Sincerely,

GWINNETT COUNTY DEPARTMENT OF WATER RESOURCES

Della Taylor, PE
Division Director, Infrastructure Support
678.376.2130

C: Gwinnett County DWR – Lorraine Campagne, Section Manager; Raghu Vemuru, Engineer V; Mikala Weston, Engineer III; Geniva Sylvain, Engineer III
Gwinnett County P&D – Charli Young, Planning Manager; Christina Dejarnette, Engineer II; Wendell Tumale, Engineer III; Dominique Graves, Engineer II

Exhibit G: Traffic Impact Study

[attached]



833 Hurricane Shoals

Traffic Impact Study

Prepared for:

Stark Partners, LLC

Prepared by:

KCI Technologies Inc.

2160 Satellite Boulevard, Suite 130

Duluth, GA 30097

www.kci.com

678.990.6200

November 2025



11/11/2025

**RISE TO THE
CHALLENGE**

Executive Summary

The purpose of this study is to evaluate the potential traffic impacts of the proposed 833 Hurricane Shoals Road development. The site is located in Gwinnett County along Hurricane Shoals Road, east of the intersection of Progress Center Avenue. The development proposes 41,225 SF of office/workshop space. The site zoning plan (see Appendix B) for the development illustrates the site layout and the one proposed site driveway location along Hurricane Shoals Road. Hurricane Shoals Road is a four-lane roadway with a center two-way left turn lane, a posted 45-mph speed limit, and is classified as a major arterial by Gwinnett County in the vicinity of the proposed development.

For the purposes of the traffic study, the analysis included the expected completion (build-out) of the development by year 2027. The study performed an analysis of existing and future traffic conditions. Existing conditions analysis was conducted for the one study intersection of Hurricane Shoals Road at Progress Center Avenue. The future conditions analysis was performed for the year 2027 No Build Conditions (without the 833 Hurricane Shoals Road development) at the study intersection. The year 2027 Build Conditions (with the 833 Hurricane Shoals Road development) analysis was completed for the study intersection and proposed site driveway location.

The project volumes were calculated based on the Institute of Transportation Engineers' (ITE) Trip Generation Manual, Twelfth Edition. For the purposes of the traffic study, the analysis included up to 41,225 SF of office/workshop space. The most applicable ITE land use (LU) code was LU 110 (General Light Industrial). The estimated total driveway volumes are 148 vehicles per day (74 entering and 74 exiting), 20 vehicles during the AM peak hour (18 entering and 2 exiting) and 20 vehicles during the PM peak hour (5 entering and 15 exiting). Due to the development type, pass-by reductions were not included for the traffic analysis.

The Gwinnett County DOT traffic impact study guidelines, version 1 (June 30, 2023) was utilized. Based on the proposed development project trips, the traffic study scope included Level 2 elements. The traffic impact study network consisted of the one proposed site driveway location east of the intersection of Hurricane Shoals Road at Progress Center Avenue.

During the existing year 2025, the intersection of Hurricane Shoals Road at Progress Center Avenue is currently operating at a low level of service LOS E during the AM and PM peak hours; however, the all-way stop control operation does balance the vehicle delay at the four intersection approaches. In the future year 2027 No Build Conditions (without the 833 Hurricane Shoals Road development traffic volumes), the intersection of Hurricane Shoals Road at Progress Center is expected to continue operating at a low level of service LOS E during the AM and PM peak hours.

In the future year 2027 Build Conditions (with the 833 Hurricane Shoals Road development traffic volumes), the intersection of Hurricane Shoals Road at Progress Center Avenue is expected to continue operating at a low level of service LOS E during the AM and PM peak hours. The low development traffic volumes do not significantly affect the level of service / average vehicle delay results.

Additionally, at the intersection of Hurricane Shoals Road at Progress Center Avenue, the Year 2027 Build volumes are not expected to be high enough to meet MUTCD traffic signal volume warrants; therefore, no intersection control improvements were identified to reduce average vehicle delay.

The results of the traffic analysis indicate the one proposed site driveway, operating with side-street stop-control, is expected to operate with an acceptable level of service LOS B during the AM and PM peak hours.

The traffic study included a review of the need for a right-turn deceleration lane at the site driveway based on Gwinnett County criteria. Based on Gwinnett County's roadway classification, the Gwinnett County criteria is met requiring installation of a right-turn deceleration lane at the proposed site driveway.

Based on estimated traffic volumes in the year 2027 Build year conditions, and the Gwinnett County UDO criteria, the following geometric recommendations are provided:

- Hurricane Shoals Road at Proposed Site Driveway:
 - Provide one entry lane and one exit lane on the site driveway; driveway to have stop-control
 - Construct an eastbound right-turn deceleration lane along Hurricane Shoals Road

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A: Figures

- Figure 1 – Location Map
- Figure 2 – Aerial & Access Locations
- Figure 3 – Existing Traffic Conditions
- Figure 4 – Project Trip Distribution
- Figure 5 – Future Build (Year 2027) Conditions

B: Concept Plan

C: Traffic Count Data

D: GDOT Traffic Data

E: Intersection Volume Development

F: Capacity Analysis Reports

1. Existing Conditions

1.1 Site Conditions

The proposed development is located on property with no existing development. **Figure 1** provides a general location map. **Figure 2** is an aerial that shows the site location and the proposed site driveway (Figures included in Appendix A). Access to the property is proposed as one full movement driveway to be provided along the south side of Hurricane Shoals Road. The concept plan is included in Appendix B. Nearby land uses are primarily businesses and industrial.

1.2 Roadway Conditions

Hurricane Shoals Road travels in the east-west direction in the vicinity of the proposed development. It is a four-lane facility with a center two-way left-turn lane, that is functionally classified as a major arterial by Gwinnett County with a posted speed limit of 45 mph.

Progress Center Avenue travels in the north-south direction to the west of the proposed development. It is a four-lane facility with a raised median, that is functionally classified as a residential road by Gwinnett County with a posted speed limit of 35 mph.

The intersection of Hurricane Shoals Road at Progress Center Avenue is a four-leg intersection, operates with all-way stop-control, and has the following lane configuration on all four approaches:

- One shared through and left-turn lane, stop-control
- One right-turn lane, with "Keep moving" sign (free-flow condition)

Gwinnett County Transit does not provide bus routes near the development site.

There are no sidewalks located at the intersection of Hurricane Shoals Road at Progress Center Avenue and along Hurricane Shoals Road near the development site.

1.3 Traffic Volumes

Traffic counts were collected on Tuesday, October 29, 2025, when Gwinnett County public schools were in session, for use in the traffic analysis. The traffic data collected included:

- 4-hr turning movement count (TMC), 7-9 AM and 4-6PM, for Hurricane Shoals Road at Progress Center Avenue
- 24-hr average daily traffic (ADT) count along Hurricane Shoals Road at the proposed driveway location

Additionally, Gwinnett County collected a bi-directional 24-hr average daily traffic (ADT) count on Hurricane Shoals Road between Progress Center Avenue and Cedar Road on September 19, 2024, and the weekday volume was 6,066 vehicles.

Historical traffic volume data available from the GDOT TADA source were utilized to inform the annual growth factor. The three locations are indicated in Appendix D. The three locations are:

- GDOT Count Station #135-0127 located on Buford Drive, north of Hurricane Shoals Road
- GDOT Count Station #135-0249 located on University Parkway/SR 316, west of Hurricane Shoals Road

- GDOT Count Station #135-0252 located on University Parkway/SR 316, east of Hurricane Shoals Road

Figure 3 (in Appendix A) illustrates the existing 2025 traffic volumes. The 2025 traffic volumes are indicated in the Intersection Volume Development tables included in the Appendix E.

2. Future Conditions

2.1 Future No-Build Traffic Volumes

Future No-Build traffic volumes were developed by reviewing the historical traffic volumes roadways within the vicinity of the project and historic population growth in the county. Three GDOT count stations in the area were reviewed. The annual historic compound growth rate averaged 1.62% between the three count stations. The calculations are included in Appendix D. Gwinnett County's population growth rate was most recently reported as 1.74% per year in 2020. The Governor's Office of Planning and Budget developed population projections indicate an estimated growth of 0.93% by 2030 in Gwinnett County.

Considering this data, a 1.5% per year growth rate to account for background traffic volume growth was used in the traffic study. For the purposes of this study the proposed development is expected to be completed and opened by 2027. A 1.5% per year growth rate was applied to the 2025 existing volumes to calculate year 2027 No-Build traffic volumes.

The 2027 No-Build traffic volumes are indicated in the Intersection Volume Development tables included in Appendix E.

2.2 Future Roadway Conditions

A review of Gwinnett County and Georgia DOT planned and programmed transportation projects was performed. There were no Gwinnett County planned projects near the development. There were no Georgia DOT planned projects near the development site that will affect the proposed traffic volumes for the expected 2027 build year.

3. Proposed Development Traffic

Project traffic was calculated for the proposed development. Project traffic is defined as the vehicular trips expected to be generated by the development and distributed over the roadway network.

3.1 Trip Generation

The project driveway volumes were calculated based on the Institute of Transportation Engineers' (ITE) Trip Generation Manual, Twelfth Edition. The development proposes a total of 41,225 SF of office/workshop space. The most applicable ITE land use (LU) code is LU 110 (Light General Industrial) to account for the development. Due to the development type, pass-by reductions were not included for the traffic analysis.

Table 1 below summarizes the trips expected daily, during the AM peak hour and during the PM peak hour for the development.

Table 1: Proposed Site Trip Generation								
Land Use (ITE Code)	Units	Daily Trips	AM Peak Hour			PM Peak Hour		
		Two-Way Total	Enter	Exit	Total	Enter	Exit	Total
General Light Industrial (110)	SF	148	18	2	20	5	15	20
Driveway Totals		137	18	2	20	5	15	20

3.2 Trip Distribution and Assignment

An overall trip distribution and assignment of project trips was based on existing traffic patterns and a review of land uses and the street network in the area. This information was used to apply the project traffic volumes at the study intersections and development driveway.

The directional distribution for the proposed development is estimated to be:

- 10% to/from the north along Progress Center Avenue
- 20% to/from the south along Progress Center Avenue
- 20% to/from the east along Hurricane Shoals Drive
- 50% to/from the west along Hurricane Shoals Drive

Project trip distribution is illustrated in **Figure 4** in Appendix A.

3.3 Future Build Traffic Volumes

The 2027 future Build traffic volumes were calculated by adding the proposed development (833 Hurricane Shoals Road) traffic volumes to the projected year 2027 No-Build traffic volumes. **Figure 5** (in Appendix A) illustrates the year 2027 Build traffic volumes.

4. Capacity Analysis

Capacity analysis was performed at the study intersections for the weekday AM and PM peak hours. Intersection Level of Service (LOS) was calculated based on the methodologies contained in the Highway Capacity Manual, 7th Edition. The Synchro Studio software, which utilizes the HCM 7th Edition methodology, was utilized to perform the analyses.

Capacity is defined as the maximum number of vehicles that can pass over a particular road segment or through a particular intersection within a specified period under prevailing roadway, traffic, and control conditions. Level of service (LOS) is used to describe the operating characteristics of a road segment or intersection in relation to its capacity. LOS is defined as a qualitative measure that describes operational conditions and motorist's perceptions. The Highway Capacity Manual defines six levels of service, LOS A through LOS F. Level of service A indicates excellent operations with little delay to motorists, while level of service F indicates extremely long delay.

Level of service for unsignalized intersections is calculated for the average control delay incurred for vehicles on the stop control approach, compared to the average control delay per vehicle for all approaches at a signalized intersection. Control delay for vehicles include initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. **Table 2** below indicates the relationship between delay and LOS for signalized and unsignalized intersections, respectively. Level-of-service "E" is typically considered to be the limit of acceptable delay.

Several factors affect the controlled delay for unsignalized intersections, including the availability of gaps in the cross-street traffic, and acceptable gap time to make the movement from the stop position. For stop-control intersections, LOS E and F exist when there are insufficient gaps in traffic, resulting in long delays. Low level of service for stop-control approaches are not uncommon at major cross-streets.

Table 2: Level of Service Criteria		
Level of Service	Average Control Delay Per Vehicle (sec)	
	Signalized Intersection	Unsignalized Intersection
A	≤10	≤10
B	>10 and ≤20	>10 and ≤15
C	>20 and ≤35	>15 and ≤25
D	>35 and ≤55	>25 and ≤35
E	>55 and ≤80	>35 and ≤50
F	>80	>50

4.1 Existing Conditions Capacity Analysis

Capacity analysis was performed for the year 2025 Existing Conditions. The Existing traffic conditions and volumes are illustrated in **Figure 3**. **Table 3** summarizes the results of the capacity analysis.

Table 3: Existing (Year 2025) Level of Service				
Intersection	Intersection Control	Approach	AM Peak Hour LOS (Delay*)	PM Peak Hour LOS (Delay*)
Hurricane Shoals Road at Progress Center Avenue	All-way Stop-Control	Overall	E (41)	E (41)
		NB Progress Center Ave	B (14)	C (18)
		SB Progress Center Ave	C (16)	B (15)
		EB Hurricane Shoals Rd	C (18)	F (79)
		WB Hurricane Shoals Rd	F (68)	D (26)

*Average vehicle delay in seconds

Under existing conditions, the intersection of Hurricane Shoals Road at Progress Center Avenue is currently operating at a low level of service LOS E during the AM and PM peak hours; however, the all-way stop control operation does balance the vehicle delay at the four intersection approaches.

4.2 Future No Build Conditions Capacity Analysis

Capacity analysis was performed for the year 2027 Future No Build Conditions and includes the No-Build traffic volumes (without the 833 Hurricane Shoals Road development) and existing roadway conditions. **Table 4** summarizes the results of the capacity analysis.

Table 4: No Build (Year 2027) Level of Service				
Intersection	Intersection Control	Approach	AM Peak Hour LOS (Delay*)	PM Peak Hour LOS (Delay*)
Hurricane Shoals Road at Progress Center Avenue	All-way Stop-Control	Overall	E (45)	E (48)
		NB Progress Center Ave	B (14)	C (19)
		SB Progress Center Ave	C (16)	C (15)
		EB Hurricane Shoals Rd	C (18)	F (96)
		WB Hurricane Shoals Rd	F (75)	D (29)

*Average vehicle delay in seconds

By the 2027 Future No-Build Conditions, the intersection of Hurricane Shoals Road at Progress Center Avenue is expected to continue operating at a low level of service LOS E during the AM and PM peak hours.

4.3 Future Build Conditions Capacity Analysis

Capacity analysis was performed for the year 2027 Future Build Conditions and includes the No-Build traffic volumes plus the 833 Hurricane Shoals Road development volumes. The Build traffic conditions and volumes are illustrated in **Figure 5. Table 5** summarizes the results of the capacity analysis.

Table 5: Build (Year 2027) Level of Service				
Intersection	Intersection Control	Approach	AM Peak Hour LOS (Delay*)	PM Peak Hour LOS (Delay*)
Hurricane Shoals Road at Progress Center Avenue	All-way Stop-Control	Overall	E (48)	E (45)
		NB Progress Center Ave	B (15)	C (17)
		SB Progress Center Ave	C (17)	B (15)
		EB Hurricane Shoals Rd	C (18)	F (88)
		WB Hurricane Shoals Rd	F (81)	D (28)
Hurricane Shoals Road at Proposed Driveway	Two-way Stop Control	NB Proposed Driveway	B (11)	B (13)

*Average vehicle delay in seconds

In the future year 2027 Build Conditions (with the 833 Hurricane Shoals Road development traffic volumes), the intersection of Hurricane Shoals Road at Progress Center Avenue is expected to continue operating with a low level of service LOS E during the AM peak hour and PM peak hour; however, the all-way stop control operation does balance the vehicle delay at the four intersection approaches. The low development traffic volumes do not significantly affect the level of service / average vehicle delay results.

At the intersection of Hurricane Shoals Road at Progress Center Avenue, the Year 2027 Build volumes are not expected to be high enough to meet MUTCD traffic signal volume warrants; therefore, no intersection control improvements were identified to reduce average vehicle delay.

The results of the traffic analysis indicate the one proposed site driveway, operating with side-street stop-control, is expected to operate with an acceptable level of service LOS B during the AM peak hour and PM peak hour. The capacity analysis included adding the eastbound right-turn deceleration lane along Hurricane Shoals Road as dictated by the Gwinnett County right-turn lane criteria.

Capacity analysis reports can be found in Appendix F.

5. Recommendations

Recommendations for access for the proposed 833 Hurricane Shoals Road development are based on existing conditions, the proposed development use, and expected traffic volumes. The need for dedicated turn lanes at the proposed development driveway and appropriate traffic control (i.e. stop control) were based on Gwinnett County criteria. Recommendations were based on transportation industry standards and engineering judgment. Specifics of the driveway design will need to follow Gwinnett County requirements for the driveway along Hurricane Shoals Road.

5.1 Turn Lane Analysis at Site Driveway

Hurricane Shoals Road is a county-maintained road. For the purposes of the traffic study, the Gwinnett County turn lane analysis criteria was evaluated.

Gwinnett County has criteria for installation of right-turn lanes as set forth by the Unified Development Ordinance (UDO) for County-maintained roads. The County UDO states that right-turn deceleration lanes shall be provided at each project driveway or subdivision street entrance that is provided street access to a Minor Collector Street or Major Thoroughfare. Since Hurricane Shoals Road is classified as a major arterial by Gwinnett County, a right-turn deceleration lane WILL be required at the proposed driveway.

5.2 Recommended Driveway Geometry

Based on estimated traffic volumes in the year 2027 Build year conditions, and the Gwinnett County UDO criteria, the following geometric recommendations are provided:

- Hurricane Shoals Road at Proposed Site Driveway:
 - Provide one entry lane and one exit lane on the site driveway; driveway to have stop-control
 - Construct an eastbound right-turn deceleration lane along Hurricane Shoals Road

Exhibit H: Public Participation Plan and Report

[attached]



Public Participation Plan

1. Who do you intend to contact in addition to property owners within 1,000 feet of the site, if any?

Only property owners within 1,000 feet of the site

2. Where do you plan to host the Public Participation Meeting? Ensure that the meeting is held in person at a convenient location for interested parties.

Rabit Hill Park Pavillion near Field 7

401 Rabbit Hill Road Dacula, GA 30019

3. What date and time do you plan to host the Public Participation Meeting? Ensure that the meeting is held at a convenient time for interested parties, e.g. not during typical work hours on weekdays.

January 17th 2026 at 6:00 PM Eastern Standard Time

4. What is your method for providing opportunities for discussion with interested parties at the Public Participation Meeting?

Ample Time For Questions and Answers for all attendees.

Public Participation Report

- 1. List all groups that you notified of the requested application. Include a copy of the notification package and stamped Postal Service Form 3665 (attached) with itemized name and address of each addressee.**

Please see attached Stamped Postal Service Form 3665 attached with the email.

This form includes all 17 groups notified and the address of each.

- 2. Provide the date, time, and location of all meeting(s) that the applicant and/or representative attended to discuss an application with interested parties.**

The Public Participation Meeting for Zon2025-00105 / Rez2026-00007 was held on 2.12.26 at 6:00 PM est at Rabbit Hill Park under Pavilion 7.

- 3. Provide the number of people who participated in the meeting(s). Include the sign-in sheet(s) with meeting date, time, location, and attendee names.**

0 people participated in the meeting

- 4. What issues and concerns were expressed by attendees at the meeting(s)?**

No issues or concerns were expressed. There were 0 attendees.

- 5. What are the applicant's responses to the issues and concerns that were expressed at the meetings? What changes to the development will be made?**

There was no response to any issues or concerns because there were 0 attendees.

Exhibit I: Internal and External Agency Review Comments

[attached]



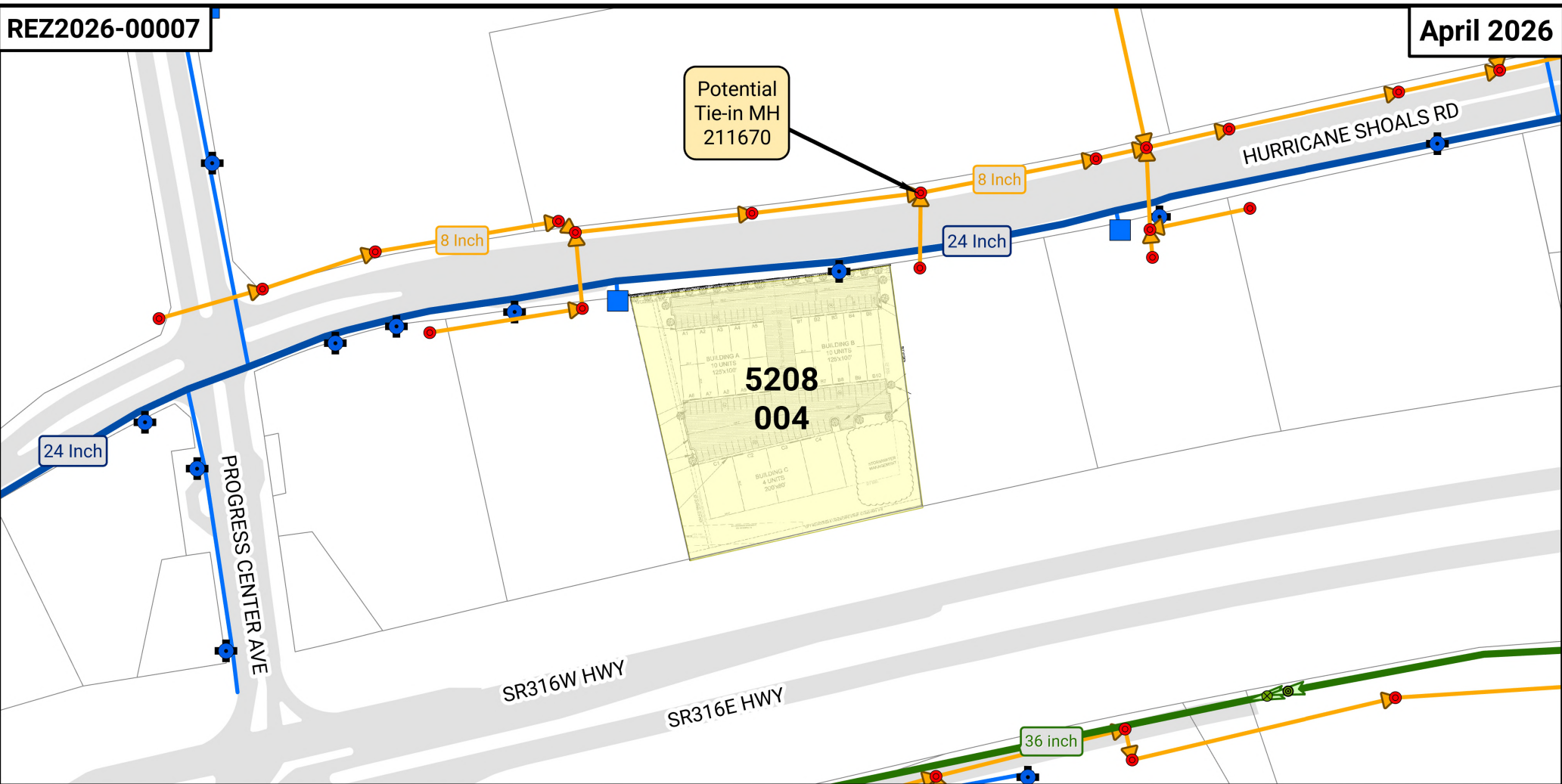
**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		02/20/2026	
Department/Agency Name:		Transportation	
Reviewer Name:		Brent Hodges	
Reviewer Title:		Construction Manager 2	
Reviewer Email Address:		Brent.Hodges@gwinnettcountry.com	
Case Number:		REZ2026-00007	
Case Address:		833 Hurricane Shoals Road, Lawrenceville, 30043	
Comments:		<input checked="" type="checkbox"/> YES	<input type="checkbox"/> NO
1	Hurricane Shoals Road is a major collector. The Average Daily Traffic (ADT) volume is 6,317.		
2	The nearest Ride Gwinnett facility (Stop 4528 / Route 45) is located 2.3 miles away at Collins Hill Road and Collins Industrial Way.		
3	The developer shall provide the minimum 50' taper to go along with the standard deceleration lane storage, per section 360-30 of the UDO.		
4	The developer shall construct a 5' concrete sidewalk along the entire site frontage, per section 360-90 of the UDO.		
5			
6			
7			
Recommended Zoning Conditions:		<input type="checkbox"/> YES	<input checked="" type="checkbox"/> NO
1			
2			
3			
4			
5			
6			
7			



**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		
Department/Agency Name:	DWR	
Reviewer Name:	Lorraine Campagne	
Reviewer Title:	Section Manager	
Reviewer Email Address:	Lorraine.Campagne@gwinnettcounty.com	
Case Number:	REZ2026-00007	
Case Address:	833 Hurricane Shoals Rd	
Comments:		<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO
1	Water: The proposed development may connect to an existing 24-inch water main located along the southern right-of-way of Hurricane Shoals Road.	
2	Sewer: A Sewer Capacity Certification (C2025-311-12) was approved in December 2025 for a 50,000 square foot warehouse. The proposed development plans to connect to an existing 8-inch gravity sewer located on the northern right-of-way of Hurricane Shoals Road. A jack-and-bore will be required.	
3		
4		
5		
6		
7		
Recommended Zoning Conditions:		<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO
1		
2		
3		
4		
5		
6		
7		

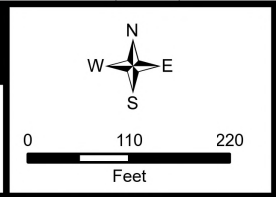


LEGEND

- Flow Management
- Pump Station
- Regional
- Manhole
- Hydrant
- City
- Water Main
- Proposed Water Main
- Sewer Collector
- Sewer Interceptor
- Sewer Force Main
- Proposed Sewer Collector

833 Hurricane Shoals Rd
C-2 to M-1

Water & Sewer Utility Map



LOCATION



Water Comments: The proposed development may connect to an existing 24-inch water main located along the southern right-of-way of Hurricane Shoals Road.

Sewer Comments: A Sewer Capacity Certification (C2025-311-12) was approved in December 2025 for a 50,000 square foot warehouse. The proposed development plans to connect to an existing 8-inch gravity sewer located on the northern right-of-way of Hurricane Shoals Road. A jack-and-bore will be required.

Water Availability: Water demands imposed by the proposed development may require upsizing or extensions of existing water mains in order to meet Gwinnett County Standards and fire flow demands. Any cost associated with such required improvements will be the responsibility of the development. Current Gwinnett County Standards require a minimum of 12" pipe size for commercial developments and a minimum of 8" pipe size for residential developments. Additionally, connection to a minimum of 12" and 8" mains are required for commercial and residential developments, respectively. It is the responsibility of the developer's engineer to confirm pressure and volumes are available for the development.

Sewer Availability: A Sewer Capacity Certification must be obtained from Gwinnett County to confirm the existing system can serve the development. Sewer demands imposed by the proposed development may require upsizing and/or extensions of existing sewer mains, and/or upsizing of an existing pump station, and/or installation of a new pump station. Any cost associated with such required improvements will be the responsibility of the development. The developer shall provide easements for future sewer connection to all locations designated by Gwinnett County during plan review.

Water and Sewer Design and Construction Requirements: Extensions of the water and/or sanitary sewer systems within the subject development must conform to this department's policies, Gwinnett County's ordinances, and the Water Main and Sanitary Sewer Design and Construction Standards and Specifications, dated April 5th, 2016. Subsequent to design, construction, inspection, and final acceptance of the required utilities, service would then become available under the applicable utility permit rate schedules.

Private Road Developments: Any development with private roads must comply with the Standard Policy Requirement for the Installation of Water and Sanitary Sewer Mains within Private Developments. This policy stipulates minimum easement requirements and location of public mains and appurtenances, among other requirements.

Exhibit J: Maps

[attached]


HURRICANE SHOALS RD



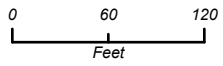
SR316W HWY

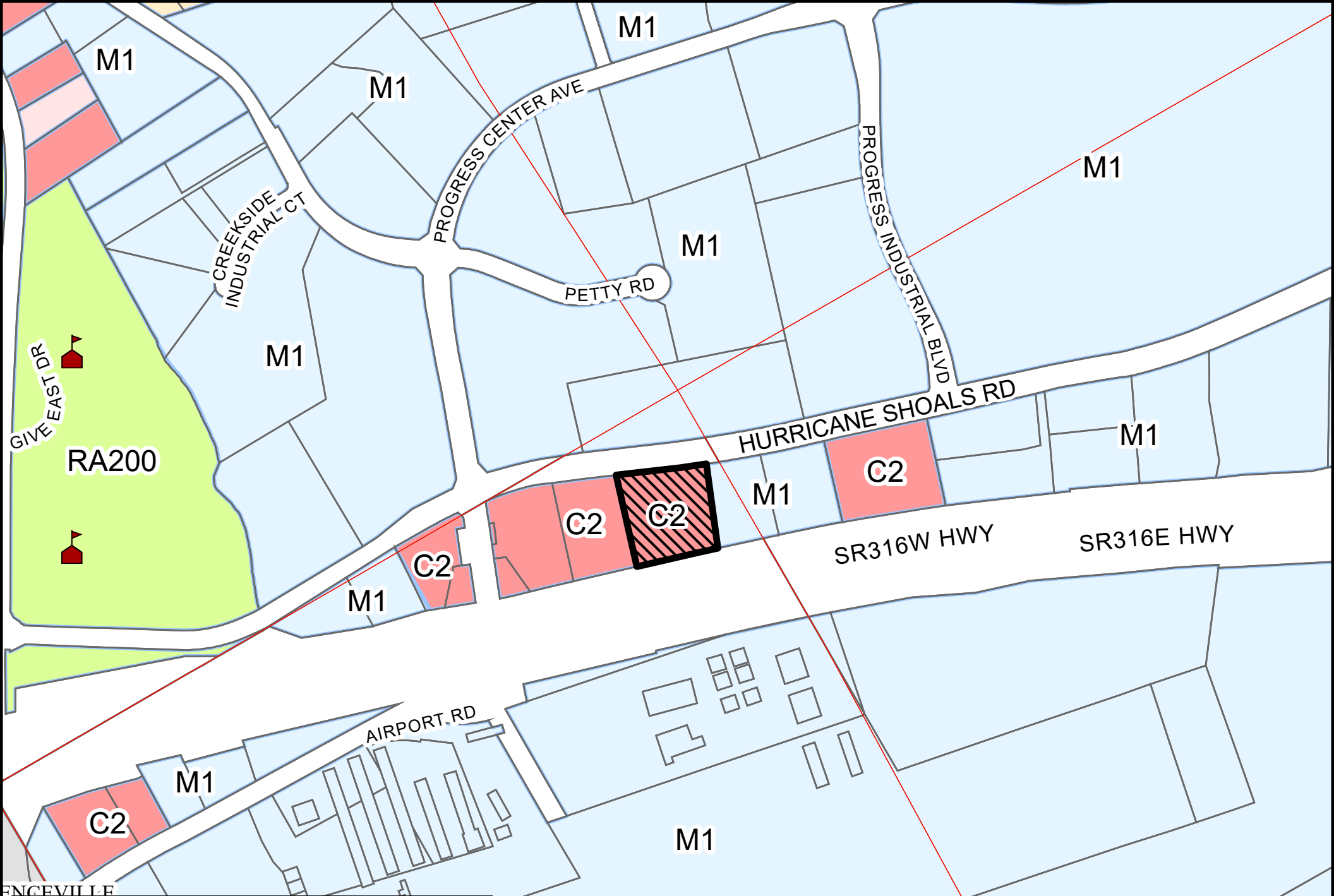
SR316E HWY

AIRPORT RD

 REZ2026-00007

Created: January 2, 2026





 REZ2026-00007

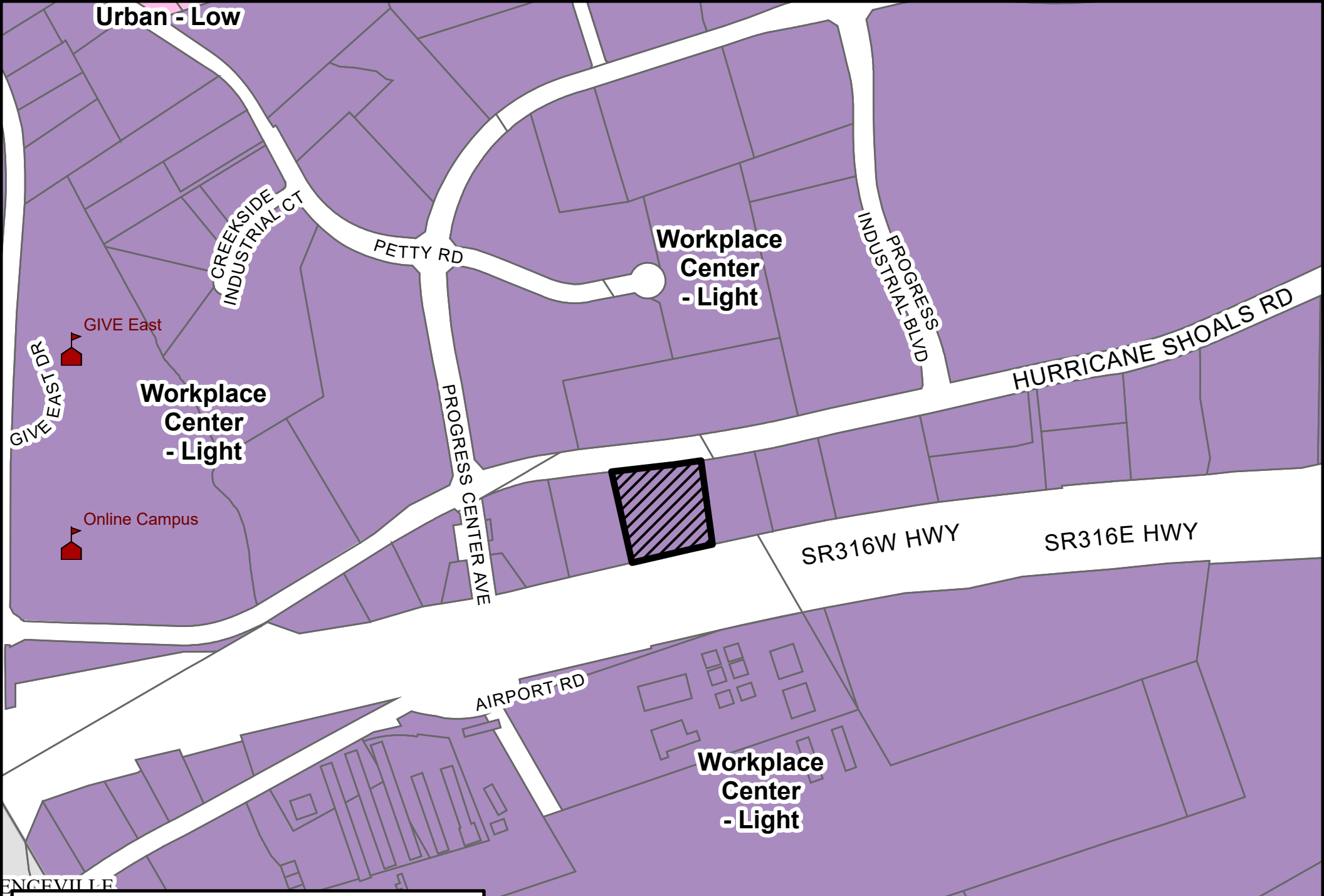
Created: January 2, 2026

N

0 250 500
Feet

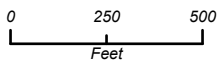
SUBJECT SITE AND SURROUNDING ZONING

Urban - Low



REZ2026-00007

Created: January 2, 2026

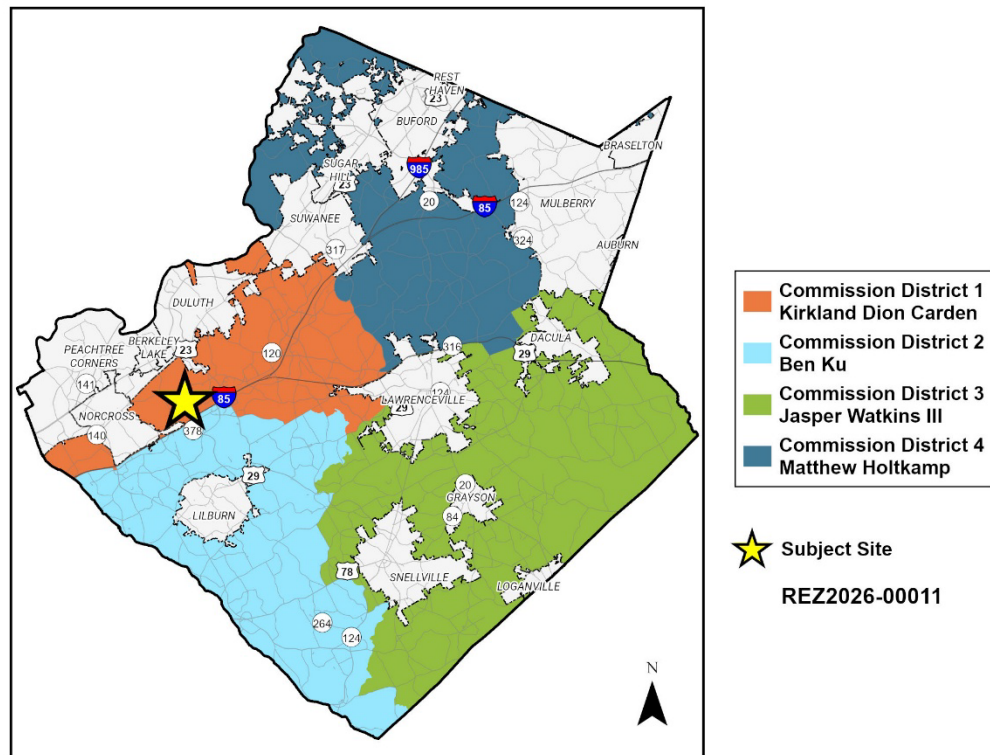


2045 UNIFIED PLAN FUTURE DEVELOPMENT CLASSIFICATION

PLANNING AND DEVELOPMENT DEPARTMENT CASE REPORT

Case Number: REZ2026-00011
Current Zoning: HRR (High-Rise Residential Multifamily District)
Request: Rezoning to **C-2** (General Business District)
Overlay District: Gateway 85 Overlay District
Address: 4400 Satellite Boulevard
Map Number: R6210 034
Site Area: 2.04 acres
Square Feet: 24,841
Proposed Development: Shopping Center
Commission District: District 1 – Commissioner Carden
Future Development Type: Urban – Medium

Staff Recommendation: **APPROVAL WITH CONDITIONS**



Applicant: Jupiter Development, Inc.
c/o Mahaffey Pickens Tucker, LLP
1550 North Brown Road, Suite 125
Lawrenceville, GA 30043

Owner: R&D Investments Group, LLC
4500 Satellite Boulevard, Suite 1140
Duluth, GA 30096

Contact: Gabrielle Schaller

Phone: 770.232.0000

Zoning History

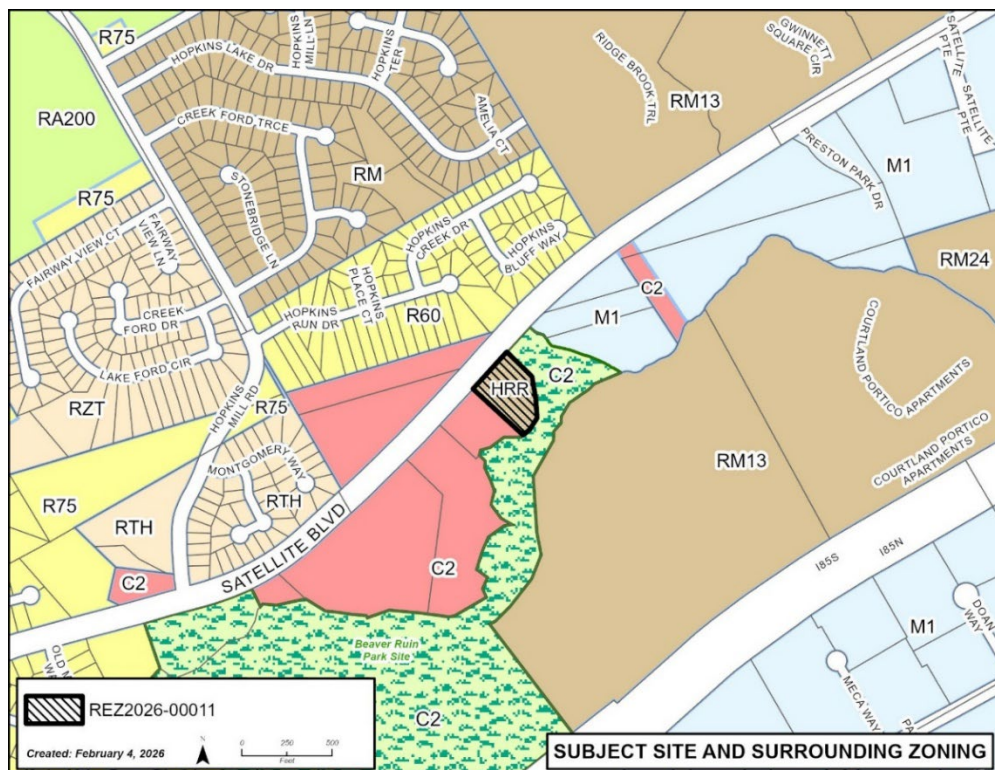
The subject property is zoned HRR (High-Rise Residential Multifamily District). In 2019, the property was rezoned from C-2 (General Business District) to HRR for a mixed-use development, pursuant to RZM2019-00003. The subject property is located in the Gateway 85 Overlay District.

Existing Site Condition

The subject property is a 2.04-acre parcel located along Satellite Boulevard, east of its intersection with Beaver Ruin Road. The property is undeveloped and contains dense vegetation. There is a full-access entrance with inter-parcel connection to the property to the west, and a 5-foot-wide sidewalk along the road frontage. The nearest Ride Gwinnett transit stop is approximately 600 feet from the subject property.

Surrounding Use and Zoning

The property is surrounded by a mixture of commercial, civic, and residential uses. A self-storage facility is to the north, across Satellite Boulevard. The Beaver Ruin Wetland Park is located to the east and south. A convenience store with fuel pumps and a shopping center are located to the west. Single-family detached residences and apartments are located nearby. The following is a summary of surrounding uses and zoning:



Location	Land Use	Zoning
Proposed	Shopping Center	C-2
North	Commercial (Self-storage Facility)	C-2
East	Civic (Beaver Ruin Wetland Park)	C-2
South	Civic (Beaver Ruin Wetland Park)	C-2
West	Commercial (convenience store with fuel pumps) Commercial (shopping center)	C-2

Project Summary

The applicant requests rezoning from HRR to C-2 for a shopping center, including:

- A 24,841-square-foot building with 12 suites, including 11 one-story suites and one two-story restaurant/bar, yielding a floor area ratio (FAR) of 0.28.
- Exterior building materials of brick, cementitious siding, and glass on all façades.
- 77 parking spaces throughout the site including two EV charging stations, and one loading space behind the building.
- Underground stormwater management facility beneath the driveway along the western property line.
- A 12-foot-wide multi-use path along Satellite Boulevard while maintaining the full-access entrance.

Zoning and Development Standards

The following is a summary of applicable standards from the Unified Development Ordinance (UDO):

Standard	Required	Proposed	Meets Standard?
FAR	Maximum 1.0	0.28	YES
Lot Coverage	Maximum 80%	79.9%	YES
Building Height	Maximum 45'	26'4"	YES
Parking	Minimum 50 spaces Maximum 83 spaces	77 spaces	YES
Loading Spaces	1	1	YES

The proposal meets all applicable requirements of the Gateway 85 Overlay District.

Public Participation

The applicant held a community meeting on February 11, 2026, at the Shorty Howell Park Activities Building in Duluth. There was one community member in attendance, who raised questions regarding the public hearing process and whether a convenience store would be a tenant of the shopping center. The applicant responded with the dates of the public hearings and that future tenants are still under consideration at this time.

Staff Analysis

Rezoning Request Analysis: The standards governing the exercise of zoning power are outlined in Section 270-20.6 of the UDO. Staff makes the following findings related to the zoning requests:

A. Whether the proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

The property is surrounded by a mixture of commercial, civic and residential uses along a commercial corridor. The proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

B. Whether the proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property.

The proposed shopping center does not directly abut any residential properties, and staff has recommended a condition prohibiting certain intense commercial uses. With proper conditions, the proposed rezoning will not adversely affect the existing use or usability of adjacent or nearby properties.

C. Whether the property to be affected by the proposed rezoning has a reasonable economic use as currently zoned.

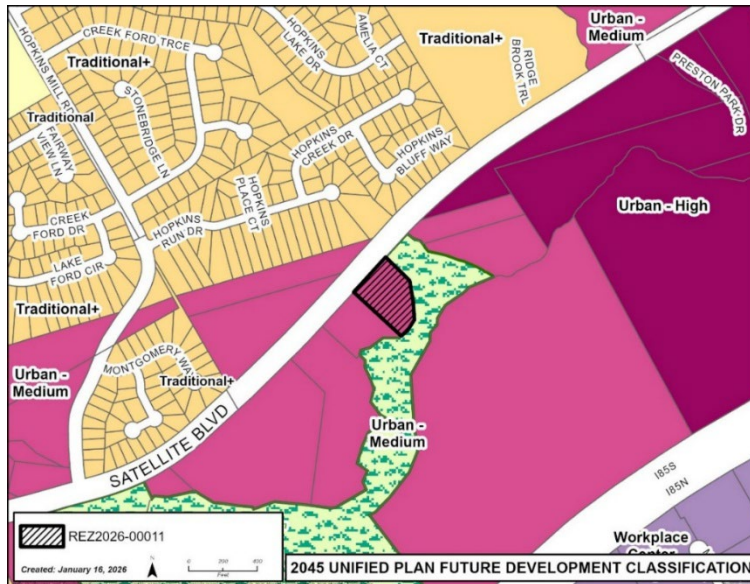
The property has a reasonable economic use as currently zoned.

D. Whether the proposed rezoning will result in a use which will or could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools.

An increase in impacts on public facilities would be anticipated in the form of traffic, utility demand, and stormwater runoff; however, these impacts could be mitigated with appropriate conditions, site development requirements, and planning. The traffic impact study estimates that the development would generate 47 morning and 131 evening peak hour trips. The traffic impact study did not recommend any roadway improvements. No impact is anticipated on school enrollment.

E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map; and

The Unified Plan's Future Development Map indicates "Urban – Medium" for the subject property, which envisions mixed-use development. Although C-2 is not a recommended zoning district, shopping centers are a recommended use and are consistent with uses in the area. The proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map.



F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning.

There are no other conditions giving support for approval or disapproval of the proposal.

Staff Recommendation

Based on the staff’s evaluation of the request and the standards governing the exercise of zoning power, the Department of Planning and Development recommends **APPROVAL WITH CONDITIONS** of the rezoning request.

Staff Recommended Conditions

Approval as C-2 (General Business District) for a shopping center, subject to the following conditions:

1. The property shall be limited to uses allowed in C-2, not exceeding 25,000 square feet, with the following uses prohibited:
 - a. Precious Metals Dealer Establishment
 - b. Restaurant, with drive-in or drive-thru service
 - c. Smoke, Novelty, or CBD Shop
2. The proposed development shall be constructed in general conformance with Exhibit B: Site Plan, dated received March 18, 2026, and Exhibit C: Building Elevations, dated received December 22, 2025, with revisions required by zoning conditions and the Unified Development Ordinance, subject to the review and approval of the Department of Planning and Development.
3. A 12-foot-wide multi-use path shall be provided along Satellite Boulevard, prior to the issuance of a Certificate of Occupancy, subject to the review and approval of the Gwinnett County Department of Transportation.

Exhibits:

- A. Site Visit Photos
- B. Site Plan
- C. Building Elevations
- D. Letter of Intent and Applicant's Response to Standards
- E. Application and Disclosure of Campaign Contributions
- F. DWR Sewer Capacity Certification Letter
- G. Traffic Impact Study
- H. Public Participation Plan and Report
- I. Internal and External Agency Review Comments
- J. Maps

Exhibit A: Site Visit Photos



Existing site conditions



Satellite Boulevard facing west



Adjacent convenience store, with fuel pumps



Shopping Center to the west

Exhibit B: Site Plan

[attached]

Exhibit C: Building Elevations

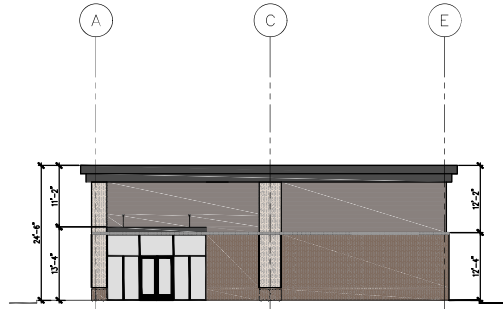
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RECEIVED

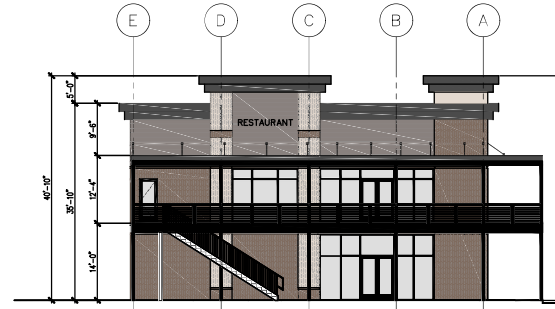
12/22/2025

MATERIAL LEGEND

- BRICK COLOR**
 (1) HOSSWOOD
 (2) COLUMBUS BRICK
 (3) MAGNOLIA BAY "PHENIX CITY COLLECTION"
 (4) MERRIMAN BRICK
- SIGN COLOR**
 (5) ASH "VINTAGE WOOD"
 (6) NICHHA
- RAILING**
 (7) BLACK METAL RAIL
- PAINT**
 (8) KENDALL CHARCOAL HC-166
 (9) BEN MOORE



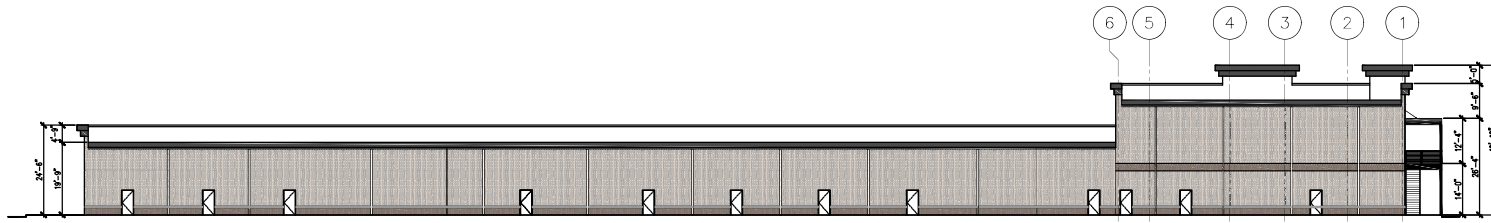
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SCALE: 3/32"=1'-0"



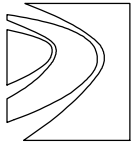
2 LEFT ELEVATION
SCALE: 3/32"=1'-0"



3 FRONT ELEVATION
SCALE: 3/32"=1'-0"



4 REAR ELEVATION
SCALE: 1/16"=1'-0"



Pate Design Group, Inc.
 Architecture / Interiors
 1380 West Paces Ferry Rd
 Suite 2090
 Atlanta, GA 30327
 (p) 770-814-0885

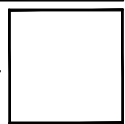
kirby@patedesigngroup.com

RETAIL BUILDING
 4400 SATELLITE BLVD.
 DULUTH, GA 30096

Revisions:

No.	Date	Description

Drawn
ADE
 Checked
KEP
 Contract No.
 25-32
 Scale
 Date
 Last Rev.



PRELIMINARY DESIGN

of sheets
 THIS DRAWING IS THE PROPERTY OF PATE DESIGN GROUP AND IS NOT TO BE COPIED, REPRODUCED, OR IN ANY MANNER USED FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREIN AND IS NOT TO BE USED IN ANY OTHER PROJECT. IT IS TO BE RETURNED UPON REQUEST.

Exhibit D: Letter of Intent and Applicant's Response to Standards

[attached]

David Belle Isle
Matthew P. Benson
Catherine W. Davidson
Gerald Davidson, Jr.*
Brian T. Easley
Rebecca B. Gober
John D. Hipes*
Christopher D. Holbrook
Jessica R. Kelly
Shane M. Lanham
Julia A. Maxwell

Jeffrey R. Mahaffey
John N. Mahaffey
Steven A. Pickens
Jack M. Ryan
Gabrielle H. Schaller
S. Tess Shaheen
Andrew D. Stancil
Michael A. Tralongo
R. Lee Tucker, Jr.

*Of Counsel

LETTER OF INTENT FOR REZONING APPLICATION

Mahaffey Pickens Tucker, LLP submits this Letter of Intent and attached rezoning application (the “Application”) on behalf of Jupiter Development, Inc. (the “Applicant”) requesting the rezoning of an approximately 2.039-acre tract of land (the “Property”) located along Satellite Boulevard adjacent to a BP gas station to the west and the entrance to the Maddox apartment complex adjacent to the east. The Property is located within the Urban Medium Future Development Area of the Gwinnett County 2045 Unified Plan (the “2045 Plan”) and is currently zoned HRR (High-Rise-Residential Multifamily District).

The Applicant submits the Application requesting to rezone the Property to the C-2 zoning classification of the Gwinnett County Unified Development Ordinance (the “UDO”) to develop the Property as an attractive, walkable, multi-tenant retail shopping center with a restaurant component. The shopping center would be constructed with attractive architectural design and building materials as depicted in the renderings submitted with the Application. The Property would include appropriate parking, pedestrian circulation, and sidewalk and the Applicant is also coordinating with the county to assist in connecting to the Beaver Ruin Wetland Park project by

Sugarloaf Office || 1550 North Brown Road, Suite 125, Lawrenceville, Georgia 30043

Alpharetta Office || 178 South Main Street, Suite 250, Alpharetta, Georgia 30009

TELEPHONE 770 232 0000

FACSIMILE 678 518 6880
Page 14 of 55

www.mptlawfirm.com

providing a 12-foot trail connection on the Satellite Boulevard frontage. The Property will have full access onto Satellite Boulevard and will also provide interparcel connection with the adjacent BP gas station.

The proposed development is compatible with surrounding land uses and zoning classifications and fits within the established pattern of development. The surrounding area is characterized by a mix of uses including a variety of residential commercial, and industrial uses, and the particular section of Satellite Boulevard where the Property is located is dominantly zoned for commercial. The adjacent BP gas station also includes a restaurant component and the strip of commercial on the other side of the gas station from the Property includes retail, restaurants, and the Paragon Golf Center. The proposed development would contribute to the commercial activity in the area and provide convenient access to more retail shopping and restaurant options to those who work and live locally, contributing to the goal of the county to provide areas for residents to live, work, and play.

The proposed development is also compatible with the 2045 Plan's specific land use policies for the Property's Future Development Area, which is "Urban Medium." Urban Medium designates retail as an appropriate land use for the area. The proposed development will also contribute to the Plan's goal of making the area more pedestrian friendly and encouraging multimodal transportation by upgrading the property to include pedestrian circulation, sidewalk, and connection to the Gwinnett County trail system. Additionally, the proposed development also aligns with the Plan's goals for economic development such as promoting investment at the neighborhood scale, attracting new business and talent, and promoting economic growth and mobility by placing retail amenities in a convenient location for nearby residents. Providing convenient access to retail will encourage residents to spend their time and money at businesses

closer to their homes which will reduce long commutes and encourage alternative modes of transportation to cars.

The Applicant and its representatives welcome the opportunity to meet with staff of the Gwinnett County Department of Planning & Development to answer any questions or to address any concerns relating to the matters set forth in this letter or in the Rezoning Application filed herewith. The Applicant respectfully requests your approval of these Applications. Applicant's response to the Standards Governing the Exercise of the Zoning Power is attached as Exhibit "A."

Respectfully submitted this 8th day of October, 2025.

MAHAFFEY PICKENS TUCKER, LLP

Gabrielle H. Schaller

Gabrielle H. Schaller
Attorneys for Applicant

Pursuant to requirements of the United Development Ordinance, the Board of Commissioners finds that the following standards are relevant in balancing the interest in promoting the public health, safety, morality, or general welfare against the right to the unrestricted use of property and shall govern the exercise of the zoning power.

- A. Whether a proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property:

Yes, approval of the proposed Rezoning Application will permit a use that is suitable in view of the use and development of adjacent and nearby property. The proposed development is a retail use that will contribute to growth and promote activity in the area.

- B. Whether a proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property:

No, approval of the proposed Rezoning Application will not adversely affect the existing use or usability of any of the nearby properties. Rather, the proposed development will complement the mix of the surrounding area.

- C. Whether the property to be affected by a proposed rezoning has reasonable economic use as currently zoned:

Due to the size, location, layout and dimensions of the subject property, the Applicant submits that the property does not have reasonable economic use as currently zoned.

- D. Whether the proposed rezoning will result in a use which could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools:

No, approval of the proposed rezoning will not result in an excessive or burdensome use of the infrastructure systems. The Property has convenient access to major transportation corridors and water and sewer utilities are present in the area.

- E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map:

Yes, approval of the proposed rezoning is in conformity with the spirit and intent of the Gwinnett County 2045 Unified Plan which designates retail as an appropriate land use for the area.

- F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning:

The Applicant submits that the location of the property and the character of surrounding development provide additional supporting grounds for approval of the Application.

Exhibit E: Application and Disclosure of Campaign Contributions

[attached]

REZONING APPLICATION

AN APPLICATION TO AMEND THE OFFICIAL ZONING MAP OF GWINNETT COUNTY, GA.

APPLICANT INFORMATION	PROPERTY OWNER INFORMATION
Name: <u>Jupiter Development, Inc. c/o Mahaffey Pickens Tucker LLP</u> Address: <u>1550 North Brown Road, Suite 125</u> City: <u>Lawrenceville</u> State: <u>GA</u> ZIP: <u>30043</u> Phone: <u>770.232.0000</u> Email: <u>gschaller@mptlawfirm.com</u>	Name: <u>R&D Investments Group LLC</u> Address: <u>4500 Satellite Boulevard, Suite 1140</u> City: <u>Duluth</u> State: <u>GA</u> ZIP: <u>30096</u> Phone: <u>770.232.0000</u> Email: <u>gschaller@mptlawfirm.com</u>
Contact Person: <u>Gabrielle Schaller</u> Phone: <u>770.232.0000</u> Contact's Email: <u>gschaller@mptlawfirm.com</u>	
APPLICANT IS THE:	
<input checked="" type="checkbox"/> Owner's Agent <input type="checkbox"/> Property Owner <input type="checkbox"/> Contract Purchaser	
Current Zoning District(s): <u>HRR</u> Requested Zoning District: <u>C-2</u> Parcel Number(s): <u>6210 034</u> Acreage: <u>2.04</u> Property Address(es): <u>4400 Satellite Boulevard</u> Proposed Development: <u>Shopping Center</u> Variance(s): <u>N/A</u> Waiver(s): <u>N/A</u>	
RESIDENTIAL DEVELOPMENT No. of Dwelling Units: _____ Dwelling Unit Sq. Ft.: _____ Density: _____ Floor Area Ratio (LRR, MRR, HRR): _____	NON-RESIDENTIAL DEVELOPMENT No. of Buildings: <u>1</u> Total Building Sq. Ft.: <u>24,842</u> Floor Area Ratio: <u>0.28</u>
MIXED-USE DEVELOPMENT	
No. of Dwelling Units: _____ Dwelling Unit Sq. Ft.: _____ Total Non-Residential Sq. Ft.: _____ Floor Area Ratio: _____	



The undersigned below, or as attached, is the owner of the property considered in this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon in less than six (6) months from the date of last action by the Board of Commissioners.

Property Owner

Name: Tina Dang
Signature: [Handwritten Signature]
Date: 5-28-25

Notary Public

Name: Mary Reeves
Signature: Mary Reeves
Date: 5-28-25



CONFLICT OF INTEREST CERTIFICATION FOR REZONING

The undersigned below, making application for a rezoning, has complied with the Official Code of Georgia Section 36-67A-1, et. seq, Conflict of Interest in Zoning Actions, and has submitted or attached the required information on the forms provided.

Signature of Applicant

Date

Type of Print Name and Title

Gabrielle Schaller

1/8/2026

Gabrielle Schaller - Attorney

Signature of Applicant's
Attorney or Representative

Date

Type or Print Name and Title

Catherine W. Davidson

1/8/2026

Signature of Notary Public

Date



DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

Have you, within the two years immediately preceding the filing of this application, made campaign contributions aggregating \$250.00 or more to a member of the Board of Commissioners?



Yes



No

Manaffey Pickens Tucker, LLP

(Your Name)


If the answer is yes, please complete the following section:


NAME AND OFFICAL POSITION OF GOVERNMENT OFFICIAL	CONTRIBUTIONS (List all which aggregate to \$250 or More)	DATE CONTRIBUTION WAS MADE (Within last two years)
<i>Please see attached</i>		

Attach additional sheets if necessary to disclose or describe all contributions.

CONFLICT OF INTEREST CERTIFICATION FOR REZONING

The undersigned below, making application for a rezoning, has complied with the Official Code of Georgia Section 36-67A-1, et. seq, Conflict of Interest in Zoning Actions, and has submitted or attached the required information on the forms provided.

 10-08-2025 AMIN PANJWANI OWNER
 Signature of Applicant Date Type of Print Name and Title

Signature of Applicant's Attorney or Representative Date Type or Print Name and Title
 10/08/2025 Keaven L Dottery
 Keaven L Dottery
 NOTARY PUBLIC
 FULTON COUNTY, GEORGIA
 My Commission Expires 09/04/2028
 Signature of Notary Public Date Notary Seal

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

Have you, within the two years immediately preceding the filing of this application, made campaign contributions aggregating \$250.00 or more to a member of the Board of Commissioners?

Yes No AMIN PANJWANI (Your Name)

If the answer is yes, please complete the following section:

NAME AND OFFICAL POSITION OF GOVERNMENT OFFICIAL	CONTRIBUTIONS (List all which aggregate to \$250 or More)	DATE CONTRIBUTION WAS MADE (Within last two years)

Attach additional sheets if necessary to disclose or describe all contributions.

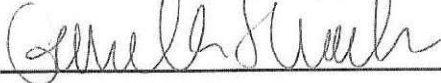


GWINNETT COUNTY
PLANNING AND DEVELOPMENT
PROPERTY TAX VERIFICATION

The undersigned certifies that all Gwinnett County property taxes are current. In no such case shall an application or reapplication be processed without such property tax verification.

Applicant Information


Name: Gabrielle Schaller, Attorney for Applicant

Signature: 

Date: June 23, 2025

Property Information

Enter all parcel IDs in the table below. Search for Parcel ID

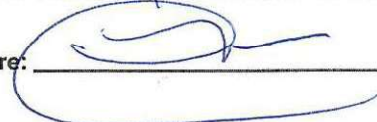
Parcel ID(s) (by Applicant)	Property Taxes Paid (Yes or No) (Tax Commissioner Use Only)
R6210 034	Yes 

Tax Commissioner Affirmation

All Gwinnett County property taxes are current for the above referenced parcel(s) and confirmed by the signature below.

Name: Jessilyn McJuffie

Title: TSA

Signature: 

Date: 6/24/25

**CONFLICT OF INTEREST CERTIFICATION FOR REZONING/CHANGE IN
CONDITIONS AND SPECIAL USE PERMIT APPLICATIONS**

MAHAFFEY PICKENS TUCKER, LLP

<u>NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL</u>	<u>CONTRIBUTION</u>	<u>DATE</u>
Nicole Love Hendrickson, Chairwoman	\$3,300	10/01/2024
Ben Ku, Commissioner	\$2,000	04/25/2025
Matthew Holtkamp, Commissioner	\$2,000	06/19/2025

Shane M. Lanham, Attorney at Law, PC

<u>NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL</u>	<u>CONTRIBUTION</u>	<u>DATE</u>
Nicole Love Hendrickson, Chairwoman	\$500.00	10/02/2024

Updated 08/29/2025

Exhibit F: DWR Sewer Capacity Certification Letter

[attached]



June 11, 2025

Alex Panjwani
Jupiter Development, Inc.
2335 Pleasant Hill Rd.
Duluth, GA. 30096

<input checked="" type="checkbox"/>	APPROVED
<input type="checkbox"/>	DENIED
<input type="checkbox"/>	CONDITIONALLY APPROVED
Sewer Capacity Request #C2025-140-06	
Expiration Date: 06/11/2026	
Tie-In Manhole FID: 3320587	

RE: Sewer Availability for Proposed Development – Satellite Retail Center
Parcel ID 6210 034

Dear Mr. Panjwani:

Gwinnett County Department of Water Resources (GCDWR) has reviewed the impact of the proposed development consisting of 60 restaurant seats, 100 bar seats, and 16,929-sf of retail space on the above parcel and determined:

Gwinnett County has adequate sewer capacity to serve the proposed development.

This confirmation is based on your anticipated annual average daily flow of **6.94 gpm** discharging to the sewer tie-in manhole at Facility ID **3320587**.

Capacity Allocation: Sewer capacities are allocated only upon the issuance of a Development Permit, by Gwinnett County Planning & Development and after all applicable conditions established in this Sewer Capacity Certification, if any, are met.

Certification Expiration: The Sewer Capacity Certification is valid for 12 months from the date of this letter. If rezoning is approved, GCDWR will issue a revised Sewer Capacity Certification based on the approved units, as needed. The capacity request can be renewed for an additional 12 months. If a Development Permit has not been obtained by the expiration date above the developer may apply for renewal by providing evidence of active rezoning, plan review, or permitting efforts.

Once the Sewer Capacity Certification expires, the capacity is no longer allocated, and a new Sewer Capacity Certification request must be submitted for re-evaluation with no guarantee of available capacity. The evaluation will be based on the updated conditions of the sewer system.

Certification Invalidation: This certification is not valid if there are proposed changes to your development that could impact downstream sewer capacity (e.g. land use density, sewer tie-in



manhole, parcel changes, and/or rezoning). In this case the development must resubmit for a new Sewer Capacity Request with updated information prior to the expiration of the previous one.

Please contact us at 678-376-7026 if you have any questions.

Sincerely,

GWINNETT COUNTY DEPARTMENT OF WATER RESOURCES

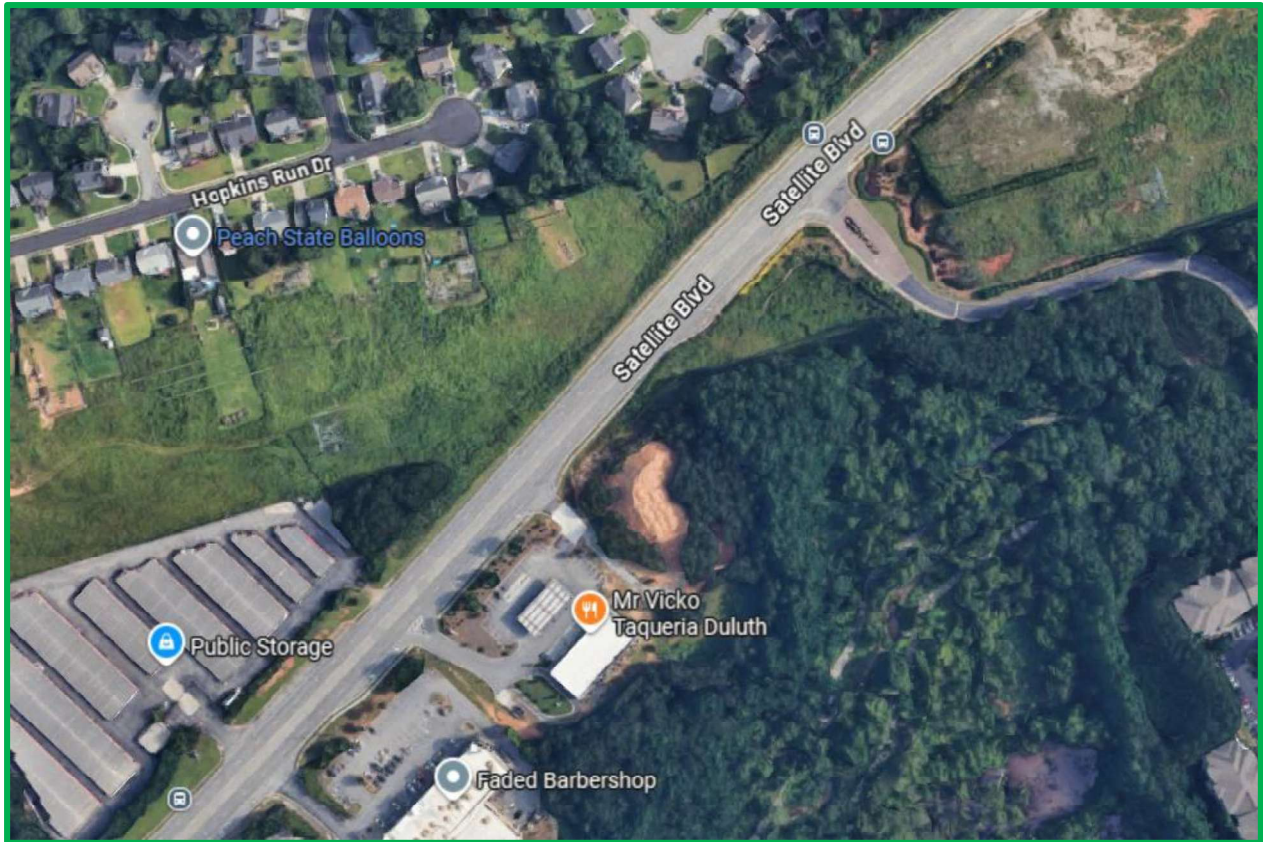
Della Taylor, PE
Division Director, Infrastructure Support
678.376.2130

C: Gwinnett County DWR – Lorraine Campagne, Section Manager; Raghu Vemuru, Engineer V; Mikala Weston, Engineer III
Gwinnett County P&D – Charli Young, Planning Manager; Geniva Sylvain, Engineer I; Christina Dejarnette, Engineer I;
Wendell Tumale Engineer III

Exhibit G: Traffic Impact Study

[attached]

A LEVEL 2 TRAFFIC IMPACT STUDY
for
SATELLITE BOULEVARD RETAIL CENTER
4400 SATELLITE BLVD
GWINNETT COUNTY, GEORGIA



Prepared for

Evans Design Group, Inc.

4755 Summer Song Court
Buford, Georgia 30519

September 29, 2025

Prepared by





9/27/2025
MR



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Introduction

The purpose of this study is to examine and evaluate the traffic impacts occurring at the existing intersection on Satellite Blvd and the BP driveway in Gwinnett County, Georgia, as shown in Figure 1. The traffic analysis evaluates the current operations as stated in the current edition of the Gwinnett County Traffic Impact Study Guidelines for a Level 2 study. The proposed development will be a strip retail plaza consisting of 1 - 3,023 s.f. restaurant and 11- retail stores totaling 16,810 s.f. for a gross floor area of 19,833 s.f.

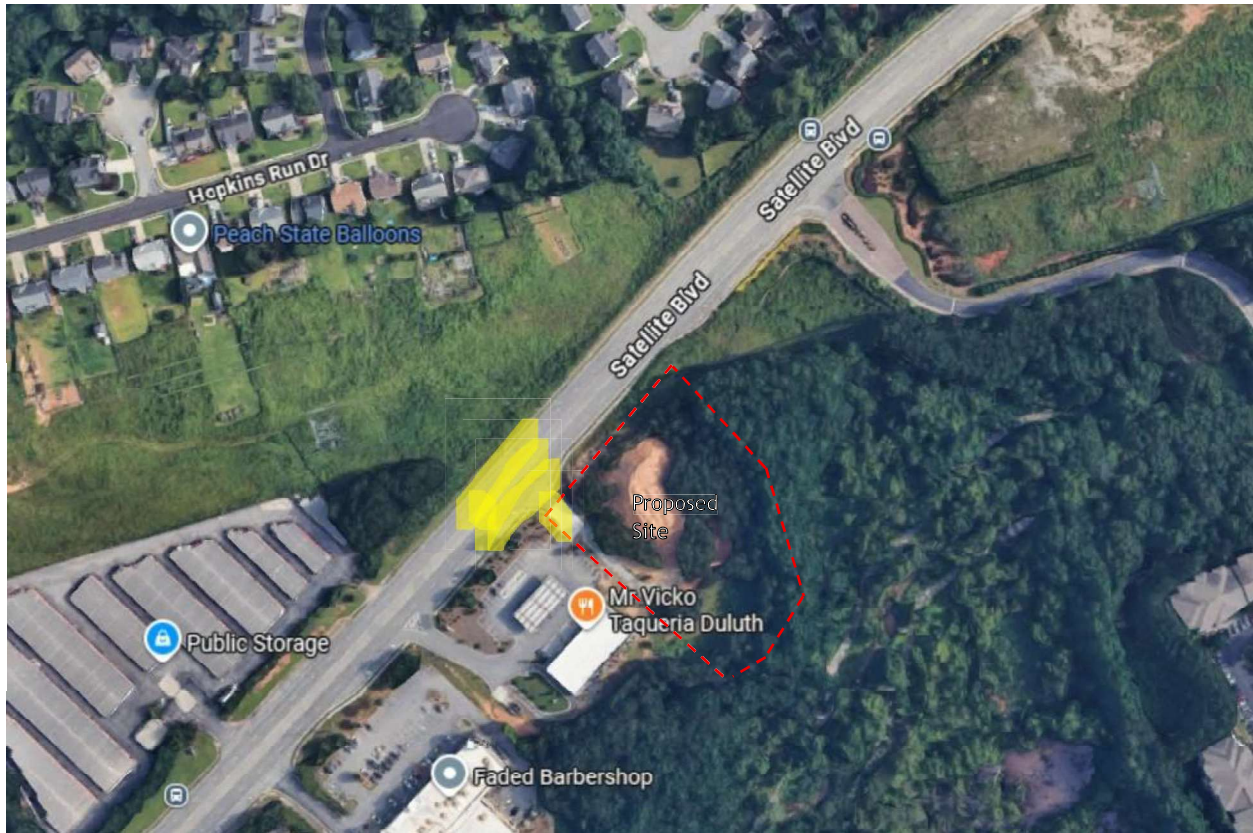


Figure 1. Location Map & Study Area

Access to the proposed development will consist of an existing driveway that will operate as a joint use driveway with the adjacent convenience store and gas station. The joint use driveway will have full access to Satellite Blvd.

The AM and PM peak hours have been analyzed in this study. In addition to the site access point, this study includes the evaluation of traffic operations at the existing driveway, that will be used jointly with the proposed retail plaza, and Satellite Blvd intersection.

Recommendations to improve traffic operations have been identified as appropriate and are discussed in detail in the following sections of the report. The location for the development and the surrounding roadway network within the study area are highlighted in Figure 1.

Existing Facilities / Conditions

The following is a brief description of each of the roadway facilities located in the study area of the site:

Satellite Boulevard - is a five (5) section roadway with a two-way-left-turn lane in the center and a posted speed limit of 45 mph traveling in a northeast - southwest direction. Satellite Blvd is functionally classified as a Major Collector. There are sidewalks on both sides of Satellite Blvd.

Existing BP Driveway - currently the driveway having access to Satellite Blvd is serving the BP gas station and convenience store. The driveway is fully accessible for left and right turning movements and is stop sign controlled. The driveway's width is approximately 30 feet from face-of-curb to face-of-curb. There is a 165-foot right turn deceleration lane with a 63-foot opening for weave maneuvering from the upstream driveway. Left turns into the driveway are made from the TWLTL. The sidewalk at the driveway is ADA accessible. The nearest traffic signal is approximately 1625 feet southwest of the driveway at the Paragon Retail Center.

Existing Transit Routes and Stops

The Gwinnett Transit Authority - Ride Gwinnett, operates buses along Satellite Blvd on Route 10a. There are two stops on Satellite Blvd, 1) approximately 860 feet south of the driveway and 2) 690 feet north of the BP driveway.

Study Methodology

In this study, the methodology used for evaluating traffic operations at the study area intersection is based on the criteria set forth in the Gwinnett County Traffic Impact Study Guidelines for a Level 2 study. Synchro software, which utilizes the Highway Capacity Manual (HCM) methodology, was used for the analysis. ITE Trip Generation, 11th Edition was used to analyze the land uses in the project. The methodology for this study was presented to Brent Hodges, Zoning and Development Review Manager in the Department of Transportation and approved.

Unsignalized Intersections

For unsignalized intersections at which the side street or minor street is controlled by a stop sign, the criteria for evaluating traffic operations are the level-of-service (LOS) for the turning movements at the intersections and/or the approaches to the intersections. Level-of-service is based on control delay incurred at the intersection. Control delay for unsignalized intersections includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. Several factors affect the control delay for unsignalized intersections, such as the availability and distribution of gaps in the conflicting traffic stream, critical gaps, and follow-up time for a vehicle in the queue.

Level-of-service is assigned a letter designation from “A” to “F.” Level-of-service A indicates excellent operations with little to no delays to motorists, while LOS F exists whether are insufficient gaps of acceptable size to allow vehicles on the side street to cross safely, resulting in extremely long total delays and long queues. The LOS criteria for two-way stop controlled and all-way stop sign-controlled intersections are given in Table 1.

Table 1 - Level-of-Service Criteria for Unsignalized Intersections	
Level-of-Service	Control Delay (sec)
A	≤ 10
B	>10 and ≤ 15
C	>15 and ≤ 25
D	>25 and ≤ 35
E	>35 and ≤ 50
F	>50

Source: Highway Capacity Manual

Existing 2025 Traffic Analysis

Existing Traffic Volumes

Existing traffic turning movement counts were obtained at the following intersection:

1. Satellite Boulevard at the BP Driveway

Turning movement counts were collected on Wednesday, September 17, 2025. All turning movement counts were recorded during the AM and PM peak hours between 7:00 AM to 9:00 AM and 4:00 PM to 6:00 PM, respectively. The four consecutive 15-minute interval volumes that summed to produce the highest volume at the intersections were then determined. These volumes make up the peak hour traffic volumes for the intersections counted and are shown in Figure 2.

Twenty-four-hour traffic data was collected on Satellite Blvd on Wednesday, September 17, 2025. In summary the 24-hour count on Satellite Blvd was 20,474, with 9,391 (45.8%) vehicles in the northbound direction and 11,083 (54.2%) vehicles in the southbound direction.

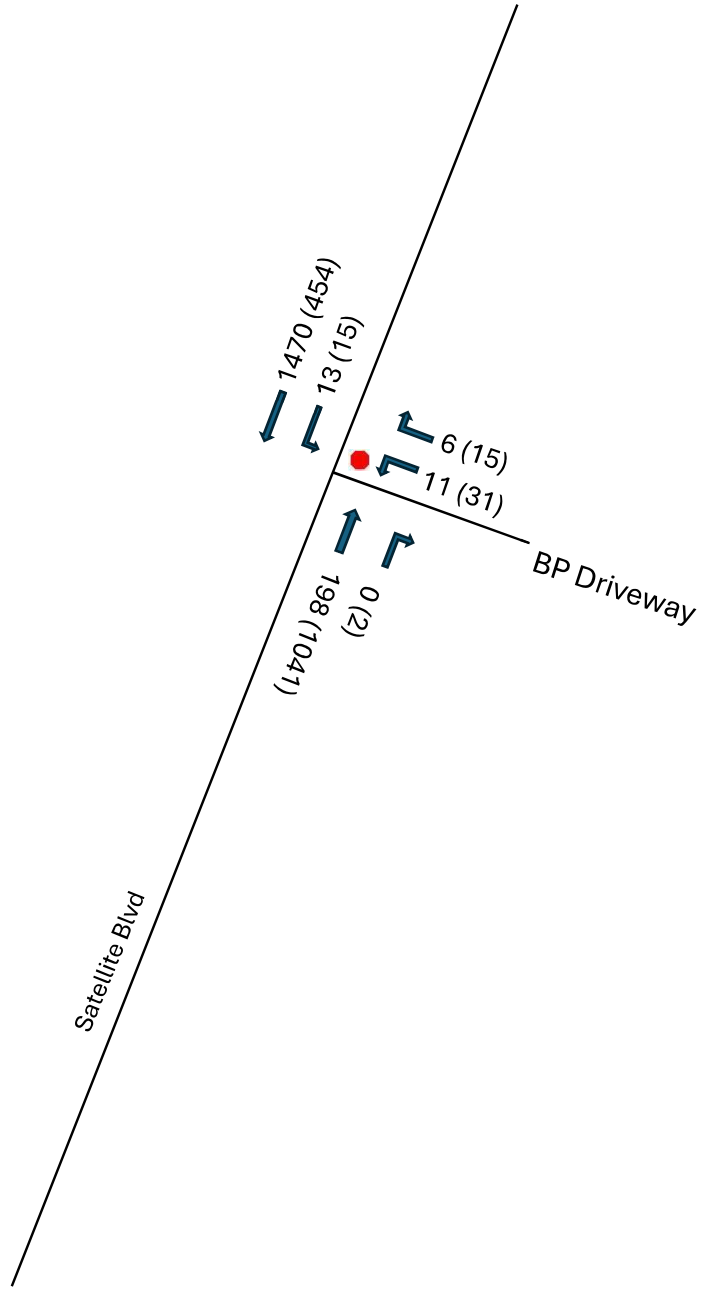
Annual Traffic Growth

Historic traffic data was used as provided by the Georgia Department of Transportation to calculate the annual traffic growth in this area of the proposed site development. Per the GDOT TADA database, Count Station #135-0611, over a 4-year period from 2021 to 2025, there was a 3.08% increase in traffic volume along Satellite Blvd. for an average annual growth rate of 0.77%. A 1.0% growth rate was agreed to by Gwinnett County and was used in the traffic projections.

Projected Traffic Volumes

The 1% annual growth rate was used to project the ADTs to the base or opening year of the proposed development and the 20-year horizon as shown in Table 2.

Table 2. Projected Traffic Volumes				
Roadway	Current ADT Volumes	FV = PV(1+r%) ^t	Opening year (2027)	20-Year Horizon (2047)
Satellite Blvd	20,474		20,886	25,484



AM (PM)

FIGURE 2. EXISTING PEAK HOUR VOLUMES – 2025

Existing Traffic Operations

Existing 2025 traffic operations were analyzed at the study intersections in accordance with the HCM methodology. The results of the analyses are shown in Table 3.

TABLE 3 - EXISTING INTERSECTION OPERATIONS			
Intersections	Traffic Control	LOS (Delay, sec.)	
		AM Peak Hour	PM Peak Hour
Satellite Blvd @ BP driveway	Stop Sign		
Westbound		B (13.7)	C (21.8)
Southbound Left		A (7.70)	B (11.1)

The results of existing traffic operations analysis indicate the stop sign controlled intersection of Satellite Blvd and the BP driveway is operating at an acceptable level-of-service “C” or better during the AM and PM peak hours.

The existing traffic control and lane geometry for the intersection is shown in Figure 3.

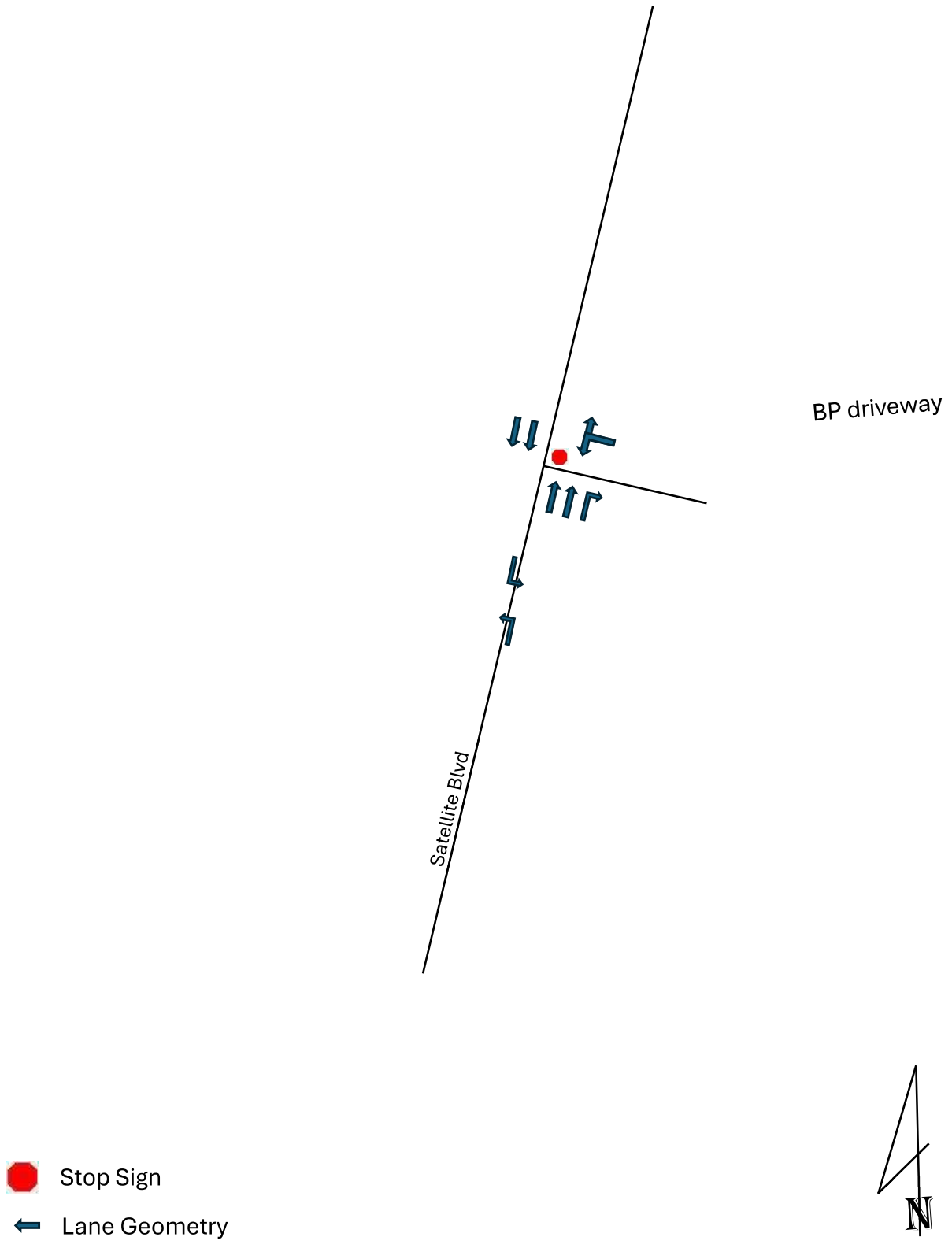


FIGURE 3. Existing Traffic Control and Lane Geometry

Proposed Development

The purpose of this study is to examine and evaluate the traffic impacts occurring at the existing intersection on Satellite Blvd and the BP driveway in Gwinnett County, Georgia. The proposed retail development will consist of 1 restaurant and 11 retail stores for a complete gross floor area of 19,833 square feet. A site plan is shown in Figure 4.

The development proposes access at the following location:

- Site Driveway 1: Fully accessible driveway accessing the existing joint use driveway

Trip Generation

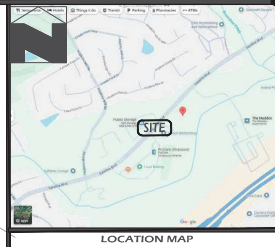
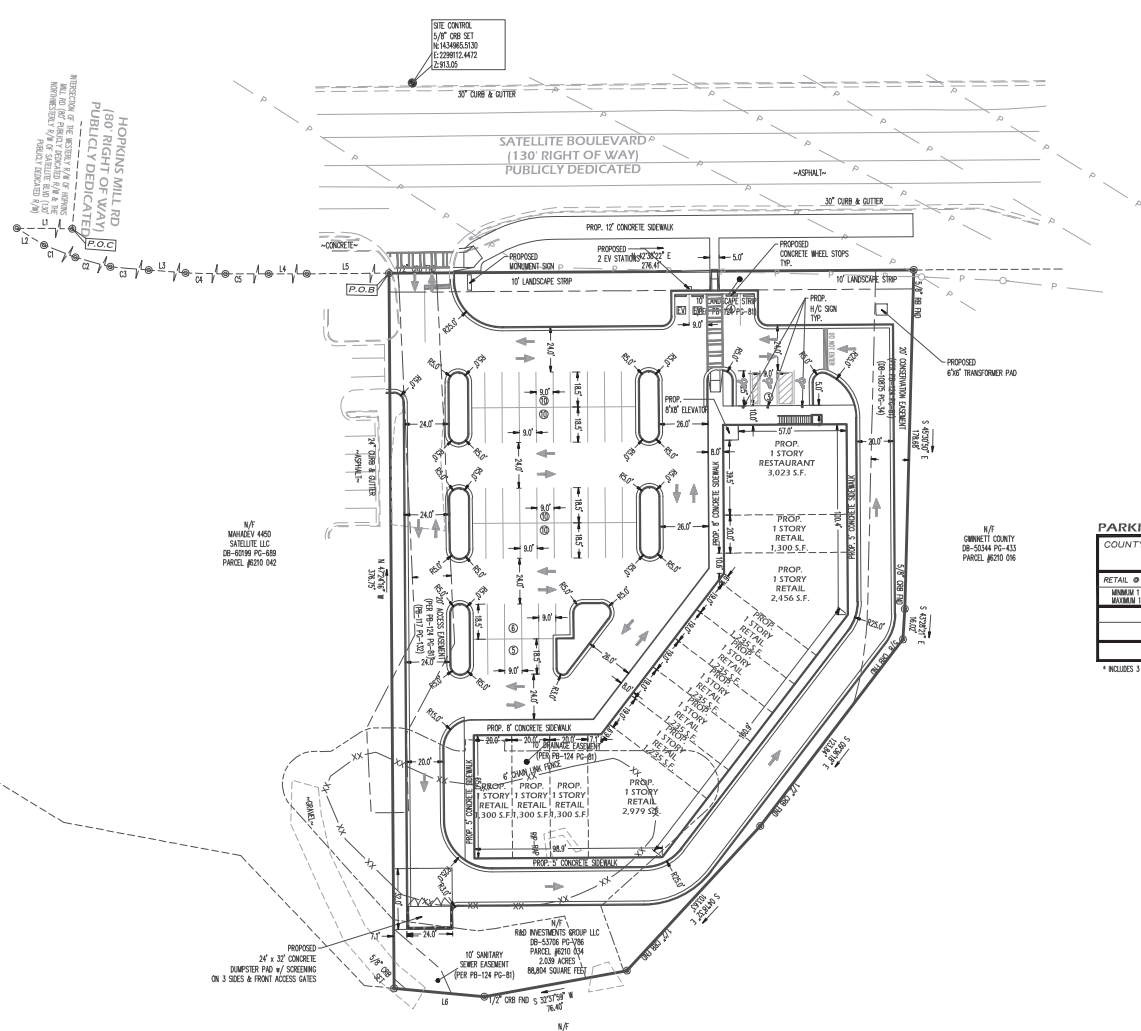
Trip generation estimates for the project were based on the rates and equations published in the 11th Edition of the Institute of Transportation Engineers (ITE) Trip Generation report. This reference contains traffic volume count data collected at similar facilities nationwide. The trip generation was based on the following ITE Land Use: 822 - *Strip Retail Plaza <40ksf*. The calculated total trip generation for the proposed development is shown in Table 4.

As shown in Table 4, the trips generated by this development consist of primary trips only. This is due to the nature of the proposed land uses. Primary trips are new trips on the roadway network. Primary trips typically enter the site and exit in the direction from which they entered or back to its origin.

Land Use (Code)	Intensity	Daily Trips	AM Peak Hr.			PM Peak Hr.		
			Enter	Exit	Total	Enter	Exit	Total
Strip Retail Plaza (<40k) (822)	19.833 sf	1,080	28	19	47	66	65	131

Trip Distribution

Trip distribution for the land use is calculated using the data provided by ITE Trip Generation. The trip distribution determines the percentage of trips entering and exiting the proposed site. The total trip patterns for the proposed land uses will be consistent with the existing background traffic. After the trips were distributed along the existing roadway network, they were then assigned to the access point of the proposed development with a certain level of reasonableness, based on the location of the driveways and the land use layout, as shown in Figure 5.



ZONING DATA TABLE:

CURRENT ZONING	HR
PROPOSED ZONING	C2
OVERLAY DISTRICT	GATEWAY 85 OVERLAY DISTRICT
PROPOSED USE ALLOWED IN ZONING	YES
MINIMUM LOT AREA	N/A
MINIMUM LOT WIDTH	N/A
MINIMUM FRONT SETBACK	N/A
MAXIMUM BUILDING HEIGHT	45'
MAXIMUM COVER	80% (TOTAL COVERAGE 78.8%)
BUILDING SETBACKS	
FRONT	0'
SIDE	0'
REAR	0'
LANDSCAPE STRIPS	10'
OTHER	
BUFFER	
ZONING BUFFER	50' ABUTTING RESIDENTIAL
OTHER	

THIS PROPERTY DOES NOT LIE IN A 100-YR FLOOD ZONE AS SHOWN ON FIRM COMMUNITY PANEL NO. 233330000E DATED MARCH 4, 2015.

PARKING REQUIREMENTS:

COUNTY REQUIREMENTS	REQUIRED MINIMUM	REQUIRED MAXIMUM	TOTAL PARKING PROVIDED
RETAIL @ 24,842 S.F.			
MINIMUM 1 SPACE PER 500 S.F.	50	83	58
MAXIMUM 1 SPACE PER 300 S.F.			
TOTAL:			58*

* INCLUDES 3 ADA SPACES (3 REQUIRED)



PROJECT NAME
SATELLITE BLVD. RETAIL CENTER

4400 SATELLITE BLVD.,
LL-210, 6TH DIST.,
GWINNETT COUNTY,
GEORGIA

DATE:
02-21-25

DESIGN BY: JME
DRAWN BY: JME
CHECKED BY: JME

Not Released For Construction
 Released For Construction

OWNER/DEVELOPER

© COPYRIGHT 2025
EVANS DESIGN GROUP, INC.
ALL RIGHTS RESERVED.

REVISIONS

JOB NUMBER:
25-009

SHEET TITLE
SITE PLAN

C-4



UTILITY DISCLAIMER:
IN ADDITION TO SHOWING THE STRUCTURES TO BE BUILT UNDER THIS CONTRACT, THE DRAWINGS SHOW CERTAIN INFORMATION OBTAINED BY THE ENGINEER REGARDING THE PIPES, POLE LINES, CONDUITS, AND OTHER STRUCTURES WHICH EXIST ALONG THE LINE OF THE WORK, BOTH AT AND BELOW THE SURFACE OF THE GROUND. THE ENGINEER AND THE OWNER EXPRESSLY DISCLAIM ANY RESPONSIBILITY FOR THE ACCURACY OR COMPLETENESS OF THE INFORMATION GIVEN ON THE DRAWINGS WITH REGARD TO EXISTING STRUCTURES, AND THE CONTRACTOR WILL NOT BE ENTITLED TO ANY EXTRA COMPENSATION ON ACCOUNT OF ANY INACCURACY OR INCOMPLETENESS OF SUCH INFORMATION, SAID STRUCTURES BEING INDICATED ONLY FOR THE CONVENIENCE OF THE CONTRACTOR, WHO MUST VERIFY THE INFORMATION TO HIS OWN SATISFACTION. THE OWNING OF THIS INFORMATION UPON THE CONTRACT DRAWINGS WILL NOT RELIEVE THE CONTRACTOR OF HIS OBLIGATION TO SUPPORT AND PROTECT ALL PIPES, CONDUITS, AND OTHER STRUCTURES. THE CONTRACTOR SHALL LOCATE ALL UNDERGROUND OBSTRUCTIONS PRIOR TO CONSTRUCTION SO AS TO PREVENT ANY DAMAGE TO THOSE SERVICES OR OTHER UTILITIES. ANY SUCH DAMAGES MUST BE REPAIRED WITHOUT DELAY AND THE COST OF SUCH REPAIRS MUST BE BORNE BY THE CONTRACTOR.

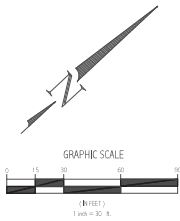
LINE TABLE

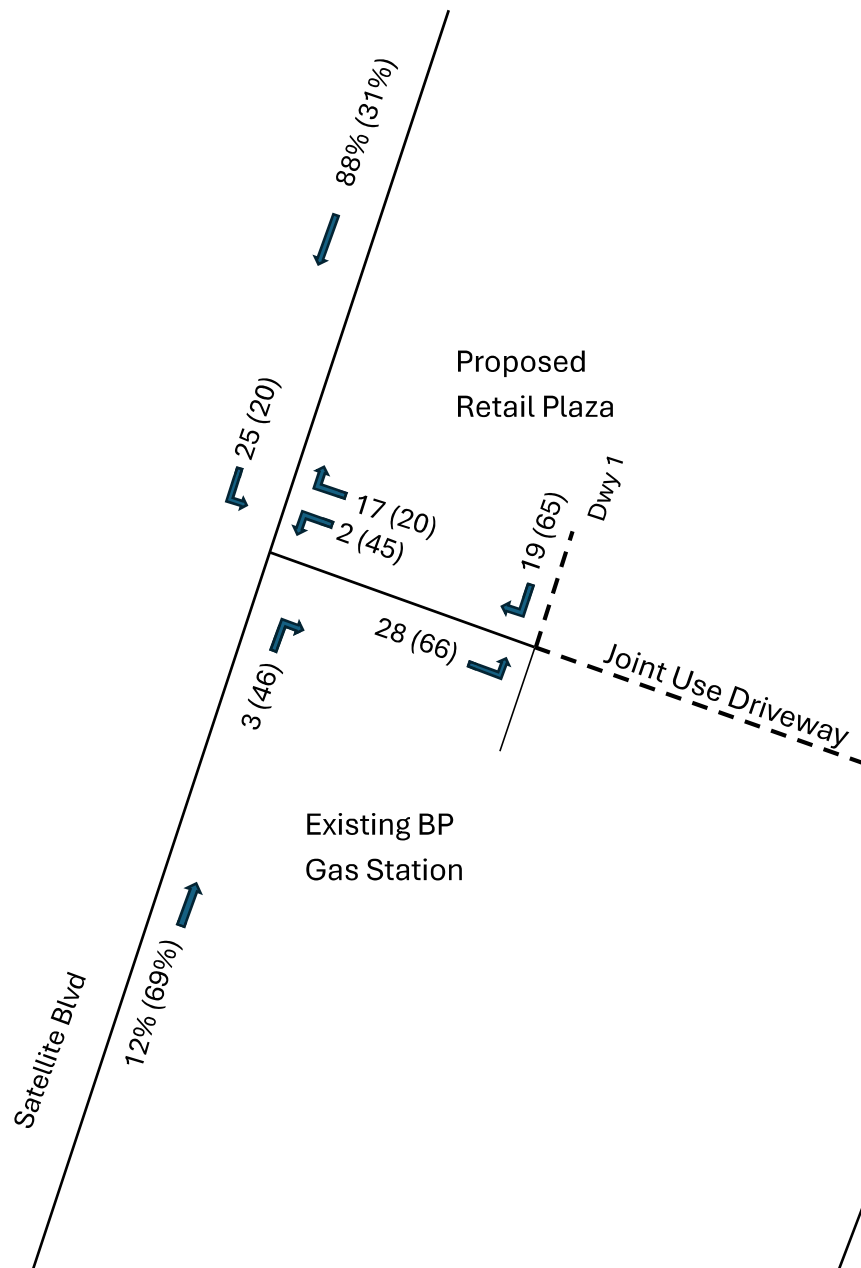
LINE	BEARING	DISTANCE
1	S. 43°07'17" W	285.57'
2	N. 4°01'00" E	1.00'
3	N. 47°25'17" E	382.50'
4	N. 8°58'00" E	1.00'
5	N. 42°38'19" E	271.88'
6	S. 48°20'08" W	40.77'

CURVE TABLE

CURVE	RADIUS	ARC LENGTH	CHORD LENGTH	CHORD BEARING
C1	1469.65'	189.81'	189.80'	N. 89°39'21" E
C2	1469.65'	188.57'	188.56'	N. 89°52'49" E
C3	1469.65'	187.83'	187.83'	N. 89°58'43" E
C4	2078.75'	263.53'	263.56'	N. 46°52'26" E
C5	2078.75'	143.04'	143.04'	N. 47°32'46" E

OWNER/DEVELOPER:
NAME:
ADDRESS:
PHONE:
EMAIL:
24-HR CONTACT:
NAME:
PHONE:
EMAIL:
ENGINEER:
EVANS DESIGN GROUP, INC.
4755 SUMMER SONG COURT
BUFORD, GA 30519
678.207.6830 (Ph)
CONTACT: JON M. EVANS, P.E.





AM (PM)

FIGURE 5. TRIP DISTRIBUTION AND SITE GENERATED WEEKDAY PEAK HOUR VOLUMES - Strip Retail Plaza

AM (PM)

Sight Distance

Since this is an existing driveway the sight distance for left and right turns met the minimum criteria in accordance with the AASHTO Intersection Sight Distance guidance for Left Turn from the Minor Road - Case B1 and Right Turn from the Minor Road - Case B2.

Future Identified Projects

Gwinnett County Department of Transportation does not have any future improvement programs or projects identified for implementation in the vicinity of the proposed site development.

Turn Lane Analysis

Included below are analysis for left-turn lanes and deceleration lanes for all site driveways per GDOT Regulations for Driveways and Encroachment Control manual. The analysis below are based off trip distribution included in aforementioned **Trip Distribution** section. According to the trip distribution, 540 vehicles will enter and 540 will exit the strip retail plaza site daily.

Tables 5 shows the minimum GDOT required number of right-turn vehicles to warrant a right-turn deceleration lane.

Table 5. GDOT Minimum Volumes Requiring Right Turn Lanes				
Intersections	Right turn traffic (% total entering)	Right-turn vol (veh/day)	Roadway Speed/Lanes/AADT	GDOT Threshold (veh/day)
Satellite Blvd @ BP Driveway	45.8%	540x0.458 = 247	45 mph / >2-lanes / >10,000	75

Tables 6 shows the minimum GDOT required number of left-turn vehicles to warrant a left-turn lane.

Table 6. GDOT Requirements for Left Turn Lanes				
Intersections	Right turn traffic (% total entering)	Right-turn vol (veh/day)	Roadway Speed/Lanes/AADT	GDOT Threshold (veh/day)
Satellite Blvd @ BP Driveway	54.2%	540x0.542 =293	45 mph / >2-lanes / >10,000	250

Both maneuvers into the joint use driveway for the proposed retail site warrants an auxiliary turn lane.

Summary Findings

The results of this Level 2 Traffic Impact Study for the proposed redevelopment of a strip retail plaza that is less than 40,000 square feet overall indicate that, under existing conditions, the stop sign controlled driveway intersection with Satellite Blvd currently operating at a Level of Service (LOS) C or better during the AM and PM peak hour.

Turn lane analyses were conducted at the proposed site's access points. Both left turn and right turn maneuvers warranted turn lanes. Currently, there exists a right turn deceleration lane approximately 165 feet in length and the TWLTL is used for left turns.

Recommendations

Based on the results of the findings and analyses, it is recommended that Gwinnett County to continue monitoring the joint use driveway at Satellite Blvd for any changes that may cause the level of service to decrease - including approach delays, volume to capacity ratio or crashes.

Exhibit H: Public Participation Plan and Report

[attached]

Public Participation Plan

- 1. Who do you intend to contact in addition to property owners within 1,000 feet of the site, if any?**

District Planning Commissioners and District Commissioner.

- 2. Where do you plan to host the Public Participation Meeting? Ensure that the meeting is held in person at a convenient location for interested parties.**

Shorty Howell Park Activities Building, 2750 Pleasant Hill Road, Duluth, GA 30096

- 3. What date and time do you plan to host the Public Participation Meeting? Ensure that the meeting is held at a convenient time for interested parties, e.g. not during typical work hours on weekdays.**

February 11, 2026 at 6:30 pm.

- 4. What is your method for providing opportunities for discussion with interested parties at the Public Participation Meeting?**

After a presentation, the Applicant team would open the meeting for comments and questions from the attendees.

Public Participation Report

1. **List all groups that you notified of the requested application. Include a copy of the notification package and stamped Postal Service Form 3877 (attached) with itemized name and address of each addressee.**

Please see attached.

2. **Provide the date, time, and location of all meeting(s) that the applicant and/or representative attended to discuss an application with interested parties.**

February 11, 2026 at 6:30 pm at Shorty Howell Park Activities Building, 2750 Pleasant Hill Road,

Duluth, GA 30096

3. **Provide the number of people who participated in the meeting(s). Include the sign-in sheet(s) with meeting date, time, location, and attendee names.**

1 person attended and participated in the meeting.

4. **What issues and concerns were expressed by attendees at the meeting(s)?**

The attendee was the neighboring property owner, and his concern was solely about the potential tenants of the proposed development. Specifically, he was concerned about a convenience store, but was fine with other retail. He also wanted to ensure he knew when and where the public hearings would be.

5. **What are the applicant's responses to the issues and concerns that were expressed at the meetings? What changes to the development will be made?**

The applicant team told him that the current tenants do not include a convenience store, but that we are still waiting on inquiries. We also asked for his contact information so that we can provide him any updates. We told him that public hearings for both Planning Commission and Board of Commissioners are held at the Gwinnett Justice Center, and that our hearings are currently scheduled for April 14 and April 28. We also told him he would receive notices of the hearings just as he received a notice of the meeting tonight.

Exhibit I: Internal and External Agency Review Comments

[attached]



**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		02/20/2026	
Department/Agency Name:		Transportation	
Reviewer Name:		Brent Hodges	
Reviewer Title:		Construction Manager 2	
Reviewer Email Address:		Brent.Hodges@gwinnettcountry.com	
Case Number:		REZ2026-00011	
Case Address:		4400 Satellite Boulevard, Duluth, 30096	
Comments:		<input checked="" type="checkbox"/>	<input type="checkbox"/>
		YES	NO
1	Satellite Boulevard is a major arterial. The Average Daily Traffic (ADT) Volume is 23,547.		
2	The nearest Ride Gwinnett facility (Stop 1105 / Route 10A) is located 600' away at Satellite Boulevard and Menlo Creek apartments.		
3	As shown on the zoning site plan, the developer shall construct a 12' concrete trail along the Satellite Boulevard frontage, per section 360-100 of the UDO.		
4			
5			
6			
7			
Recommended Zoning Conditions:		<input type="checkbox"/>	<input checked="" type="checkbox"/>
		YES	NO
1			
2			
3			
4			
5			
6			
7			

Note: Attach additional pages, if needed

Revised 7/26/2021

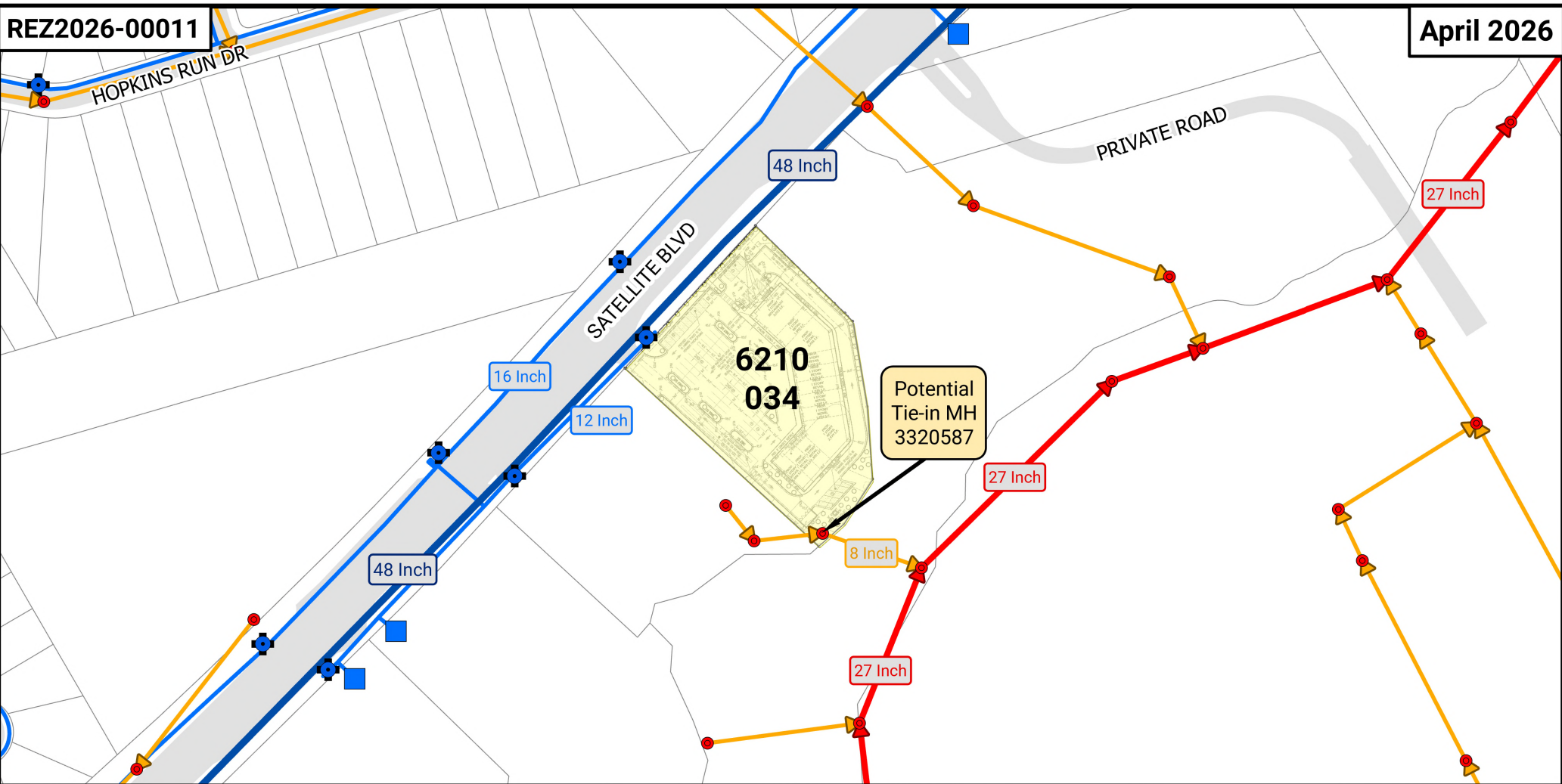


**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:			
Department/Agency Name:		DWR	
Reviewer Name:		Lorraine Campagne	
Reviewer Title:		Section Manager	
Reviewer Email Address:		Lorraine.Campagne@gwinnettcountry.com	
Case Number:		REZ2026-00011	
Case Address:		4400 Satellite Blvd	
Comments:		<input checked="" type="checkbox"/>	<input type="checkbox"/>
		YES	NO
1	Water: The proposed development may connect to an existing 12-inch water main located along the southern right-of-way of Satellite Boulevard. The existing 48-inch water main on the southern right-of-way of Satellite Boulevard must be avoided during all phases of construction, including the construction of deceleration lanes and connection to utilities.		
2	Sewer: A Sewer Capacity Certification (C2025-140-06) was approved in June 2025 for 60 restaurant seats, 100 bar seats, and 16,929 square feet of retail space. The proposed development plans to connect to an existing 8-inch gravity sewer located on the subject site.		
3			
4			
5			
6			
7			
Recommended Zoning Conditions:		<input type="checkbox"/>	<input checked="" type="checkbox"/>
		YES	NO
1			
2			
3			
4			
5			

Note: Attach additional pages, if needed

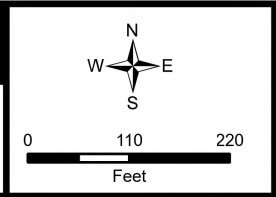
Revised 7/26/2021



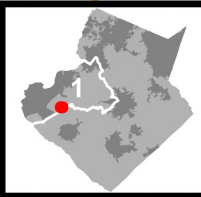
LEGEND

- Flow Management
- Pump Station
- Regional
- Manhole
- Hydrant
- City
- Water Main
- Proposed Water Main
- Sewer Collector
- Sewer Interceptor
- Sewer Force Main
- Proposed Sewer Collector

4400 Satellite Blvd
HRR to C-2
Water & Sewer
Utility Map



LOCATION



Water Comments: The proposed development may connect to an existing 12-inch water main located along the southern right-of-way of Satellite Boulevard. The existing 48-inch water main on the southern right-of-way of Satellite Boulevard must be avoided during all phases of construction, including the construction of deceleration lanes and connection to utilities.

Sewer Comments: A Sewer Capacity Certification (C2025-140-06) was approved in June 2025 for 60 restaurant seats, 100 bar seats, and 16,929 square feet of retail space. The proposed development plans to connect to an existing 8-inch gravity sewer located on the subject site.

Water Availability: Water demands imposed by the proposed development may require upsizing or extensions of existing water mains in order to meet Gwinnett County Standards and fire flow demands. Any cost associated with such required improvements will be the responsibility of the development. Current Gwinnett County Standards require a minimum of 12" pipe size for commercial developments and a minimum of 8" pipe size for residential developments. Additionally, connection to a minimum of 12" and 8" mains are required for commercial and residential developments, respectively. It is the responsibility of the developer's engineer to confirm pressure and volumes are available for the development.

Sewer Availability: A Sewer Capacity Certification must be obtained from Gwinnett County to confirm the existing system can serve the development. Sewer demands imposed by the proposed development may require upsizing and/or extensions of existing sewer mains, and/or upsizing of an existing pump station, and/or installation of a new pump station. Any cost associated with such required improvements will be the responsibility of the development. The developer shall provide easements for future sewer connection to all locations designated by Gwinnett County during plan review.

Water and Sewer Design and Construction Requirements: Extensions of the water and/or sanitary sewer systems within the subject development must conform to this department's policies, Gwinnett County's ordinances, and the Water Main and Sanitary Sewer Design and Construction Standards and Specifications, dated April 5th, 2016. Subsequent to design, construction, inspection, and final acceptance of the required utilities, service would then become available under the applicable utility permit rate schedules.

Private Road Developments: Any development with private roads must comply with the Standard Policy Requirement for the Installation of Water and Sanitary Sewer Mains within Private Developments. This policy stipulates minimum easement requirements and location of public mains and appurtenances, among other requirements.

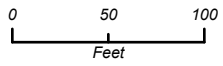
Exhibit J: Maps

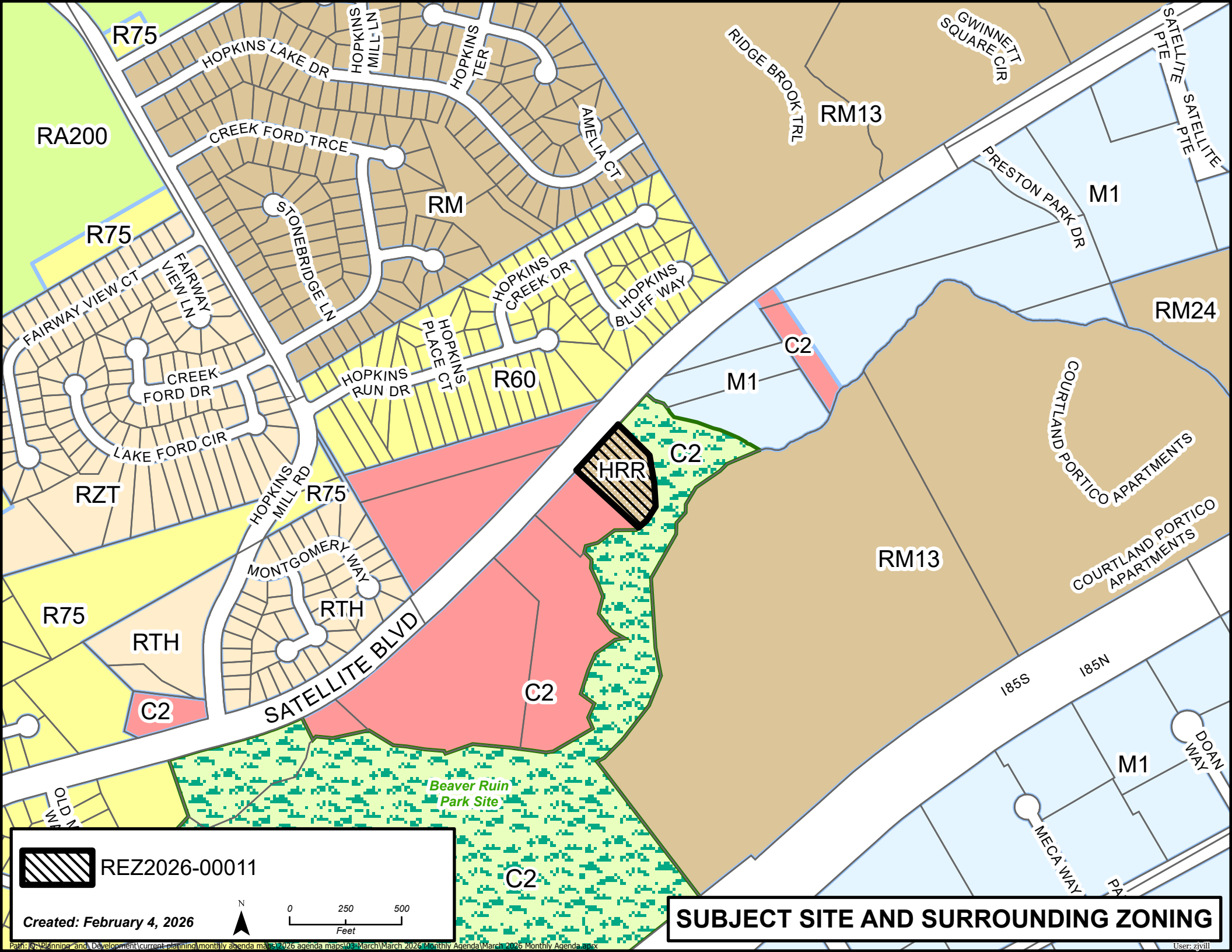
[attached]


SATELLITE BLVD

REZ2026-00011

Created: January 16, 2026



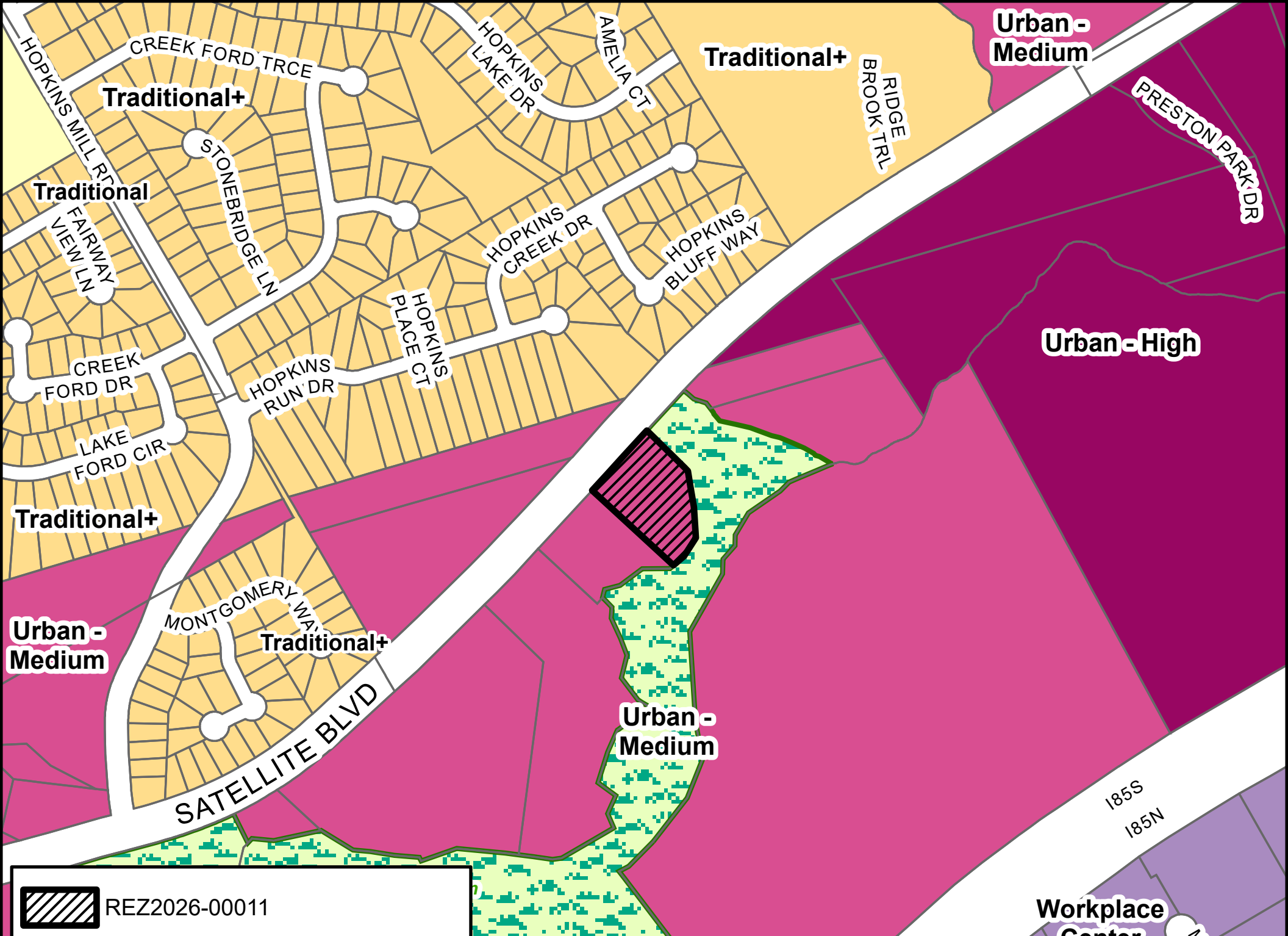


 REZ2026-00011

Created: February 4, 2026

0 250 500
Feet

SUBJECT SITE AND SURROUNDING ZONING



 REZ2026-00011

Created: January 16, 2026

0 200 400
Feet

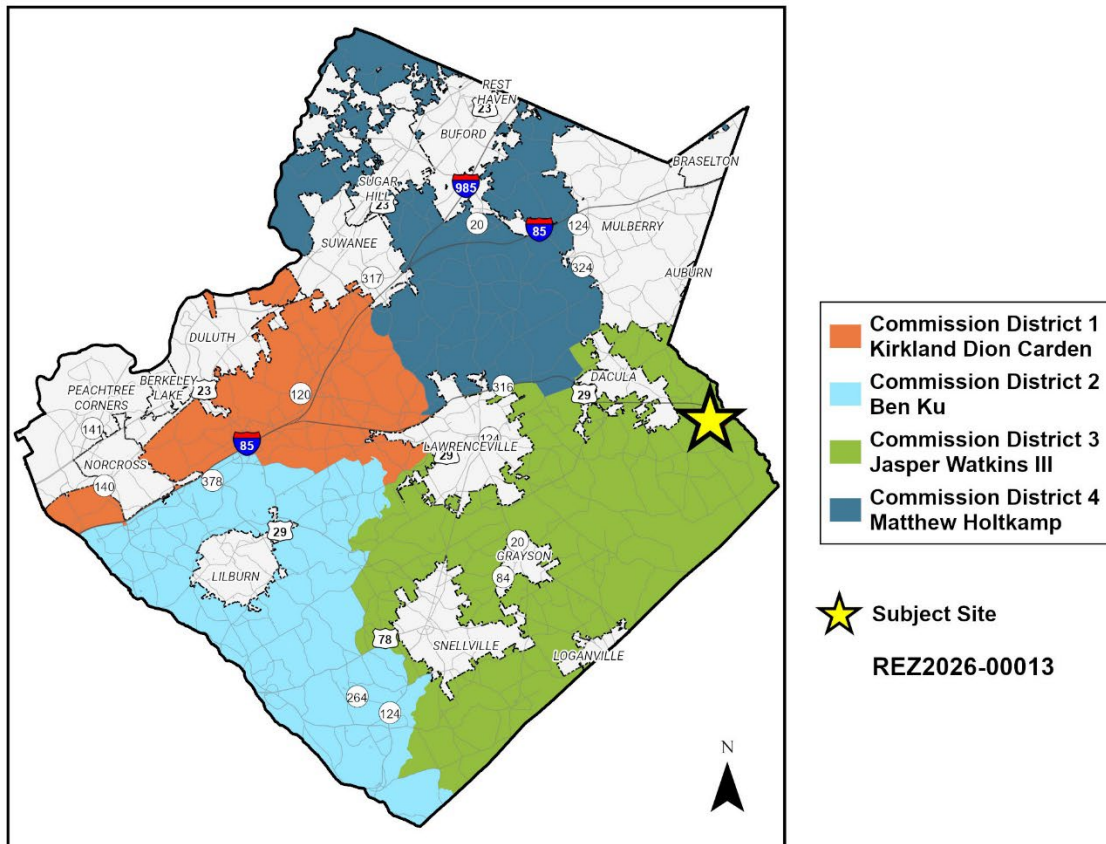
N

2045 UNIFIED PLAN FUTURE DEVELOPMENT CLASSIFICATION

PLANNING AND DEVELOPMENT DEPARTMENT CASE REPORT

Case Number: REZ2026-00013
Current Zoning: RA-200 (Agriculture-Residence District)
Request: Rezoning to **R-100** (Single-Family Residence District)
Address: 1790 Whitley Road
Map Number: R5358 001
Site Area: 13.14 acres
Units: 6
Proposed Development: Single-Family Detached Subdivision
Commission District: District 3 – Commissioner Watkins
Future Development Type: Rural

Staff Recommendation: **APPROVAL WITH CONDITIONS**



Applicant: DP2 HOLDINGS, LLC
1790 Whitley Road
Dacula, GA 30019

Owner: DP2 HOLDINGS, LLC
1790 Whitley Road
Dacula, GA 30019

Contact: Jakob Sullins

Contact Phone: 706.773.3849

Zoning History

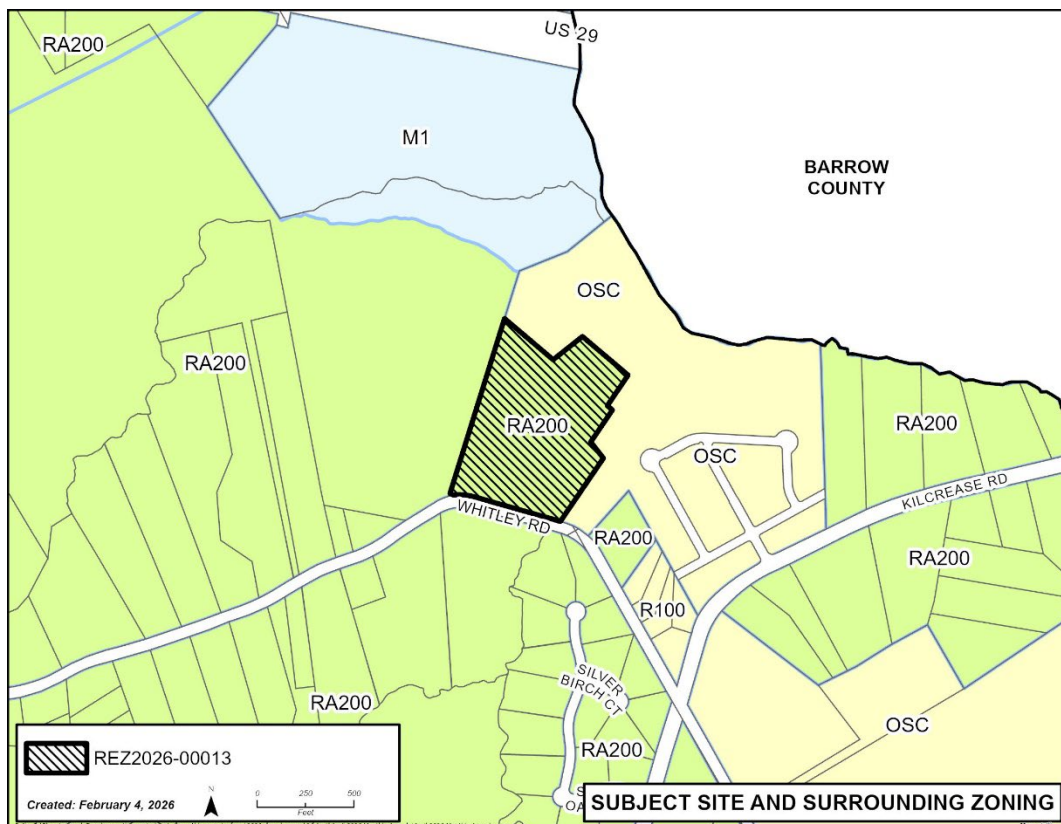
The subject property is zoned RA-200 (Agriculture-Residence District). In 2024, a rezoning request from RA-200 to R-60 (Single-Family Residence District) was denied, pursuant to RZR2024-00048.

Existing Site Condition

The subject property is a 13.14-acre parcel located along Whitley Road, west of its intersection with Kilcrease Road. The site contains a single-family residence with accessory structures. Floodplain is located in the northern portion of the property, and along the stream near the eastern property line. Dense vegetation exists in the western portion of the site. There is no sidewalk along the road frontage. The nearest Ride Gwinnett transit stop is approximately 10.5 miles from the subject property.

Surrounding Use and Zoning

The property is surrounded by single-family residences within subdivisions and on large lots, as well as undeveloped parcels. The Appaloosa Park subdivision, which is currently under development, is located to the north and east. A residence on a large lot and the River Birch Landing subdivision is to the south, across Whitley Road. An undeveloped parcel located within the Innovation Overlay District is to the west. The following is a summary of surrounding uses and zoning:



Location	Land Use	Zoning	Density
Proposed	Single-Family Residential	R-100	0.46 units per acre
North	Single-Family Residential (Appaloosa Park)	OSC	1.83 units per acre (approved)
East	Single-Family Residential (Appaloosa Park)	OSC	1.83 units per acre (approved)
South	Single-Family Residential	RA-200	0.12 units per acre
	Single-Family Residential (River Birch Landing)	RA-200	0.78 units per acre
West	Undeveloped	RA-200	N/A

Project Summary

The applicant requests rezoning from RA-200 to R-100 for a single-family detached subdivision, including:

- Six single-family residences on lots ranging from 0.64 acres to 5.30 acres, yielding a density of 0.46 units per acre.
- Two-story, front- and side-loaded residences with a minimum heated floor area of 2,800 square feet.
- Exterior building materials of brick on all façades.
- A 40-foot-wide setback with a 10-foot-wide landscape strip along Whitley Road.
- Lots to be accessed by a private drive within a 40-foot-wide access and utility easement.

Zoning and Development Standards

The following is a summary of applicable standards from the Unified Development Ordinance (UDO):

Standard	Required	Proposed	Meets Standard?
Lot Size	Minimum 15,000 square feet	Minimum 15,000 square feet	YES
Lot Width	Minimum 100'	Minimum 100'	YES
Lot Coverage	Maximum 45%	Maximum 45%	YES
Front Yard	Minimum 25'	Minimum 25'	YES
Rear Yard	Minimum 30'	Minimum 30'	YES
Side Yard	Minimum 10'	Minimum 10'	YES
Building Height	Maximum 35'	Maximum 35'	YES

Staff Analysis

Rezoning Request Analysis: The standards governing the exercise of zoning power are outlined in Section 270-20.6 of the UDO. Staff makes the following findings related to the zoning requests:

A. Whether the proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

The proposed rezoning would allow low-intensity single-family residences in a residential area. The proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

B. Whether the proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property.

The proposed development is compatible with the character and development pattern of nearby residences. The proposal would not adversely affect the existing use or usability of adjacent or nearby property.

C. Whether the property to be affected by a proposed rezoning has a reasonable economic use as currently zoned.

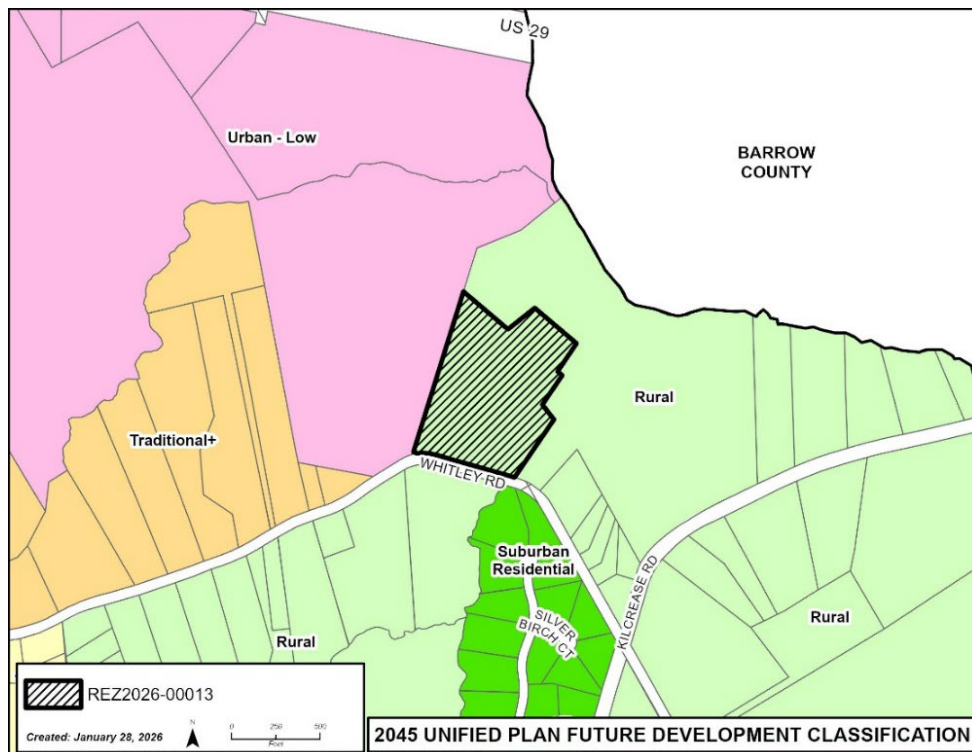
The property has a reasonable economic use as currently zoned.

D. Whether the proposed rezoning will result in a use which will or could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools.

An increase in impacts on public facilities would be anticipated in the form of traffic, utility demand, and stormwater runoff; however, these impacts would be mitigated with appropriate conditions, site development requirements, and planning. An increased impact is anticipated on school enrollment.

E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map; and

The Unified Plan's Future Development Map indicates "Rural" for the subject property, which strives to maintain a low-intensity, pastoral character of the outermost parts of the County. Single-family residential is a recommended land use and R-100 is a recommended zoning district. The proposal is in conformity with the policy and intent of the Unified Plan and Future Development Map.



F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning.

There are no other conditions giving support for approval or disapproval of the proposal.

Staff Recommendation

Based on the staff's evaluation of the request and the standards governing the exercise of zoning power, the Department of Planning and Development recommends **APPROVAL WITH CONDITIONS** of the rezoning request.

Staff Recommended Conditions

Approval as R-100 (Single-Family Residence District) for a single-family detached subdivision, subject to the following conditions:

1. The development shall be limited to single-family detached dwellings and accessory uses and structures, not exceeding six units.
2. The proposed development shall be constructed in general conformance with Exhibit B: Site Plan, dated received January 9, 2026, and Exhibit C: Building Elevations dated received January 6, 2026, with revisions required by zoning conditions and the Unified Development Ordinance, subject to the review and approval of the Department of Planning and Development.
3. The minimum heated floor area shall be 2,800 square feet.
4. The development shall be limited to a shared entrance along Whitley Road, subject to the review and approval of the Gwinnett County Department of Transportation.

Exhibits:

- A. Site Visit Photos
- B. Site Plan
- C. Building Elevations
- D. Letter of Intent and Applicant's Response to Standards
- E. Application and Disclosure of Campaign Contributions
- F. DWR Sewer Capacity Certification Letter
- G. Internal and External Agency Review Comments
- H. Maps

Exhibit A: Site Visit Photos



Existing site conditions



Whitley Road facing north

Exhibit B: Site Plan

[attached]

RECEIVED

1.9.2026

FLOOD NOTE:

- FLOOD HAZARD AREA DOES EXIST ON SITE AS PER F.I.R.M. COMMUNITY PANEL No. 13135C0078G, DATED 4/11/2024

BUILDING SETBACKS:

- FRONT: 25' FROM R/W OF ROADS
- SIDE: 10' FROM PROPERTY LINES
- REAR: 30' FROM PROPERTY LINES

NARRATIVE:

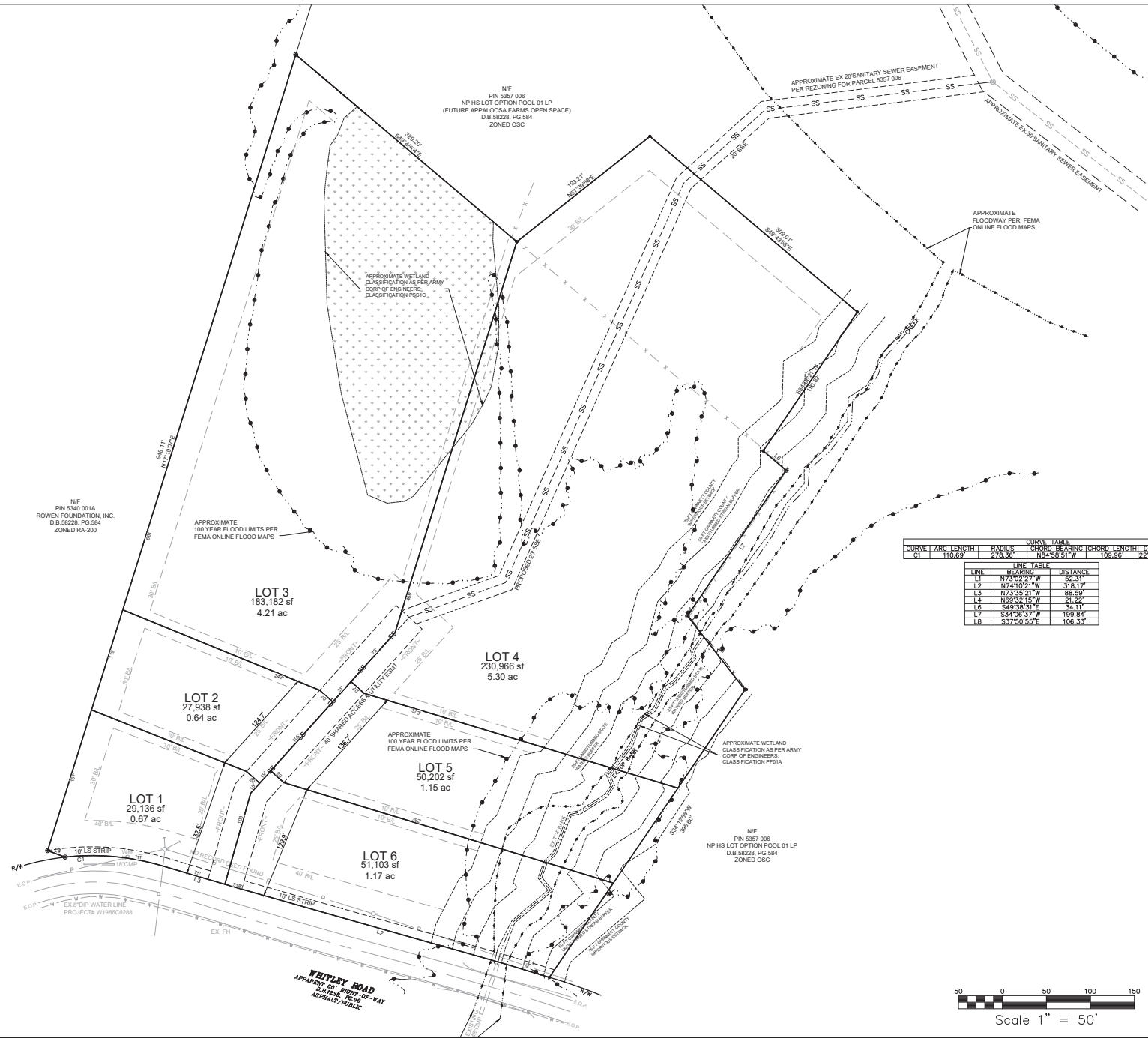
- THE OWNER IS SEEKING TO DEVELOP SIX SINGLE FAMILY DETACHED HOMES ON ITS OWN COMPOUND THROUGH A SHARED PRIVATE DRIVE. THE INTENDED USE IS FOR ALL SIX HOMES TO BE OWNED BY FAMILY MEMBERS TO BE ABLE TO LIVE IN CLOSE VICINITY. SEWER CONNECTION WILL REQUIRE AN EXTENSION FROM THE SEWER MAIN AT THE REAR OF THE PROPERTY THROUGH AN EASEMENT. ALL PLANS WILL BE SUBMITTED AND PROPER PERMITS PULLED.

GENERAL NOTES:

- CURRENT ZONING: RA200
- TO BE REZONED: R-100
- RURAL FUTURE DEVELOPMENT AREA
- 13.143 ACRE LOT SIZE
- MINIMUM LOT SIZE: 15,000SF
- MINIMUM LOT WIDTH: 100'
- MAX 45% LOT COVERAGE
- PROPOSED HOMES TO BE LARGE SINGLE FAMILY DETACHED UNITS
- RDPS TO BE PROVIDED WITH EACH LOT
- SEWER EXTENSION PLANS TO BE SUBMITTED SEPARATELY
- DOMESTIC WATER SUPPLY TO BE GWINNETT COUNTY
- SANITARY SEWER TO BE GWINNETT COUNTY

SURVEY:

- BOUNDARY REFERENCED FROM EXEMPTION PLAT FOR 'APPALOOSA PARK' BY TRAVIS PRUITT & ASSOCIATES DATED 10/30/24 (PB 165 PG 36)

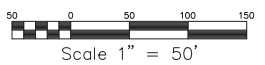


CURVE TABLE

CURVE	ARC LENGTH	RADIUS	CHORD BEARING	CHORD LENGTH	PI
C1	110.66'	278.36'	164°58'01" W	100.06'	122.4

LINE TABLE

LINE	BEARING	DISTANCE
L1	N72°59'27" W	52.31'
L2	N74°10'21" W	318.17'
L3	N73°56'17" W	58.59'
L4	N69°49'12" W	51.23'
L5	S49°38'31" W	34.11'
L6	S43°00'17" W	139.84'
L8	S37°50'55" E	106.33'



REVISION:	
DATE:	

ALL UTILITIES, CONNECTIONS AND DIMENSIONS ON THIS PROJECT SHALL BE AS SHOWN ON THE PLANS UNLESS OTHERWISE SPECIFIED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL UTILITY DEPTHS, LOCATIONS AND DIMENSIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL AFFECTED AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL AFFECTED AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL AFFECTED AGENCIES.

2606 EAST PARKWAY STREET
WINDYBROOK, GA 30086
PHONE: (678) 687-7421
FAX: (678) 687-7422

Sullins Engineering, LLC

CONTACT: MATTHEW SULLINS
CIVIL ENGINEERS - LAND PLANNERS

REZONE PLAT FOR:
1790 WHITLEY ROAD
LOCATED IN:
PARCEL 5358/001
GWINNETT COUNTY, GEORGIA



DATE	JOB #
1/8/2026	3521

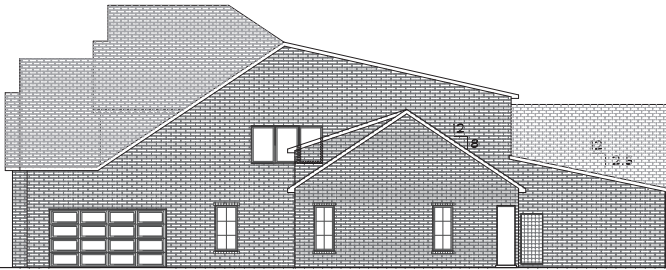
SHEET 1

Exhibit C: Building Elevations

[attached]

RECEIVED

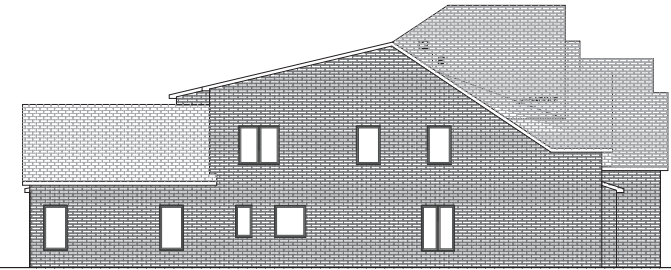
1.6.2026



RIGHT ELEVATION
SCALE: 1/8" = 1'-0"



REAR ELEVATION
SCALE: 1/8" = 1'-0"



LEFT ELEVATION
SCALE: 1/8" = 1'-0"

PROVIDE PROPER DRAINAGE
AWAY FROM BUILDING.

- LUMBER FRAMING WITH FOUR SIDED BRICK FOR EXTERIOR FINISH
- 30' BUILDING HEIGHT FROM FRONT ELEVATION
- 40' BUILDING HEIGHT AT BASEMENT ELEVATION AT REAR

HOUSE NUMBERS:

THE APPROVED ADDRESS SHALL BE DISPLAYED, PLAINLY VISIBLE & LEGIBLE FROM THE STREET FRONTING THE PROPERTY. 4" HIGH NUMBERS WILL BE USED, SUBJECT TO CITY REQUIREMENTS.

ATTIC VENTILATION

PROVIDE ATTIC VENTILATION, NET FREE AREA TO BE EQUAL TO 1/30TH OF ATTIC AREA OR 1/30TH OF ATTIC AREA PROVIDED AT LEAST FOR 10 IN HIGHER PORTION OF ROOF WITH BALANCE IN SOFT.

MUST POST ADDRESS ON CONSTRUCTION SITE VISIBLE FROM ROAD.

EVERY PERSON PERFORMING WORK FOR WHICH THE CODE IS APPLICABLE SHALL COMPLY WITH THE CODE.

NO OCCUPANCY OR USE ALLOWED UNTIL FINAL INSPECTION AND APPROVAL.

STORM WATER RUN-OFF SHALL BE CHANNLED INTO SWALE, PONDING AREA OR OTHER SUITABLE FACILITY. NO RUN-OFF SHALL BE DIRECTED ONTO ADJACENT PROPERTIES.

PROPERTY LINES MUST BE STAKED IN ORDER TO VERIFY SETBACKS TO LOT LINES. IF NOT STAKED SURVEY MUST BE PROVIDED. CLEAN STREET DAILY OF ANY MATERIAL TRACKED ONTO IT FROM THIS SITE.

BUYER NOTICE:

DUE TO THE VARIOUS SITE CONDITIONS OF INDIVIDUAL LOTS, THE GRADE LINES REFLECTED BY THIS PRINT ARE FOR ILLUSTRATIVE PURPOSES ONLY. AND DO NOT REFLECT ACTUAL FINISH GRADE OR ELEVATIONS.

ICE DAM PROTECTION:
1 LAYER OF 40# COATED ROOFING OR COATED GLASS G BARE SHEET SHALL BE APPLIED FROM THE EAVES TO A LINE 24" INSIDE THE EXTERIOR WALL LINE WITH ALL LAPS CEMENTED TOGETHER.

FLASHING NOTE:

ALL OPENINGS TO EXTERIOR MUST BE CAULKED & FLASHED. FLASHING IS REQUIRED WHERE ALL ROOF AND VERTICAL SURFACES MEET, OR WHERE SIDING MATERIAL CHANGES.



FRONT ELEVATION
SCALE: 1/4" = 1'-0"

DANIEL
BERENDE

ELEVATIONS
SCALE AS SHOWN

PLAN DATE: 10-23-23

SHEET # 1 OF 7

Exhibit D: Letter of Intent and Applicant's Response to Standards

[attached]



26 East Athens Street, Winder, GA 30680 * mattsullins@gmail.com * 678-687-6219

January 9, 2026

RE: Application for Rezone for 1790 Whitley Road

Letter of Intent

On behalf of the property owner, I am submitting this Letter of Intent to formally request approval for the rezoning of the property located at 1790 Whitley Road, identified as Tax Map and Parcel 5358 001. The property contains approximately 13.143 acres, is currently zoned RA200, and is within the Rural future development area as designated by Gwinnett County.

The request includes:

- Rezoning approximately 13.143 acres from RA200 to R100 to allow development of six single family detached homes.

This application seeks to rezone the property to allow development of a small family compound consisting of six large single family detached residences served by a shared private drive. The intent is for all six homes to be owned by family members to live in close vicinity. Representative architectural renderings and photographs are included with this application and illustrate the high quality design and modern farmhouse character proposed for the homes, including durable exterior finishes, articulated rooflines, and enhanced rear outdoor living areas.

The proposed lots are intended to maintain a low density residential pattern. Each lot is proposed to have a minimum lot size of 20,000 square feet. The homes are planned to be two story residences with attached garages, and the garages are proposed to be side entry. While a minimum heated floor area is not specifically proffered, the homes will exceed the minimum heated floor area required by the applicable zoning district and will be no less than 2,800 square feet. Exterior building materials will consist of four sided brick, consistent with the submitted elevations, and no alternate exterior materials are proposed.

Access is proposed from Whitley Road via a shared private drive and shared access and utility easements as shown on the rezoning plan. Domestic water and sanitary sewer service are proposed to be provided by Gwinnett County. Sewer connection will require an extension from the sewer main at the rear of the property through an easement, and all plans will be submitted and permits obtained as required.

Flood hazard areas are present on site per the current FEMA mapping noted on the rezoning plan, and the development will be designed to observe all applicable stream buffers and impervious setback requirements identified on the plan. No stormwater management facilities are proposed on site as the development is an exemption plat in nature, and stormwater requirements will be addressed in accordance with applicable county requirements during permitting, as needed. Individual building permits may be required to install water quality BMPs depending on the amount of total impervious area proposed at that time.

A mail kiosk will likely be proposed to serve the development. The final design, location, and any required parking or pull off area will be determined based on United States Postal Service requirements and coordinated accordingly.

Legal Rights Reservation

This letter is submitted without prejudice to any legal or equitable rights that the property owner may have. By submitting this application, the property owner does not waive any rights to seek further

remedies, administrative appeals, or legal relief under applicable law should the rezoning be denied. All rights are expressly reserved.

Conclusion

The proposed rezoning and development are consistent with the intent of the County ordinances and the Rural future development area designation. We appreciate your consideration and respectfully request approval of the rezoning application. Should you require additional information or have any questions, please do not hesitate to contact me.

Sincerely,

Matthew Sullins



GA Registration No. 28445

Pursuant to requirements of the United Development Ordinance, the Board of Commissioners finds that the following standards are relevant in balancing the interest in promoting the public health, safety, morality, or general welfare against the right to the unrestricted use of property and shall govern the exercise of the zoning power.

- A. Whether a proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property:

The proposed rezoning from RA200 to R100 for six single family residences is suitable in view of adjacent and nearby property because it maintains a residential use consistent with the surrounding rural residential character, keeps overall density low across the site, limits access impacts through a shared private drive, and includes high quality architectural design and exterior finishes that will complement nearby development. The layout also preserves substantial open space and will be designed to respect applicable buffers and floodplain constraints, so it will not introduce an incompatible use or intensity.

- B. Whether a proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property:

The proposed rezoning from RA200 to R100 for six single family detached residences is not expected to adversely affect the existing use or usability of adjacent or nearby properties because it remains a low intensity residential use, distributes homes across a large tract with substantial open space, and consolidates access through a shared private drive to reduce roadway conflicts. The development will be designed to meet all applicable setbacks and buffer requirements, will manage stormwater in accordance with County standards, and will incorporate high quality architectural design and landscaping, which together should minimize impacts related to drainage, traffic, noise, lighting, and privacy.

- C. Whether the property to be affected by a proposed rezoning has reasonable economic use as currently zoned:

The property does not have a reasonable economic use as currently zoned RA200 because the required 200 foot minimum lot width, when combined with existing stream buffers, wetlands, and FEMA flood limits on the parcel, would further reduce an already low yield and severely constrain feasible buildable envelopes and access layout. As a result, development consistent with RA200 would be unreasonably limited and economically impractical for this site.

- D. Whether the proposed rezoning will result in a use which could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools:

The proposed rezoning from RA200 to R100 for six single family detached residences is not expected to result in an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools because the request maintains a low density residential use on a 13.143 acre tract and will generate only a small number of daily trips typical of single family homes. Access is consolidated through a shared private drive to minimize roadway impacts, and domestic water and sanitary sewer service will be provided by Gwinnett County with required extensions and permits completed in accordance with County standards. Overall, the limited scale of the development is anticipated to be within the capacity of existing public facilities.

- E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map:

The proposed rezoning from RA200 to R100 is in conformity with the policy and intent of the Unified Plan and the Future Development Map because the property is designated Rural and the request maintains a low intensity residential character consistent with that designation. The plan proposes only six single family detached homes on a 13.143 acre tract, preserves substantial open space due to floodplain and buffer constraints, and uses consolidated access and utilities in a manner that supports orderly development patterns envisioned by the County.

- F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning:

Existing site constraints provide supporting grounds for approval of the proposed request. The property contains stream buffers, wetlands, and FEMA flood hazard limits that restrict buildable area and limit practical lot layout under RA200 standards, including the 200 foot minimum lot width requirement. Rezoning to R100 provides flexibility to design six low density single family homes around these environmental features while preserving open space and maintaining compatibility with the surrounding rural residential context. No variances are requested, and the proposal is intended to support orderly development given the physical constraints of the site.

Exhibit E: Application and Disclosure of Campaign Contributions

[attached]

REZONING APPLICATION

AN APPLICATION TO AMEND THE OFFICIAL ZONING MAP OF GWINNETT COUNTY, GA.

APPLICANT INFORMATION	PROPERTY OWNER INFORMATION
Name: <u>DP2 HOLDINGS, LLC</u>	Name: <u>DP2 HOLDINGS, LLC</u>
Address: <u>1790 Whitley Road</u>	Address: <u>1790 Whitley Road</u>
City: <u>Dacula</u>	City: <u>Dacula</u>
State: <u>GA</u> ZIP: <u>30019</u>	State: <u>GA</u> ZIP: <u>30019</u>
Phone: <u>404.593.3445</u>	Phone: <u>404.593.3445</u>
Email: <u>dp2holdings2023@gmail.com</u>	Email: <u>dp2holdings2023@gmail.com</u>
Contact Person: <u>Jakob Sullins</u> Phone: <u>706.773.3849</u>	
Contact's Email: <u>jakesullins@gmail.com</u>	
APPLICANT IS THE:	
<input type="checkbox"/> Owner's Agent <input checked="" type="checkbox"/> Property Owner <input type="checkbox"/> Contract Purchaser	
Current Zoning District(s): <u>RA-200</u> Requested Zoning District: <u>R-100</u>	
Parcel Number(s): <u>5358 001</u> Acreage: <u>13.14</u>	
Property Address(es): <u>1790 Whitley Road Dacula GA 30019</u>	
Proposed Development: <u>Single-Family Detached Development</u>	
Variance(s): _____ Waiver(s): _____	
<p style="text-align: center;">RESIDENTIAL DEVELOPMENT</p> No. of Dwelling Units: <u>6</u> Dwelling Unit Sq. Ft.: <u>2800</u> Density: <u>.46</u> Floor Area Ratio (LRR, MRR, HRR): _____	<p style="text-align: center;">NON-RESIDENTIAL DEVELOPMENT</p> No. of Buildings: _____ Total Building Sq. Ft.: _____ Floor Area Ratio: _____
MIXED-USE DEVELOPMENT	
No. of Dwelling Units: _____ Dwelling Unit Sq. Ft.: _____	
Total Non-Residential Sq. Ft.: _____ Floor Area Ratio: _____	



GWINNETT COUNTY
PLANNING AND DEVELOPMENT
PROPERTY TAX VERIFICATION

The undersigned certifies that all Gwinnett County property taxes are current. In no such case shall an application or reapplication be processed without such property tax verification.

Applicant Information

Name: DANIEL PERRET - DP2 HOLDINGS

Signature: 

Date: 11/17/2025


Property Information

Enter all parcel IDs in the table below. [Search for Parcel ID](#)

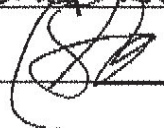
Parcel ID(s) (by Applicant)	Property Taxes Paid (Yes or No) (Tax Commissioner Use Only)
R-5358-001	Yes

Tax Commissioner Affirmation

All Gwinnett County property taxes are current for the above referenced parcel(s) and confirmed by the signature below.

Name: 


Title: TSA

Signature: 


Date: 11.17.25

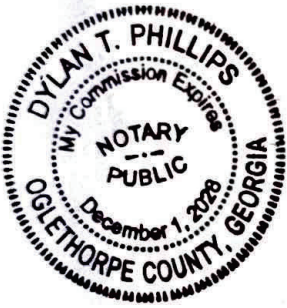
REZONING PROPERTY OWNER'S CERTIFICATION

The undersigned below, or as attached, is the owner of the property considered in this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon within 12 months from the date of last action by the Board of Commissioners unless waived by the Board of Commissioners. In no case shall an application or reapplication be acted upon in less than six months from the date of last action by the Board of Commissioners.

DP2 HOLDINGS LLC /  1/6/2026
Signature of applicant Date

DP2 HOLDINGS LLC / DANIEL PEDET, PRINCIPAL 1/6/2026
Type or print name and title Date

 1/6/2026
Signature of notary public Date Notary seal





Disclosure of campaign contributions aggregating to \$250.00 or more to a member of the Board of Commissioners is required pursuant to Section 270-25.E. of the Unified Development Ordinance.

Name: Daniel Parasca

Relationship to Application (Applicant or Representative of Applicant): Applicant

Campaign Contributions? NO

Contributions:

Official Name	Contribution Amount	Date of Contribution	Contributor



Disclosure of campaign contributions aggregating to \$250.00 or more to a member of the Board of Commissioners is required pursuant to Section 270-25.E. of the Unified Development Ordinance.

Name: Sullins Engineering

Relationship to Application (Applicant or Representative of Applicant): Representative of applicant

Campaign Contributions? NO

Contributions:

Official Name	Contribution Amount	Date of Contribution	Contributor

Exhibit F: DWR Sewer Capacity Certification Letter

[attached]



November 10, 2025

Daniel Peret
DP2 Holdings, LLC
1790 Whitley Road
Dacula, GA 30019

<input checked="" type="checkbox"/>	APPROVED
<input type="checkbox"/>	DENIED
<input type="checkbox"/>	CONDITIONALLY APPROVED
Sewer Capacity Request #C2025-295-11	
Expiration Date: 11/10/2026	
Tie-In Manhole FID: 4080999	

RE: Sewer Availability for Proposed Development – 1790 Whitley Road
Parcel ID 5358 001

Dear Mr. Peret:

This letter supersedes the earlier Sewer Capacity Certification C2024-228-09 dated September 11, 2024 addressed to Neville Allison of The Revive Land Group.

Gwinnett County Department of Water Resources (GCDWR) has reviewed the impact of the proposed development consisting of six single-family residences on the above parcel and determined:

Gwinnett County has adequate sewer capacity to serve the proposed development.

This confirmation is based on your anticipated annual average daily flow of **1.04 gpm** discharging to the sewer tie-in manhole at Facility ID **4080999**.

Capacity Allocation: Sewer capacities are allocated only upon the issuance of a Development Permit, by Gwinnett County Planning & Development and after all applicable conditions established in this Sewer Capacity Certification, if any, are met.

Certification Expiration: The Sewer Capacity Certification is valid for 12 months from the date of this letter. If rezoning is approved, GCDWR will issue a revised Sewer Capacity Certification based on the approved units, as needed. The capacity request can be renewed for an additional 12 months. If a Development Permit has not been obtained by the expiration date above the developer may apply for renewal by providing evidence of active rezoning, plan review, or permitting efforts.

Once the Sewer Capacity Certification expires, the capacity is no longer allocated, and a new Sewer Capacity Certification request must be submitted for re-evaluation with no guarantee of available capacity. The evaluation will be based on the updated conditions of the sewer system.

Certification Invalidation: This certification is not valid if there are proposed changes to your development that could impact downstream sewer capacity (e.g. land use density, sewer tie-in



manhole, parcel changes, and/or rezoning). In this case the development must resubmit for a new Sewer Capacity Request with updated information prior to the expiration of the previous one.

Please contact us at 678-376-7026 if you have any questions.

Sincerely,

GWINNETT COUNTY DEPARTMENT OF WATER RESOURCES

Della Taylor, PE
Division Director, Infrastructure Support
678.376.2130

C: Gwinnett County DWR – Lorraine Campagne, Section Manager; Raghu Vemuru, Engineer V; Mikala Weston, Engineer III; Geniva Sylvain, Engineer III
Gwinnett County P&D – Charli Young, Planning Manager; Christina Dejarnette, Engineer II; Wendell Tumale, Engineer III; Dominique Graves, Engineer II

Exhibit G: Internal and External Agency Review Comments

[attached]



**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

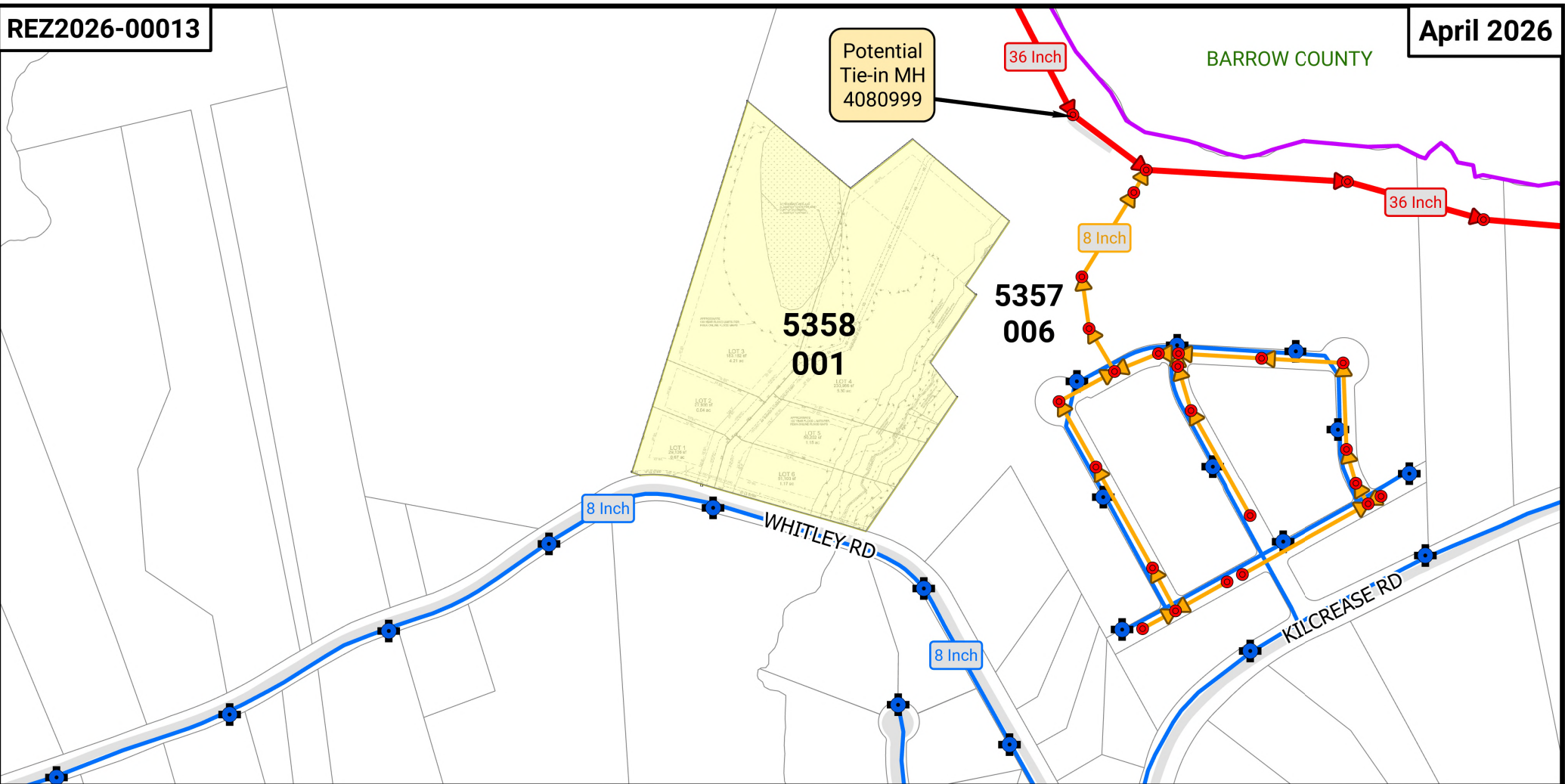
TRC Meeting Date:		02/20/2026	
Department/Agency Name:		Transportation	
Reviewer Name:		Brent Hodges	
Reviewer Title:		Construction Manager 2	
Reviewer Email Address:		Brent.Hodges@gwinnettcountry.com	
Case Number:		REZ2026-00013	
Case Address:		1790 Whitley Road, Dacula, 30019	
Comments:		<input checked="" type="checkbox"/>	<input type="checkbox"/>
		YES	NO
1	Whitley Road is a local road. The Average Daily Traffic (ADT) volume for this road is not on file.		
2	The nearest Ride Gwinnett facility (Stop 4528 / Route 45) is located 10.5 miles away at Collins Hill Road and Collins Industrial Way.		
3	The developer shall provide a sight distance certification for the proposed shared site driveway, per section 360-40.4 and table 360.2 of the UDO.		
4			
5			
6			
7			
Recommended Zoning Conditions:		<input type="checkbox"/>	<input checked="" type="checkbox"/>
		YES	NO
1			
2			
3			
4			
5			
6			
7			



**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		
Department/Agency Name:	DWR	
Reviewer Name:	Lorraine Campagne	
Reviewer Title:	Section Manager	
Reviewer Email Address:	Lorraine.Campagne@gwinnettcounty.com	
Case Number:	REZ2026-00013	
Case Address:	1790 Whitley Rd	
Comments:		<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO
1	Water: The proposed development may connect to an existing 8-inch water main on the southern right-of-way of Whitley Road. A jack-and-bore may be required.	
2	Sewer: A Sewer Capacity Certification (C2025-295-11) was approved in November 2025 for six single-family residences. The proposed development plans to connect to an existing 36-inch gravity sewer located along the Apalachee River on parcel 5357 006. The final plat for the adjacent Appaloosa Park development indicates an easement exists to tie-in to manhole FID 4080999.	
3		
4		
5		
6		
7		
Recommended Zoning Conditions:		<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO
1		
2		
3		
4		
5		
6		

BARROW COUNTY

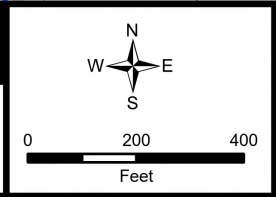


LEGEND

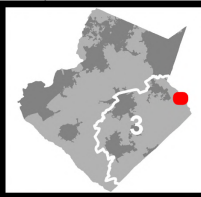
- Flow Management
- Pump Station
- Regional
- Manhole
- Hydrant
- City
- Water Main
- Proposed Water Main
- Sewer Collector
- Sewer Interceptor
- Sewer Force Main
- Proposed Sewer Collector

1790 Whitley Rd
RA-200 to R-100

Water & Sewer Utility Map



LOCATION



Water Comments: The proposed development may connect to an existing 8-inch water main on the southern right-of-way of Whitley Road. A jack-and-bore may be required.

Sewer Comments: A Sewer Capacity Certification (C2025-295-11) was approved in November 2025 for six single-family residences. The proposed development plans to connect to an existing 36-inch gravity sewer located along the Apalachee River on parcel 5357 006. The final plat for the adjacent Appaloosa Park development indicates an easement exists to tie-in to manhole FID 4080999.

Water Availability: Water demands imposed by the proposed development may require upsizing or extensions of existing water mains in order to meet Gwinnett County Standards and fire flow demands. Any cost associated with such required improvements will be the responsibility of the development. Current Gwinnett County Standards require a minimum of 12" pipe size for commercial developments and a minimum of 8" pipe size for residential developments. Additionally, connection to a minimum of 12" and 8" mains are required for commercial and residential developments, respectively. It is the responsibility of the developer's engineer to confirm pressure and volumes are available for the development.

Sewer Availability: A Sewer Capacity Certification must be obtained from Gwinnett County to confirm the existing system can serve the development. Sewer demands imposed by the proposed development may require upsizing and/or extensions of existing sewer mains, and/or upsizing of an existing pump station, and/or installation of a new pump station. Any cost associated with such required improvements will be the responsibility of the development. The developer shall provide easements for future sewer connection to all locations designated by Gwinnett County during plan review.

Water and Sewer Design and Construction Requirements: Extensions of the water and/or sanitary sewer systems within the subject development must conform to this department's policies, Gwinnett County's ordinances, and the Water Main and Sanitary Sewer Design and Construction Standards and Specifications, dated April 5th, 2016. Subsequent to design, construction, inspection, and final acceptance of the required utilities, service would then become available under the applicable utility permit rate schedules.

Private Road Developments: Any development with private roads must comply with the Standard Policy Requirement for the Installation of Water and Sanitary Sewer Mains within Private Developments. This policy stipulates minimum easement requirements and location of public mains and appurtenances, among other requirements.

Residential Rezoning Impact on Local Schools
Prepared for Gwinnett County BOC, April, 2026

											Proposed Zoning
	School	2025-26			2026-27			2027-28			Approximate Student Projections from Proposed Developments
		Forecast	Capacity	+/- Cap.	Forecast	Capacity	+/- Cap.	Forecast	Capacity	+/- Cap.	
	Dacula High School	2429	2,550	-121	2,477	2,550	-73	2,526	2,550	-24	5
REZ2026-00013 (ZON2025-00001)	Dacula Middle School	1921	1,900	21	1,959	1,900	59	1,948	1,900	48	4
	Alcova Elementary School	1496	1,150	346	1,525	1,150	375	1,556	1,150	406	7
	Grayson High School	3738	3,000	738	3,850	3,000	850	3,946	3,000	946	7
REZ2026-00014 (ZON2025-00091)	Bay Creek Middle School	1646	1,150	496	1,704	1,150	554	1,755	1,150	605	5
	Trip Elementary School	1,708	1,200	508	1,776	1,200	576	1,838	1,200	638	9
	Seckinger High School	2440	2,800	-360	2,485	2,800	-315	2,525	2,800	-275	18
REZ2026-00016 (ZON2025-00095)	Jones Middle School	1852	1,575	277	1,888	1,575	313	1,907	1,575	332	12
	Patrick Elementary School	1030	1,025	5	1,045	1,025	20	1,059	1,025	34	23
	Collins Hill High School	2420	2,625	-205	2,395	2,625	-230	2,372	2,625	-253	24
REZ2026-00017 (ZON2025-00113)	Creekland Middle School	1702	2,100	-398	1,720	2,100	-380	1,748	2,100	-352	16
	Walnut Grove Elementary School	821	1,200	-379	830	1,200	-370	818	1,200	-382	30

Exhibit H: Maps

[attached]

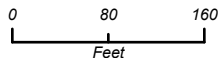


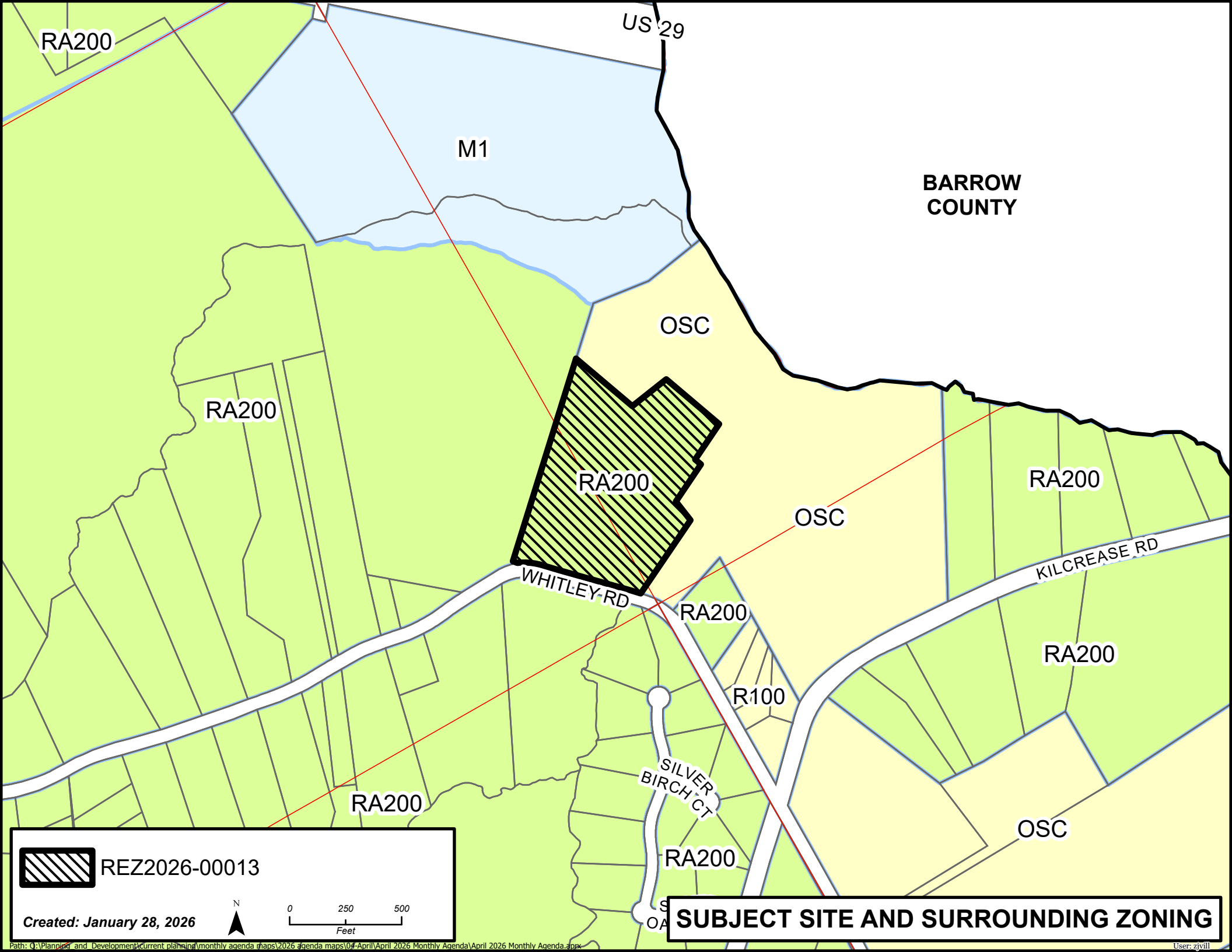
WHITLEY RD



REZ2026-00013

Created: January 28, 2026





RA200

US 29

M1

BARROW COUNTY

OSC

RA200

RA200

RA200

OSC

WHITLEY RD

RA200

KILCREASE RD

RA200


RA200

R100


SILVER BIRCH CT

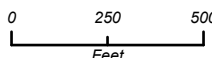
OSC

RA200

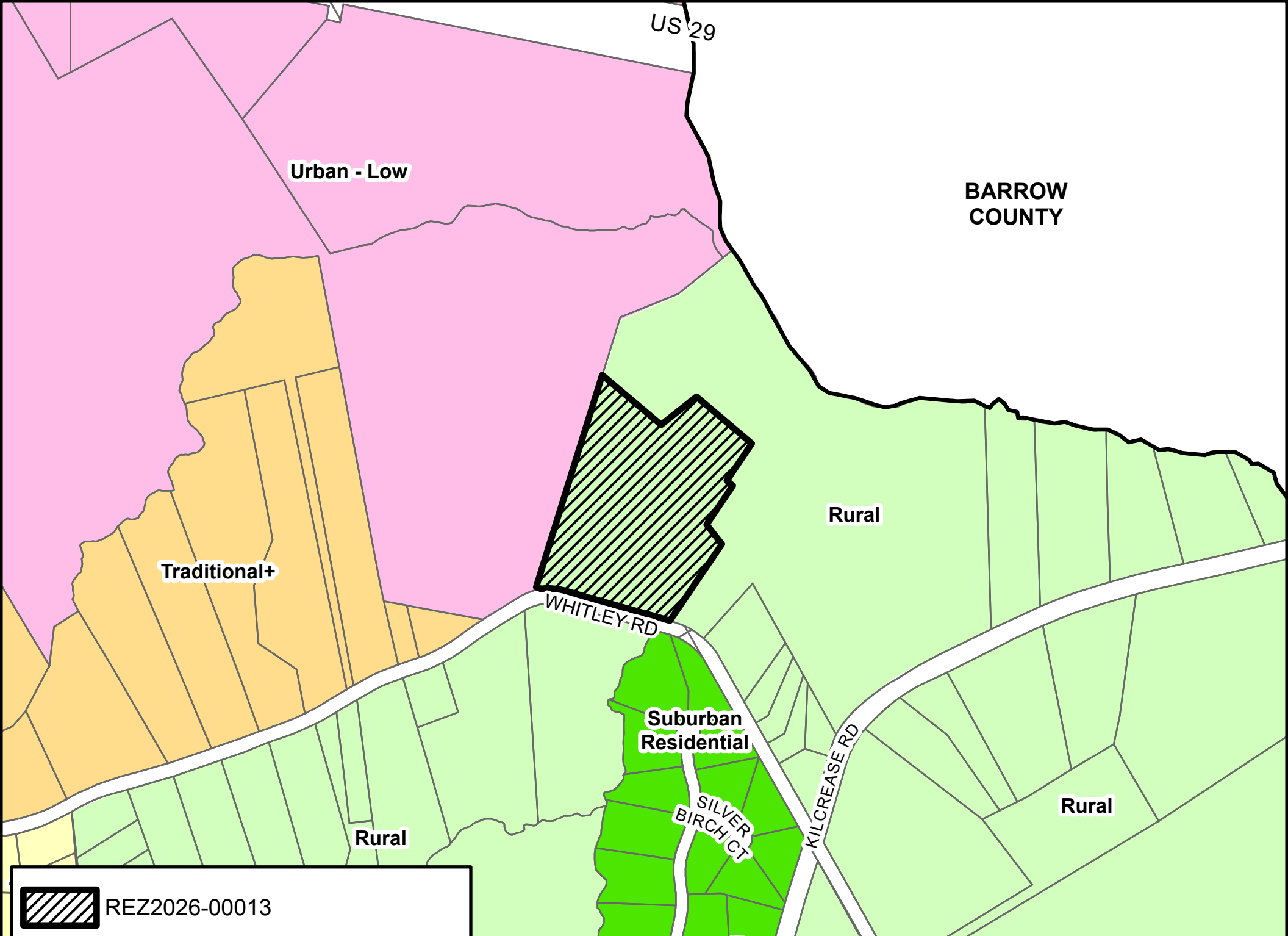
 REZ2026-00013


Created: January 28, 2026

 N

 0 250 500 Feet

SUBJECT SITE AND SURROUNDING ZONING



 REZ2026-00013

Created: January 28, 2026

N

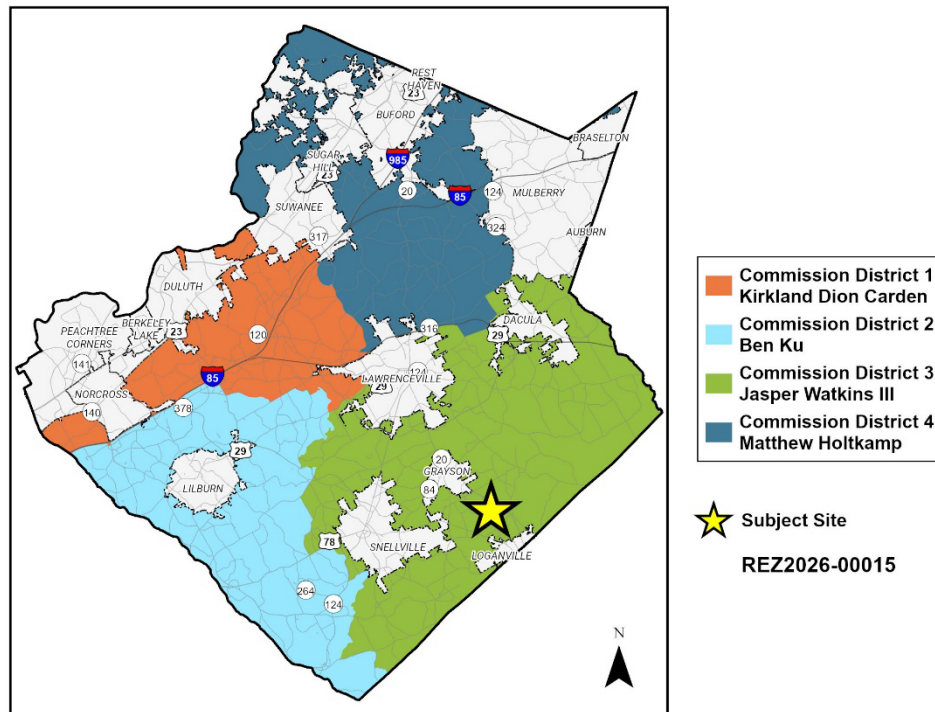
0 250 500
Feet

2045 UNIFIED PLAN FUTURE DEVELOPMENT CLASSIFICATION

PLANNING AND DEVELOPMENT DEPARTMENT CASE REPORT

Case Number: REZ2026-00015
Current Zoning: C-2 (General Business District)
Request: Rezoning to **R-IF** (Residential In-Fill District)
Overlay District: Grayson/Highway 20 Corridor Overlay District
Additional Requests: Variances
Address: 3175 Loganville Highway
Map Number: R5157 341
Site Area: 16.78
Units: 139
Proposed Development: Mixed-Residential Development (Age-Restricted)
Commission District: District 3 – Commissioner Watkins
Future Development Type: Neighborhood Traditional

Staff Recommendation: **APPROVAL WITH CONDITIONS**



Applicant: One Street Residential, LLC
c/o Mahaffey Pickens Tucker, LLP
1550 North Brown Road, Suite 125
Lawrenceville, GA 30043

Owner: Grayson Development, LLC
12645 Birmingham Highway
Alpharetta, GA 30004

Contact: Shane Lanham

Phone: 770.232.0000

Zoning History

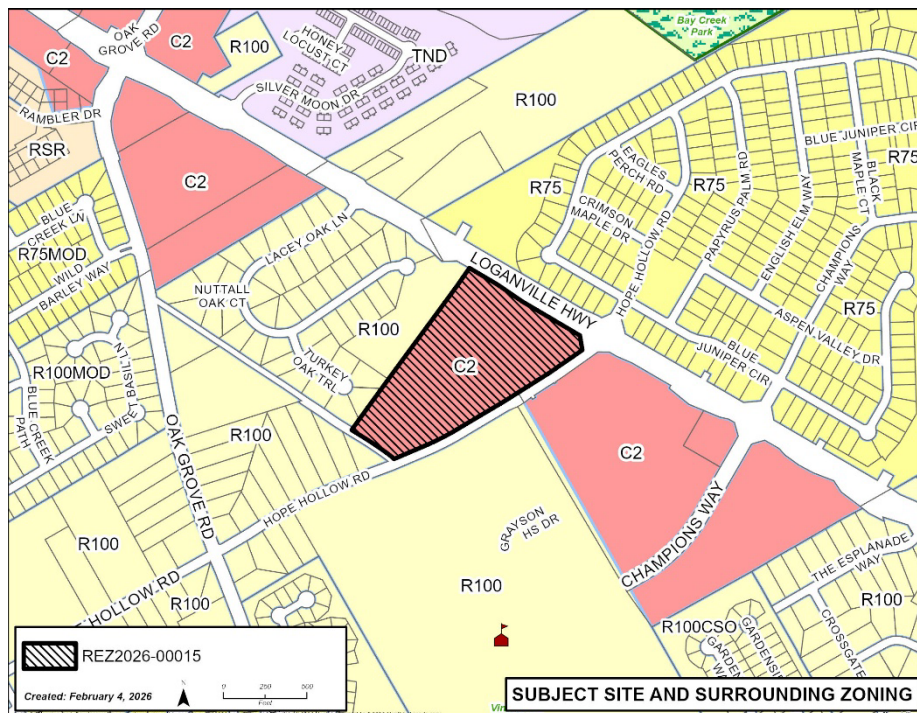
The subject property is zoned C-2 (General Business District). In 2015, the property was rezoned from R-100 (Single-Family Residence District) to C-2 for commercial uses, pursuant to RZC2015-00009. The property is located in the Grayson/Highway 20 Corridor Overlay District.

Existing Site Condition

The site is a 16.78-acre parcel located at the intersection of Loganville Highway and Hope Hollow Road. The property is undeveloped with dense vegetation. There is an existing 5-foot-wide sidewalk along the Loganville Highway frontage. The nearest Ride Gwinnett transit stop is approximately 6.9 miles from the subject property.

Surrounding Use and Zoning

The site is surrounded by single-family residences within subdivisions, undeveloped commercial property, and Grayson High School. The single-family detached portion of the Independence subdivision is located to the north, across Loganville Highway. Undeveloped commercial property and Grayson High School are located to the east and south, across Hope Hollow Road. The Oakgrove and Oakwood Corners subdivisions are located to the west. The townhouses, villas and duplexes portions of the Independence subdivision, and senior-oriented villas in the Seaboard Junction subdivision, are located farther west. The following is a summary of surrounding uses and zoning:



Location	Land Use	Zoning	Density
Proposed	Mixed-Residential Development	R-IF	8.28 units per acre
North	Single-Family Residential (Independence)	R-75	2.00 units per acre
East	Commercial (undeveloped)	C-2	N/A
South	Civic (Grayson High School)	R-100	N/A
West	Single-Family Residential (Oakgrove)	R-100	1.50 units per acre
	Single-Family Residential (Oakwood Corners)	R-100	1.02 units per acre
	Townhouses, Villas and Duplexes (Independence)	TND	4.51 units per acre
	Senior-Oriented Villas (Seaboard Junction)	R-SR	3.74 units per acre

Project Summary

The applicant requests rezoning from C-2 to R-IF for an age-restricted, mixed-residential development, including:

- 139 units comprised of 24 multiplex units, 106 duplex units, and nine single-family detached residences on a single parcel, yielding a density of 8.28 units per acre.
- Multiplex buildings (two-story):
 - Four buildings, each with six one- or two-bedroom units ranging in size from 949 to 1,419 square feet.
 - Exterior building materials of brick and cementitious siding on the front façade and cementitious siding on the side and rear façades.
 - Rear-loaded one-car garage for each unit.
- Duplex buildings (one- and two-story):
 - 53 buildings with one-, two-, or four-bedroom units ranging in size from 786 to 2,206 square feet.
 - Exterior building materials of cementitious siding with brick accents on the front façade of most product types, and cementitious siding on the side and rear façades.
 - Some units contain one- or two-car garages.
- Single-family detached residences (one-story):
 - Nine, two-bedroom units ranging in size from 1,060 to 1,103 square feet.
 - Exterior building materials of cementitious siding with brick accents (12A) and cementitious siding (13A) on the front façade, and cementitious siding on the side and rear façades.
 - Front-loaded one-car garage for each unit.
- 3.61 acres of open space, including 0.90 acres of usable open space comprised of a clubhouse, pickleball courts, community garden, dog park, and pocket parks.
- A total of 109 on-street parking spaces throughout the site.
- One stormwater management facility in the northwestern corner.
- A double row of evergreen trees along the western property line.
- One right-in/right-out entrance with a deceleration lane and a 12-foot-wide multi-use path along Loganville Highway, and one full-access entrance with a deceleration lane and a 5-foot-wide sidewalk along Hope Hollow Road.

Zoning and Development Standards

The following is a summary of applicable standards from the Unified Development Ordinance (UDO):

Standard	Required	Proposed	Meets Standard?
Lot Requirement	Located on individual lots	Located on common parcel	NO*
Front Yard	Minimum 10'	Minimum 10'	YES
Rear Yard	Minimum 20'	Minimum 20'	YES
Side Yard	Minimum 7.5'	Minimum 7.5'	YES
Building Height	Maximum 45'	Maximum 45'	YES
Open Space	Minimum 10% or 1.68 acres	22% or 3.61 acres	YES
Usable Open Space	Minimum 5% or 0.84 acres	6% or 0.90 acres	YES

* The applicant is requesting a variance from this standard.

This parcel is located within the Grayson/Highway 20 Corridor Overlay District; however, the overlay standards do not apply.

Public Participation

The applicant held a community meeting on February 23, 2026, at the Chestnut Grove Baptist Church in Grayson. There were six community members in attendance, who raised questions regarding the detention pond, noise impacts, and the development process. The applicant responded to these concerns by explaining the design of the detention pond, the minimal anticipated noise from amenities, and providing an overview of the development process.

Staff Analysis

Rezoning Request Analysis: The standards governing the exercise of zoning power are outlined in Section 270-20.6 of the UDO. Staff makes the following findings related to the zoning requests:

A. Whether the proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

The surrounding area includes a mixture of residential housing types. The Independence subdivision, located across Loganville Highway, contains single-family residences, duplexes, villas, and townhouses. The Seaboard Junction subdivision, located farther west along Loganville Highway, contains age-restricted villas. The proposed development would contain housing types similar to existing subdivisions in the area. The proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

B. Whether the proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property.

The subject property is zoned C-2, which allows for commercial and office uses. The proposed mixed-residential development is less intense than the by-right uses, and would be more compatible with abutting residential uses. The double row of evergreen trees along the western property line would provide a natural separation for the Oakgrove subdivision. The proposed rezoning will not adversely affect the existing use or usability of adjacent or nearby property.

C. Whether the property to be affected by the proposed rezoning has a reasonable economic use as currently zoned.

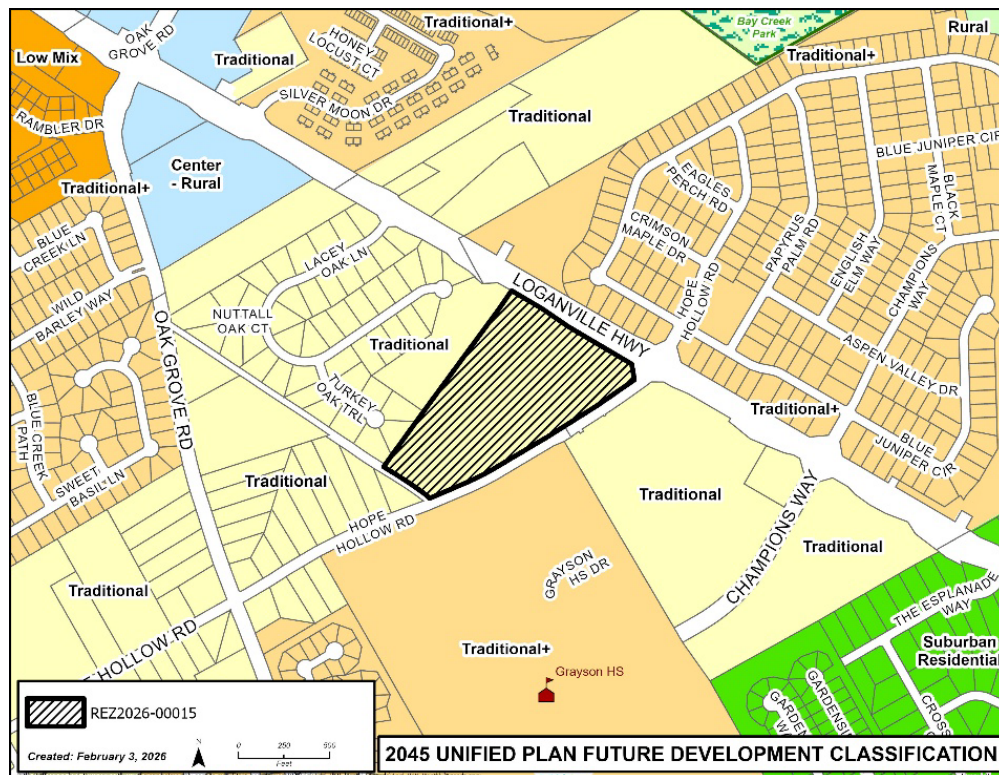
The property has a reasonable economic use as currently zoned.

D. Whether the proposed rezoning will result in a use which will or could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools.

An increase in impacts on public facilities would be anticipated in the form of traffic, utility demand, and stormwater runoff; however, these impacts could be mitigated with appropriate conditions, site development requirements, and planning. The traffic impact study estimates that the development would generate 50 morning and 57 evening peak hour trips. The study recommends a deceleration lane on Loganville Highway and a left-turn lane on Hope Hollow Road. No impact is anticipated on school enrollment.

E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map; and

The Unified Plan's Future Development Map indicates "Neighborhood Traditional" for the subject property. Given the availability of sewer, the property would be considered "Neighborhood Traditional+", which encourages gentle density through smaller footprint homes in predominantly single-family residential areas. Single-family, duplexes, and missing middle housing are recommended land uses and R-IF is a recommended zoning district. The proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map.



F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning.

The 2045 Unified Plan acknowledges that, in general, Gwinnett County has an aging population, and recommends additional housing options for seniors. The proposed development supports this recommendation.

Variance Requests and Analyses:

The applicant is seeking variances from the following provisions of the UDO:

1. **Section 210-140.13.E.** To not locate duplexes on individual lots.

Analysis: The proposed development is intended to be a rental community, owned and managed by a single entity with all units on one parcel. Approval of the variance would not nullify the intent of the UDO.

2. **Section 210-140.17.J.** To not locate multiplexes on individual lots.

Analysis: The proposed development is intended to be a rental community, owned and managed by a single entity with all units on one parcel. Approval of the variance would not nullify the intent of the UDO.

3. **Section 230-110.8 B.1.** To not incorporate at least 50 percent brick, cast stone, and/or stone on the front façade for duplexes and multiplexes.

Analysis: The proposed development features farmhouse-style architecture. Approval of the variance would not nullify the intent of the UDO.

Staff Recommendation

Based on the staff's evaluation of the request and the standards governing the exercise of zoning power, the Department of Planning and Development recommends **APPROVAL WITH CONDITIONS** of the rezoning request and **APPROVAL** of the following variances:

1. To not locate duplexes on individual lots.
2. To not locate multiplexes on individual lots.
3. To not incorporate at least 50 percent brick, cast stone, and/or stone on the front façade for duplexes and multiplexes.

Staff Recommended Conditions

Approval as R-IF (Residential In-Fill District), for a mixed-residential development, subject to the following conditions:

1. The development shall be limited to 139 units, including multiplex dwelling units (maximum 24 units), duplex dwelling units (maximum 106 units), and single-family dwelling units.
2. The proposed development shall be constructed in general conformance with Exhibit B: Site Plan, dated received March 25, 2026, and Exhibit C: Building Elevations, dated received January

26, 2026, with revisions required by zoning conditions and the Unified Development Ordinance, subject to the review and approval of the Department of Planning and Development.

3. Usable open space shall be provided in general conformance with Exhibit B: Site Plan, dated received March 25, 2026, including but not limited to a clubhouse with amenities including pickleball courts, community garden, dog park, and pocket parks, subject to the review and approval of the Department of Planning and Development.
4. A double row of evergreen trees shall be provided along the western property line, in general conformance with Exhibit B: Site Plan, dated received March 25, 2026, subject to the review and approval of the Department of Planning and Development.
5. The following roadway improvements shall be provided in general conformance with Exhibit B: Site Plan, dated received March 25, 2026, prior to the issuance of a Certificate of Occupancy, subject to the review and approval of the Gwinnett County Department of Transportation:
 - a. A right-in/right-out entrance, deceleration lane, and 12-foot-wide multi-use path along Loganville Highway.
 - b. A full-access entrance, deceleration lane, left-turn lane, and 5-foot-wide sidewalk along Hope Hollow Road.

Exhibits:

- A. Site Visit Photos
- B. Site Plan
- C. Building Elevations
- D. Letter of Intent and Applicant's Response to Standards
- E. Application and Disclosure of Campaign Contributions
- F. DWR Sewer Capacity Certification Letter
- G. Traffic Impact Study
- H. Public Participation Plan and Report
- I. Internal and External Agency Review Comments
- J. Maps

Exhibit A: Site Visit Photos



View of the undeveloped site



View of Loganville Highway frontage



View of Hope Hollow Road frontage

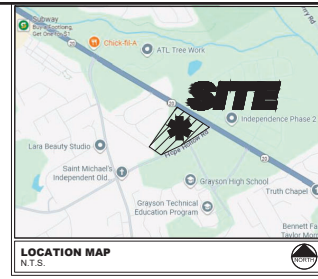


View of villas in Seaboard Junction

Exhibit B: Site Plan

[attached]

SITE & PARKING DATA										
TOTAL PARKING:	317 SPACES - 5 HANDICAP SPACES - 188 SURFACE SPACES - 204 GARAGE/DRIVEWAY SPACES									
BUILDING PLAN	R-F USE	BEDS	BATHS	MIN SF	PARKING SPACES	UNIT COUNT	MIN SPOTS	MAX SPOTS	MIN TOTAL	MAX TOTAL
1	DUPLEX	1	1.5	1129	2	7	2	4	14	28
1	DUPLEX	2	2	1928	4	7	2	4	14	28
12A	SINGLE FAMILY	2	2	1060	2	2	2	4	4	8
12B	DUPLEX	4	4	2124	2	26	2	4	52	104
13A	SINGLE FAMILY	2	2	1103	2	7	2	4	14	28
13B	DUPLEX	4	4	2206	2	22	2	4	44	88
15	DUPLEX	1	1	786	0	22	2	4	44	88
15	DUPLEX	1	1	1089	0	22	2	4	44	88
19	MULTIPLY	2	2	1370	2	4	1.5	2.5	6	10
19	MULTIPLY	2	2	1382	2	4	1.5	2.5	6	10
19	MULTIPLY	1	1	949	2	4	1.5	2.5	6	10
19	MULTIPLY	2	2	1408	2	4	1.5	2.5	6	10
19	MULTIPLY	2	2	1419	2	4	1.5	2.5	6	10
19	MULTIPLY	1	1	949	2	4	1.5	2.5	6	10
TOTAL GARAGE / DRIVEWAY SPACES:					139	SITE DENSITY (8.3 DU/AC)		266	620	



Kimley-Horn
 3180 E. JONES BRIDGE RD. SUITE 300 - PHONE 770.619.4200
 104 INTERSTATE PARKWAY E. SE - PHONE 770.602.9000
 ATLANTA, GA 30339 - WWW.KIMLEY-HORN.COM
 PREPARED FOR: **ONE STREET RESIDENTIAL**
 PROJECT: **HAYLOFT HOPE HOLLOW**
 DRAWN BY: BM
 DESIGNED BY: BT
 REVIEWED BY: AD
 DATE: 11/24/2025
 PROJECT NO: 017371001
 TITLE: **ZONING SITE PLAN**
 SHEET NUMBER: **1 OF 1**
 PRELIMINARY: NOT FOR CONSTRUCTION

Drawing name: K:\ATL_Civil\03_Civil\03_Civil\Hayloft Hope Hollow\CD\Plan\CD-2026-SITE PLAN.dwg CD-2026-SITE PLAN.dwg 11/24/2025 10:08am by: Madeline Young
 This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse or improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



SITE SUMMARY	
EXISTING SITE ZONING:	C-2 (CASE# R2C2015-00009)
OVERLAY ZONING DISTRICT:	GRAYSON HWY 20 CORRIDOR
PROPOSED SITE ZONING:	R-1F
SITE AREA:	+ 16.78 ACRES
DISTURBED AREA:	+ 15.63 ACRES
TAX PARCEL ID:	5157 341
BUILDING SETBACK LINES:	FRONT YARD: 10 FT REAR YARD: 20 FT SIDE YARD: 7.5 FT
IMPERVIOUS SURFACE AREA:	+ 9.11 ACRES
MAXIMUM BUILDING HEIGHT:	45 FT
MAXIMUM FAR:	NA
TOTAL UNITS:	139
SITE DENSITY:	8.3 DU/AC
REQUIRED OPEN SPACE:	10% (1.68 AC)
PROPOSED OPEN SPACE:	22% (3.61 AC)
OPEN SPACE SYMBOL:	
REQUIRED USABLE OPEN SPACE (50% OF REQUIRED OPEN SPACE):	5% (0.84 AC)
PROPOSED USABLE OPEN SPACE:	6.02% (0.90 AC)
USABLE OPEN SPACE SYMBOL:	

**WINNETT COUNTY
 PLANNING AND DEVELOPMENT**
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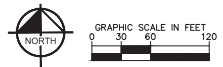
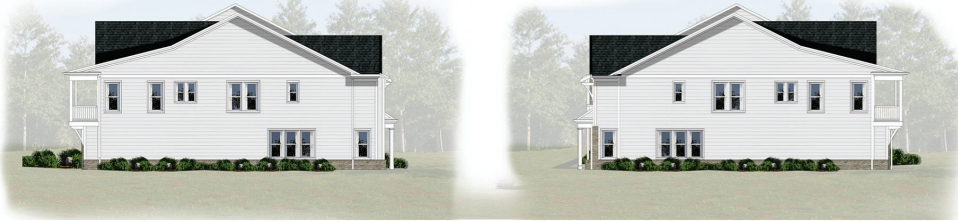


Exhibit C: Building Elevations

[attached]

1. Front porch dimensions to meet minimum 4' depth x 6' width standards
2. Trash service: Roll-out bins

Multiplex Plan 19 (A Elevation)

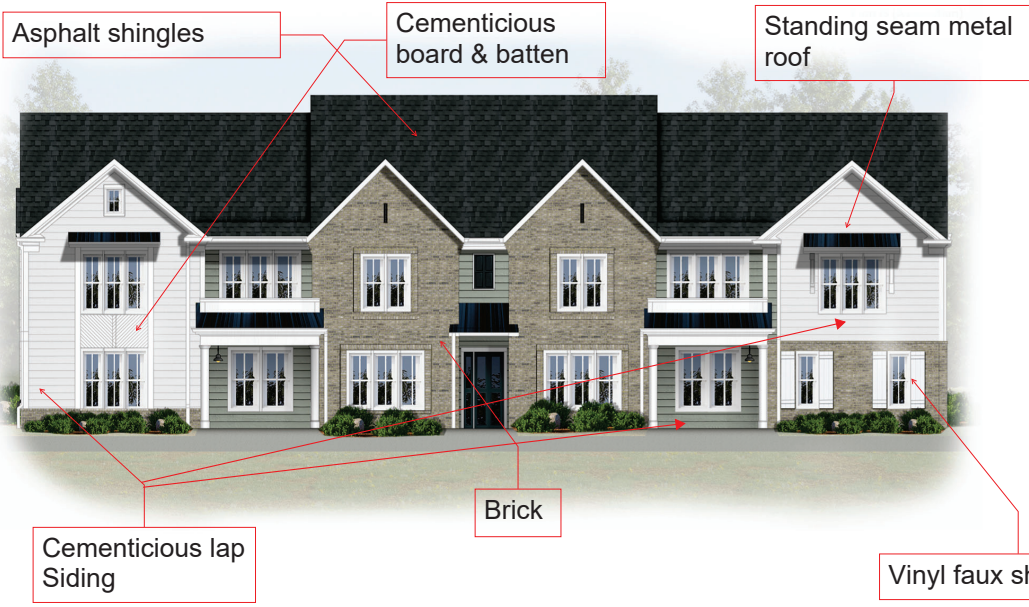


LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION



Asphalt shingles

Cementitious board & batten

Standing seam metal roof

Cementitious lap Siding

Brick

Vinyl faux shutters

FRONT ELEVATION

- Multifamily House**
-  **BODY & SHINGLES**
ACACIA HAZE SW9132
-  **TRIM:**
PURE WHITE SW7005
-  **BRICK:**
CHEROKEE BRICK AUGUSTA
-  **BRICK MORTAR:**
COOSA IVORY BUFF
-  **METAL ROOF:**
CHARCOAL
-  **ROOF:**
CAMBRIDGE CHARCOAL GREY
-  **GUTTERS/DOWNSPOUTS:**
WHITE
-  **FRONT DOOR OPTION 1:**
MOUNT ETNA SW7625
-  **FRONT DOOR OPTION 2:**
ANJOU PEAR

Exterior Scheme A

BODY: SPARE WHITE SW6203
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: ANJOU PEAR SW6381
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: UNDERSEAS SW6214

Exterior Scheme B

BODY: ACACIA HAZE SW9132
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: MOUNT ETNA SW7625
FRONT DOOR OPTION 2: IRON ORE SW7069

Exterior Scheme C

BODY: PRAIRIE GRASS SW7546
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: DRIZZLE SW6479
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: ROSEMARY SW6187

Variance requested for sec 230-110.8, elevations do not comply. This elevation is 31% brick.

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1. Front porch dimensions to meet minimum 4' depth x 6' width standards
2. Trash service: Roll-out bins

Multiplex Plan 19 (B Elevation)



LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION

Asphalt shingles

Standing seam metal roof







Cementitious lap Siding

Brick

FRONT ELEVATION

Multifamily House

-  **BODY & SHINGLES**
DUTCH TILE BLUE SW0031
-  **TRIM:**
PURE WHITE SW7005
-  **BRICK:**
CHEROKEE BRICK AUGUSTA
-  **BRICK MORTAR:**
COOSA IVORY BUFF
-  **METAL ROOF:**
CHARCOAL
-  **ROOF:**
CAMBRIDGE CHARCOAL GREY
-  **GUTTERS/DOWNSPOUTS:**
WHITE
-  **FRONT DOOR OPTION 1:**
MOUNT ETNA SW7625
-  **FRONT DOOR OPTION 2:**
ANJOU PEAR

Exterior Scheme A

BODY: SPARE WHITE SW6203
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: ANJOU PEAR SW6381
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: UNDERSEAS SW6214

Exterior Scheme B

BODY: ACACIA HAZE SW9132
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: MOUNT ETNA SW7625
FRONT DOOR OPTION 2: IRON ORE SW7069

Exterior Scheme C

BODY: PRAIRIE GRASS SW7546
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: DRIZZLE SW6479
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: ROSEMARY SW6197

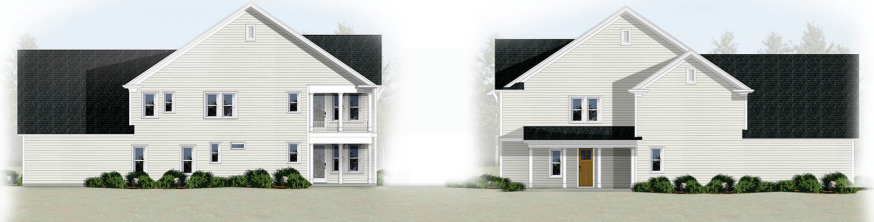
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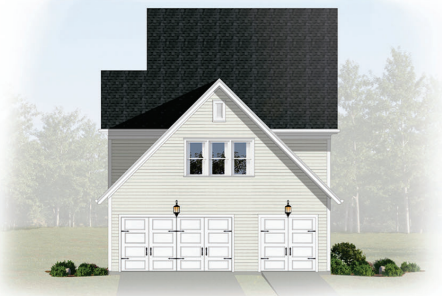
- 1. Front porch dimensions to meet minimum 4' depth x 6' width standards
- 2. Trash service: Roll-out bins

Duplex Plan 1 (A Elevation)



LEFT ELEVATION

RIGHT ELEVATION



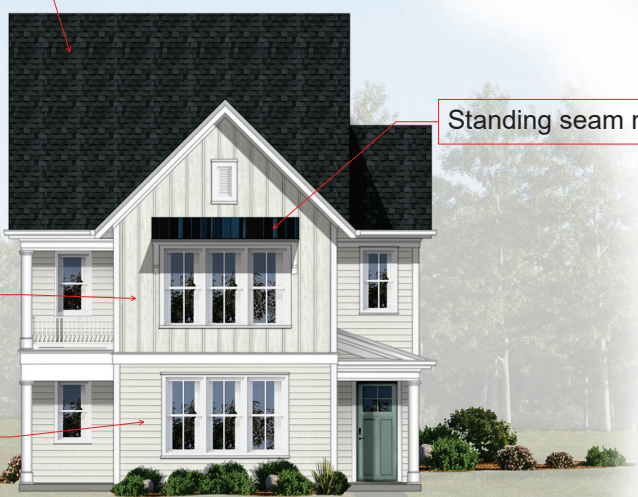
REAR ELEVATION

Asphalt shingles

Standing seam metal roof

Cementitious board & batten

Cementitious lap Siding



FRONT ELEVATION

Exterior Scheme A

- BODY & SHINGLES**
SPARE WHITE SW6203
- TRIM:**
PURE WHITE SW7005
- BRICK:**
CHEROKEE BRICK AUGUSTA
- BRICK MORTAR:**
COOSA IVORY BUFF
- METAL ROOF:**
CHARCOAL
- ROOF:**
CAMBRIDGE CHARCOAL GREY
- GUTTERS/DOWNSPOUTS:**
WHITE
- FRONT DOOR OPTION 1:**
ANJOU PEAR SW6381
- FRONT DOOR OPTION 2:**
MOUNT ETNA SW7626
- FRONT DOOR OPTION 3:**
UNDERSEAS SW6214

Exterior Scheme B

BODY: ACACIA HAZE SW9132
 TRIM: PURE WHITE SW7005
 BRICK: CHEROKEE BRICK AUGUSTA
 BRICK MORTAR: COOSA IVORY BUFF
 METAL ROOF: CHARCOAL
 ROOF: CAMBRIDGE CHARCOAL GREY
 GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: MOUNT ETNA SW7625
 FRONT DOOR OPTION 2: IRON ORE SW7069

Exterior Scheme C

BODY: PRAIRIE GRASS SW7546
 TRIM: PURE WHITE SW7005
 BRICK: CHEROKEE BRICK AUGUSTA
 BRICK MORTAR: COOSA IVORY BUFF
 METAL ROOF: CHARCOAL
 ROOF: CAMBRIDGE CHARCOAL GREY
 GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: DRIZZLE SW6479
 FRONT DOOR OPTION 2: MOUNT ETNA SW7625
 FRONT DOOR OPTION 3: ROSEMARY SW6187

Exterior Scheme D

BODY: DUTCH TILE BLUE SW0031
 TRIM: PURE WHITE SW7005
 BRICK: CHEROKEE BRICK AUGUSTA
 BRICK MORTAR: COOSA IVORY BUFF
 METAL ROOF: CHARCOAL
 ROOF: CAMBRIDGE CHARCOAL GREY
 GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: IRON ORE SW7069
 FRONT DOOR OPTION 2: ANJOUR PEAR SW6381

WINNETT COUNTY
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Variance requested for sec 230-110.8, elevations do not comply. This elevation is 0% brick.

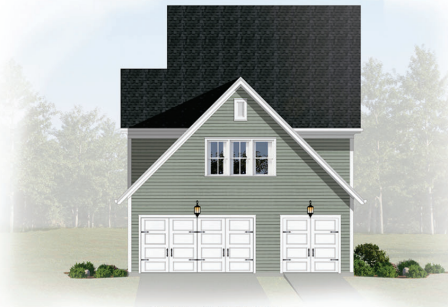
1. Front porch dimensions to meet minimum 4' depth x 6' width standards
2. Trash service: Roll-out bins

Duplex Plan 1 (B Elevation)



LEFT ELEVATION

RIGHT ELEVATION



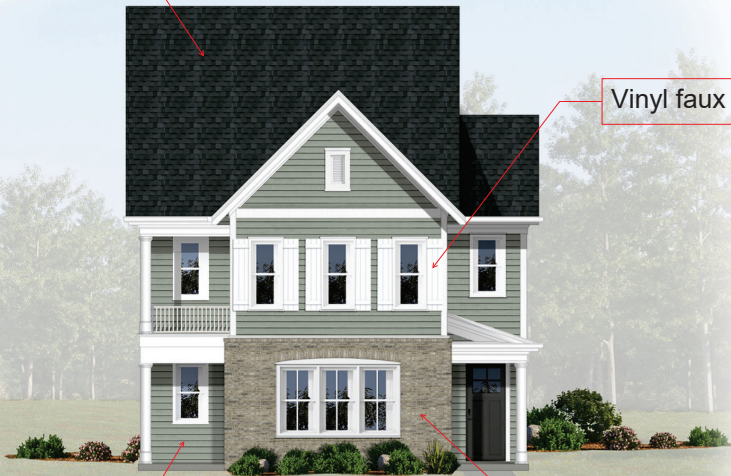
REAR ELEVATION

Asphalt shingles

Vinyl faux shutters

Cementitious lap Siding

Brick



FRONT ELEVATION

Exterior Scheme B



BODY & SHINGLES
ACACIA HAZE SW9132



TRIM:
PURE WHITE SW7005



BRICK:
CHEROKEE BRICK AUGUSTA



BRICK MORTAR:
COOSA IVORY BUFF



METAL ROOF:
CHARCOAL



ROOF:
CAMBRIDGE CHARCOAL GREY



GUTTERS/DOWNSPOUTS:
WHITE



FRONT DOOR OPTION 1:
MOUNT ETNA SW7625



FRONT DOOR OPTION 2:
IRON ORE SW7069

Exterior Scheme A

BODY: SPARE WHITE SW6203
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: ANJOU PEAR SW6381
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: UNDERSEAS SW6214

Exterior Scheme C

BODY: PRAIRIE GRASS SW7546
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: DRIZZLE SW6479
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: ROSEMARY SW6187

Exterior Scheme D

BODY: DUTCH TILE BLUE SW0031
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: IRON ORE SW7069
FRONT DOOR OPTION 2: ANJOU PEAR SW6381

WINNETT COUNTY
PLANNING AND DEVELOPMENT

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Variance requested for sec 230-110.8, elevations do not comply. This elevation is 28% brick.

- 1. Front porch dimensions to meet minimum 4' depth x 6' width standards
- 2. Trash service: Roll-out bins

Duplex Plan 1 (C Elevation)



LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION



FRONT ELEVATION

Exterior Scheme C

- BODY & SHINGLES**
PRAIRIE GRASS SW7546
- TRIM:**
PURE WHITE SW7005
- BRICK:**
CHEROKEE BRICK AUGUSTA
- BRICK MORTAR:**
COOSA IVORY BUFF
- METAL ROOF:**
CHARCOAL
- ROOF:**
CAMBRIDGE CHARCOAL GREY
- GUTTERS/DOWNSPOUTS:**
WHITE
- FRONT DOOR OPTION 1:**
DRIZZLE SW6479
- FRONT DOOR OPTION 2:**
MOUNT ETNA SW7625
- FRONT DOOR OPTION 3:**
ROSEMARY SW6187

Exterior Scheme A	Exterior Scheme B	Exterior Scheme D
BODY: SPARE WHITE SW6203 TRIM: PURE WHITE SW7005 BRICK: CHEROKEE BRICK AUGUSTA BRICK MORTAR: COOSA IVORY BUFF METAL ROOF: CHARCOAL ROOF: CAMBRIDGE CHARCOAL GREY GUTTERS/DOWNSPOUTS: WHITE FRONT DOOR OPTION 1: ANJOU PEAR SW6381 FRONT DOOR OPTION 2: MOUNT ETNA SW7625 FRONT DOOR OPTION 3: UNDERSEAS SW6214	BODY: ACACIA HAZE SW9132 TRIM: PURE WHITE SW7005 BRICK: CHEROKEE BRICK AUGUSTA BRICK MORTAR: COOSA IVORY BUFF METAL ROOF: CHARCOAL ROOF: CAMBRIDGE CHARCOAL GREY GUTTERS/DOWNSPOUTS: WHITE FRONT DOOR OPTION 1: MOUNT ETNA SW7625 FRONT DOOR OPTION 2: IRON ORE SW7069	BODY: DUTCH TILE BLUE SW0031 TRIM: PURE WHITE SW7005 BRICK: CHEROKEE BRICK AUGUSTA BRICK MORTAR: COOSA IVORY BUFF METAL ROOF: CHARCOAL ROOF: CAMBRIDGE CHARCOAL GREY GUTTERS/DOWNSPOUTS: WHITE FRONT DOOR OPTION 1: IRON ORE SW7069 FRONT DOOR OPTION 2: ANJOUR PEAR SW6381

Variance requested for sec 230-110.8, elevations do not comply. This elevation is 28% brick.

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PLANNING AND DEVELOPMENT

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BF

- 1. Front porch dimensions to meet minimum 4' depth x 6' width standards
- 2. Trash service: Roll-out bins

Duplex
Plan 12B (Attached)












LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION

Exterior Scheme B

-  **BODY & SHINGLES**
ACACIA HAZE SW9132
-  **TRIM:**
PURE WHITE SW7005
-  **BRICK:**
CHEROKEE BRICK AUGUSTA
-  **BRICK MORTAR:**
COOSA IVORY BUFF
-  **METAL ROOF:**
CHARCOAL
-  **ROOF:**
CAMBRIDGE CHARCOAL GREY
-  **GUTTERS/DOWNSPOUTS:**
WHITE
-  **FRONT DOOR OPTION 1:**
MOUNT ETNA SW7625
-  **FRONT DOOR OPTION 2:**
IRON ORE SW7069

Asphalt shingles

Vinyl faux shutters



Brick

Cementitious lap Siding

Cementitious board & batten

FRONT ELEVATION

Exterior Scheme A

BODY: SPARE WHITE SW6203
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: ANJOU PEAR SW6381
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: UNDERSEAS SW6214

Exterior Scheme C

BODY: PRAIRIE GRASS SW7546
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: DRIZZLE SW6479
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: ROSEMARY SW6187

Exterior Scheme D

BODY: DUTCH TILE BLUE SW0031
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: IRON ORE SW7069
FRONT DOOR OPTION 2: ANJOU PEAR SW6381

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Variance requested for sec 230-110.8, elevations do not comply. This elevation is 19% brick.

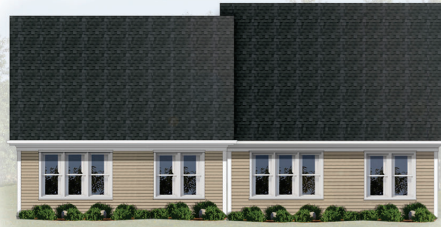
1. Front porch dimensions to meet minimum 4' depth x 6' width standards
2. Trash service: Roll-out bins

Duplex Plan 13B (Attached)

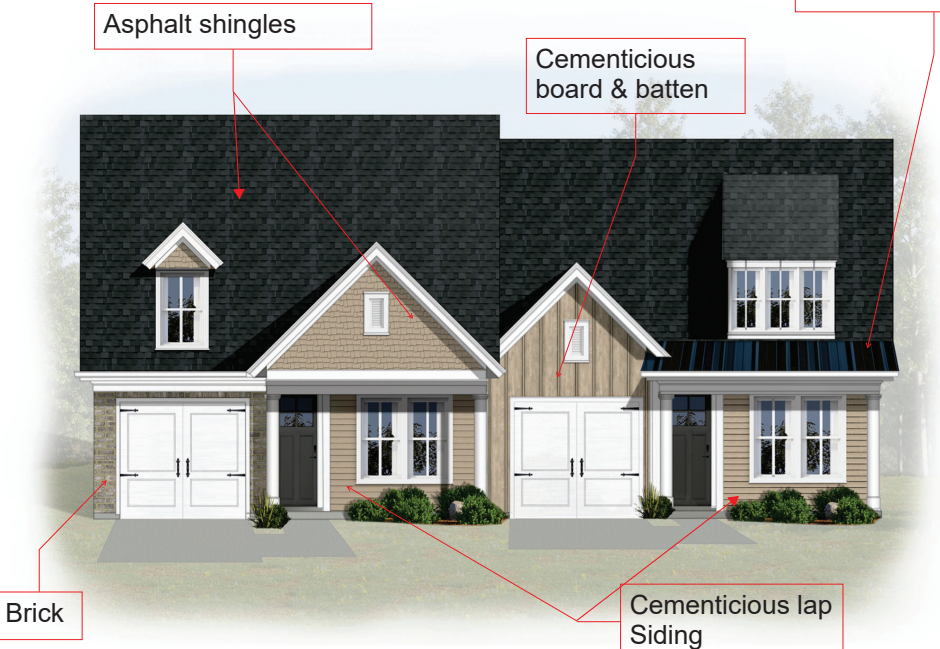


LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION



Asphalt shingles

Cementitious board & batten


Standing seam metal roof

Brick

Cementitious lap Siding

FRONT ELEVATION

Exterior Scheme C

-  **BODY & SHINGLES**
PRAIRIE GRASS SW7546
-  **TRIM:**
PURE WHITE SW7005
-  **BRICK:**
CHEROKEE BRICK AUGUSTA
-  **BRICK MORTAR:**
COOSA IVORY BUFF
-  **METAL ROOF:**
CHARCOAL
-  **ROOF:**
CAMBRIDGE CHARCOAL GREY
-  **GUTTERS/DOWNSPOUTS:**
WHITE
-  **FRONT DOOR OPTION 1:**
DRIZZLE SW6479
-  **FRONT DOOR OPTION 2:**
MOUNT ETNA SW7625
-  **FRONT DOOR OPTION 3:**
ROSEMARY SW6187

Exterior Scheme A

BODY: SPARE WHITE SW6203
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: ANJOU PEAR SW6381
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: UNDERSEAS SW6214

Exterior Scheme B

BODY: ACACIA HAZE SW9132
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: MOUNT ETNA SW7625
FRONT DOOR OPTION 2: IRON ORE SW7069

Exterior Scheme D

BODY: DUTCH TILE BLUE SW0031
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: IRON ORE SW7069
FRONT DOOR OPTION 2: ANJOU PEAR SW6381

WINNETT COUNTY
PLANNING AND DEVELOPMENT

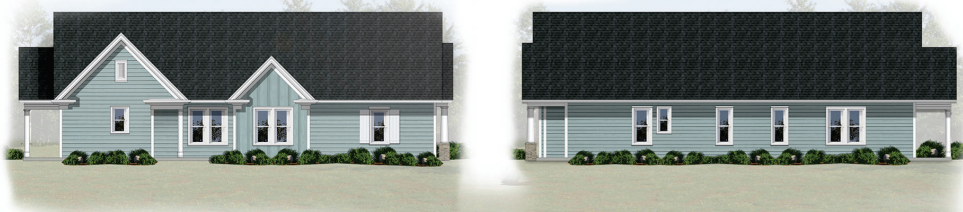
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Variance requested for sec 230-110.8, elevations do not comply. This elevation is 8% brick.

1. Front porch dimensions to meet minimum 4' depth x 6' width standards
2. Trash service: Roll-out bins

Duplex Plan 15 (A Elevation)



LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION

Asphalt shingles

Cementitious lap Siding

Vinyl faux shutters



Cementitious board & batten

Brick

FRONT ELEVATION

Exterior Scheme D

- BODY & SHINGLES**
DUTCH TILE BLUE SW0031
- TRIM:**
PURE WHITE SW7005
- BRICK:**
CHEROKEE BRICK AUGUSTA
- BRICK MORTAR:**
COOSA IVORY BUFF
- METAL ROOF:**
CHARCOAL
- ROOF:**
CAMBRIDGE CHARCOAL GREY
- GUTTERS/DOWNSPOUTS:**
WHITE
- FRONT DOOR OPTION 1:**
IRON ORE SW7069
- FRONT DOOR OPTION 2:**
ANJOUR PEAR SW6381

Exterior Scheme A	Exterior Scheme B	Exterior Scheme C
BODY: SPARE WHITE SW6203 TRIM: PURE WHITE SW7005 BRICK: CHEROKEE BRICK AUGUSTA BRICK MORTAR: COOSA IVORY BUFF METAL ROOF: CHARCOAL ROOF: CAMBRIDGE CHARCOAL GREY GUTTERS/DOWNSPOUTS: WHITE FRONT DOOR OPTION 1: ANJOU PEAR SW6381 FRONT DOOR OPTION 2: MOUNT ETNA SW7625 FRONT DOOR OPTION 3: UNDERSEAS SW6214	BODY: ACACIA HAZE SW9132 TRIM: PURE WHITE SW7005 BRICK: CHEROKEE BRICK AUGUSTA BRICK MORTAR: COOSA IVORY BUFF METAL ROOF: CHARCOAL ROOF: CAMBRIDGE CHARCOAL GREY GUTTERS/DOWNSPOUTS: WHITE FRONT DOOR OPTION 1: MOUNT ETNA SW7625 FRONT DOOR OPTION 2: IRON ORE SW7069	BODY: PRAIRIE GRASS SW7546 TRIM: PURE WHITE SW7005 BRICK: CHEROKEE BRICK AUGUSTA BRICK MORTAR: COOSA IVORY BUFF METAL ROOF: CHARCOAL ROOF: CAMBRIDGE CHARCOAL GREY GUTTERS/DOWNSPOUTS: WHITE FRONT DOOR OPTION 1: DRIZZLE SW6479 FRONT DOOR OPTION 2: MOUNT ETNA SW7625 FRONT DOOR OPTION 3: ROSEMARY SW6197

Variance requested for sec 230-110.8, elevations do not comply. This elevation is 3% brick.

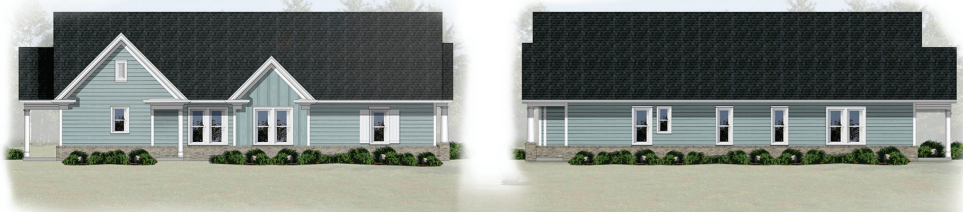
PLANNING AND DEVELOPMENT

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- 1. Front porch dimensions to meet minimum 4' depth x 6' width standards
- 2. Trash service: Roll-out bins



LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION

Vinyl faux shutters

Cementitious lap Siding

Asphalt shingles

Cementitious board & batten












Brick

FRONT ELEVATION

Duplex Plan 15 (A Elevation) Watertable

Exterior Scheme D

-  **BODY & SHINGLES**
DUTCH TILE BLUE SW0031
-  **TRIM:**
PURE WHITE SW7005
-  **BRICK:**
CHEROKEE BRICK AUGUSTA
-  **BRICK MORTAR:**
COOSA IVORY BUFF
-  **METAL ROOF:**
CHARCOAL
-  **ROOF:**
CAMBRIDGE CHARCOAL GREY
-  **GUTTERS/DOWNSPOUTS:**
WHITE
-  **FRONT DOOR OPTION 1:**
IRON ORE SW7069
-  **FRONT DOOR OPTION 2:**
ANJOUR PEAR SW6381

Exterior Scheme A

- BODY:** SPARE WHITE SW6203
- TRIM:** PURE WHITE SW7005
- BRICK:** CHEROKEE BRICK AUGUSTA
- BRICK MORTAR:** COOSA IVORY BUFF
- METAL ROOF:** CHARCOAL
- ROOF:** CAMBRIDGE CHARCOAL GREY
- GUTTERS/DOWNSPOUTS:** WHITE
- FRONT DOOR OPTION 1:** ANJOU PEAR SW6381
- FRONT DOOR OPTION 2:** MOUNT ETNA SW7625
- FRONT DOOR OPTION 3:** UNDERSEAS SW6214

Exterior Scheme B

- BODY:** ACACIA HAZE SW9132
- TRIM:** PURE WHITE SW7005
- BRICK:** CHEROKEE BRICK AUGUSTA
- BRICK MORTAR:** COOSA IVORY BUFF
- METAL ROOF:** CHARCOAL
- ROOF:** CAMBRIDGE CHARCOAL GREY
- GUTTERS/DOWNSPOUTS:** WHITE
- FRONT DOOR OPTION 1:** MOUNT ETNA SW7625
- FRONT DOOR OPTION 2:** IRON ORE SW7069

Exterior Scheme C

- BODY:** PRAIRIE GRASS SW7546
- TRIM:** PURE WHITE SW7005
- BRICK:** CHEROKEE BRICK AUGUSTA
- BRICK MORTAR:** COOSA IVORY BUFF
- METAL ROOF:** CHARCOAL
- ROOF:** CAMBRIDGE CHARCOAL GREY
- GUTTERS/DOWNSPOUTS:** WHITE
- FRONT DOOR OPTION 1:** DRIZZLE SW6479
- FRONT DOOR OPTION 2:** MOUNT ETNA SW7625
- FRONT DOOR OPTION 3:** ROSEMARY SW6197

Variance requested for sec 230-110.8, elevations do not comply. This elevation is 19% brick.

SWINNEY COUNTY
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- 1. Front porch dimensions to meet minimum 4' depth x 6' width standards
- 2. Trash service: Roll-out bins

Duplex
Plan 15 (B Elevation)



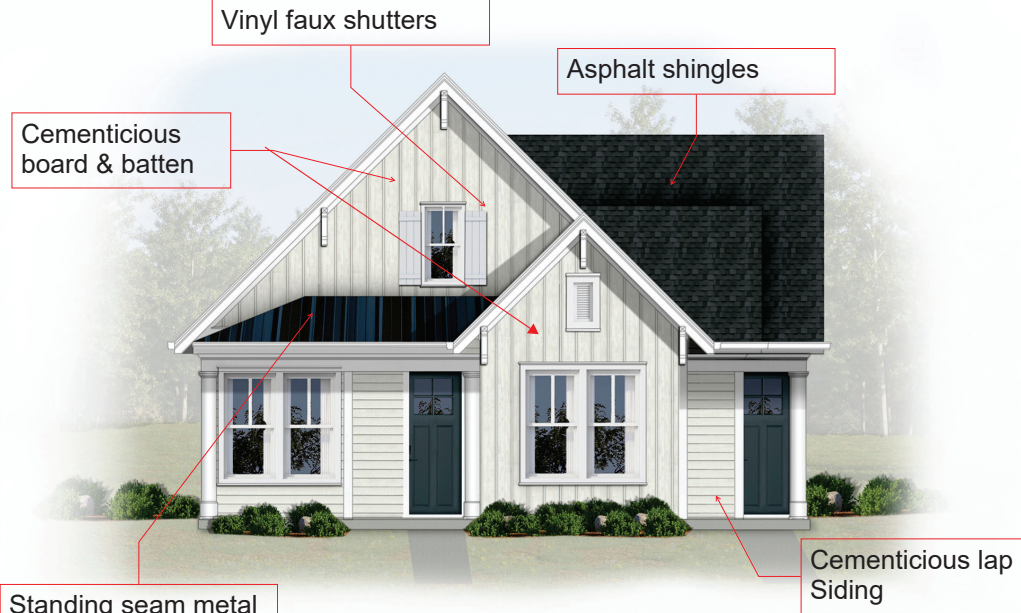
LEFT ELEVATION



RIGHT ELEVATION



REAR ELEVATION



FRONT ELEVATION

Exterior Scheme A

- BODY & SHINGLES SPARE WHITE SW6203
- TRIM: PURE WHITE SW7005
- BRICK: CHEROKEE BRICK AUGUSTA
- BRICK MORTAR: COOSA IVORY BUFF
- METAL ROOF: CHARCOAL
- ROOF: CAMBRIDGE CHARCOAL GREY
- GUTTERS/DOWNSPOUTS: WHITE
- FRONT DOOR OPTION 1: ANJOU PEAR SW6381
- FRONT DOOR OPTION 2: MOUNT ETNA SW7626
- FRONT DOOR OPTION 3: UNDERSEAS SW6214

Exterior Scheme B

BODY: ACACIA HAZE SW9132
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: MOUNT ETNA SW7625
FRONT DOOR OPTION 2: IRON ORE SW7069

Exterior Scheme C

BODY: PRAIRIE GRASS SW7546
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: DRIZZLE SW6479
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: ROSEMARY SW6187

Exterior Scheme D

BODY: DUTCH TILE BLUE SW0031
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: IRON ORE SW7069
FRONT DOOR OPTION 2: ANJOUR PEAR SW6381

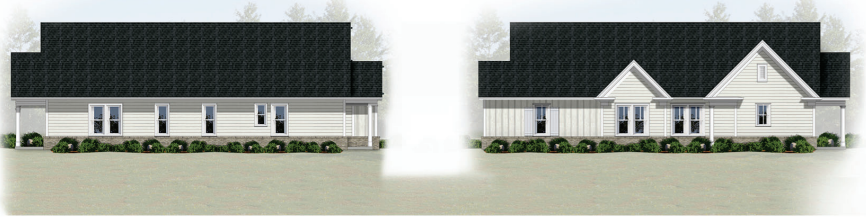
WINNETT COUNTY
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Variance requested for sec 230-110.8, elevations do not comply. This elevation is 0% brick.

1. Front porch dimensions to meet minimum 4' depth x 6' width standards
2. Trash service: Roll-out bins

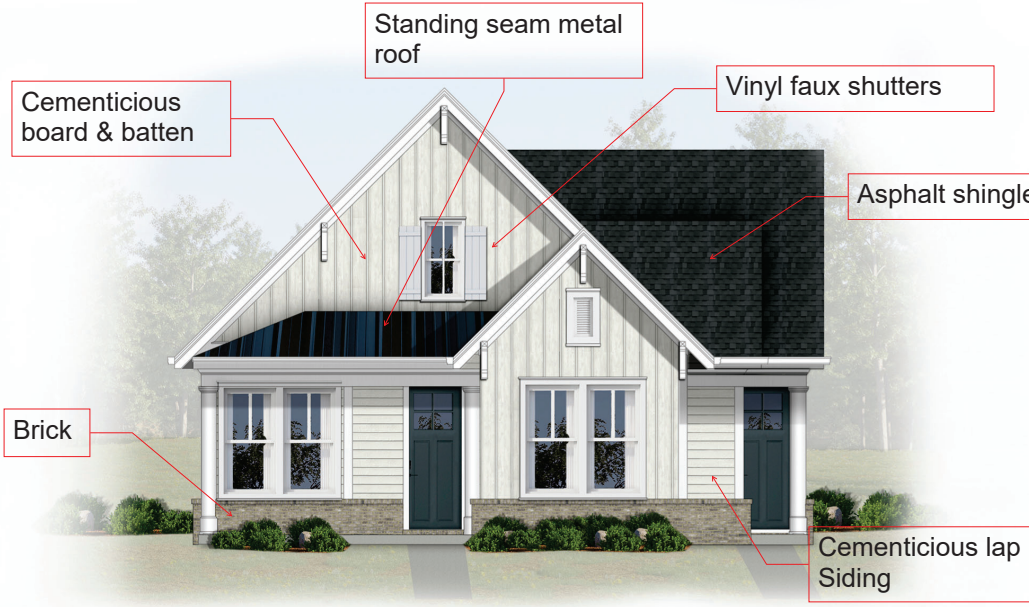


LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION



Cementitious board & batten

Standing seam metal roof

Vinyl faux shutters

Asphalt shingles

Brick

Cementitious lap Siding

FRONT ELEVATION

Duplex Plan 15 (B Elevation) Watertable

Exterior Scheme A



BODY & SHINGLES
SPARE WHITE SW6203



TRIM:
PURE WHITE SW7005



BRICK:
CHEROKEE BRICK AUGUSTA



BRICK MORTAR:
COOSA IVORY BUFF



METAL ROOF:
CHARCOAL



ROOF:
CAMBRIDGE CHARCOAL GREY



GUTTERS/DOWNSPOUTS:
WHITE



FRONT DOOR OPTION 1:
ANJOU PEAR SW6381



FRONT DOOR OPTION 2:
MOUNT ETNA SW7626



FRONT DOOR OPTION 3:
UNDERSEAS SW6214

Exterior Scheme B

BODY: ACACIA HAZE SW9132
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: MOUNT ETNA SW7625
FRONT DOOR OPTION 2: IRON ORE SW7069

Exterior Scheme C

BODY: PRAIRIE GRASS SW7546
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: DRIZZLE SW6479
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: ROSEMARY SW6187

Exterior Scheme D

BODY: DUTCH TILE BLUE SW0031
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: IRON ORE SW7069
FRONT DOOR OPTION 2: ANJOUR PEAR SW6381

WINNETT COUNTY
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Variance requested for sec 230-110.8, elevations do not comply. This elevation is 16% brick.

1. Front porch dimensions to meet minimum 4' depth x 6' width standards
2. Trash service: Roll-out bins

Single Family Detached Plan 12A (A Elevation)



LEFT ELEVATION



RIGHT ELEVATION



REAR ELEVATION



FRONT ELEVATION

Asphalt shingles

Cementitious lap Siding

Brick

Exterior Scheme A



BODY & SHINGLES
SPARE WHITE SW6203



TRIM:
PURE WHITE SW7005



BRICK:
CHEROKEE BRICK AUGUSTA



BRICK MORTAR:
COOSA IVORY BUFF



METAL ROOF:
CHARCOAL



ROOF:
CAMBRIDGE CHARCOAL GREY



GUTTERS/DOWNSPOUTS:
WHITE



FRONT DOOR OPTION 1:
ANJOU PEAR SW6381



FRONT DOOR OPTION 2:
MOUNT ETNA SW7626



FRONT DOOR OPTION 3:
UNDERSEAS SW6214

Exterior Scheme B

BODY: ACACIA HAZE SW9132
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: MOUNT ETNA SW7625
FRONT DOOR OPTION 2: IRON ORE SW7069

Exterior Scheme C

BODY: PRAIRIE GRASS SW7546
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: DRIZZLE SW6479
FRONT DOOR OPTION 2: MOUNT ETNA SW7625
FRONT DOOR OPTION 3: ROSEMARY SW6187

Exterior Scheme D

BODY: DUTCH TILE BLUE SW0031
TRIM: PURE WHITE SW7005
BRICK: CHEROKEE BRICK AUGUSTA
BRICK MORTAR: COOSA IVORY BUFF
METAL ROOF: CHARCOAL
ROOF: CAMBRIDGE CHARCOAL GREY
GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: IRON ORE SW7069
FRONT DOOR OPTION 2: ANJOU PEAR SW6381

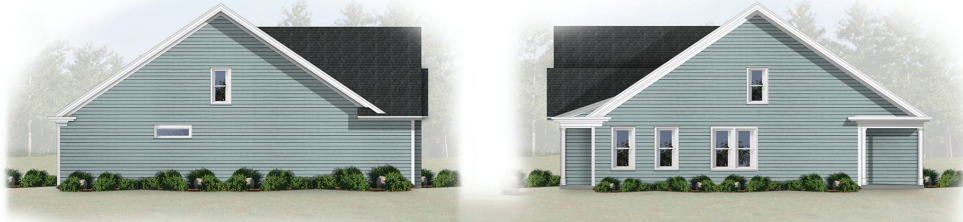
WINNETT COUNTY
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- 1. Front porch dimensions to meet minimum 4' depth x 6' width standards
- 2. Trash service: Roll-out bins

Single Family Detached Plan 12A (B Elevation)



LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION

Asphalt shingles



Cementitious board & batten

Cementitious lap Siding

FRONT ELEVATION

Exterior Scheme D

- BODY & SHINGLES**
DUTCH TILE BLUE SW0031
- TRIM:**
PURE WHITE SW7005
- BRICK:**
CHEROKEE BRICK AUGUSTA
- BRICK MORTAR:**
COOSA IVORY BUFF
- METAL ROOF:**
CHARCOAL
- ROOF:**
CAMBRIDGE CHARCOAL GREY
- GUTTERS/DOWNSPOUTS:**
WHITE
- FRONT DOOR OPTION 1:**
IRON ORE SW7069
- FRONT DOOR OPTION 2:**
ANJOUR PEAR SW6381

Exterior Scheme A	Exterior Scheme B	Exterior Scheme C
<p>BODY: SPARE WHITE SW6203 TRIM: PURE WHITE SW7005 BRICK: CHEROKEE BRICK AUGUSTA BRICK MORTAR: COOSA IVORY BUFF METAL ROOF: CHARCOAL ROOF: CAMBRIDGE CHARCOAL GREY GUTTERS/DOWNSPOUTS: WHITE</p> <p>FRONT DOOR OPTION 1: ANJOUR PEAR SW6381 FRONT DOOR OPTION 2: MOUNT ETNA SW7625 FRONT DOOR OPTION 3: UNDERSEAS SW6214</p>	<p>BODY: ACACIA HAZE SW9132 TRIM: PURE WHITE SW7005 BRICK: CHEROKEE BRICK AUGUSTA BRICK MORTAR: COOSA IVORY BUFF METAL ROOF: CHARCOAL ROOF: CAMBRIDGE CHARCOAL GREY GUTTERS/DOWNSPOUTS: WHITE</p> <p>FRONT DOOR OPTION 1: MOUNT ETNA SW7625 FRONT DOOR OPTION 2: IRON ORE SW7069</p>	<p>BODY: PRAIRIE GRASS SW7546 TRIM: PURE WHITE SW7005 BRICK: CHEROKEE BRICK AUGUSTA BRICK MORTAR: COOSA IVORY BUFF METAL ROOF: CHARCOAL ROOF: CAMBRIDGE CHARCOAL GREY GUTTERS/DOWNSPOUTS: WHITE</p> <p>FRONT DOOR OPTION 1: DRIZZLE SW6479 FRONT DOOR OPTION 2: MOUNT ETNA SW7625 FRONT DOOR OPTION 3: ROSEMARY SW6197</p>

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- 1. Front porch dimensions to meet minimum 4' depth x 6' width standards
- 2. Trash service: Roll-out bins

Single Family Detached Plan 13A (A Elevation)



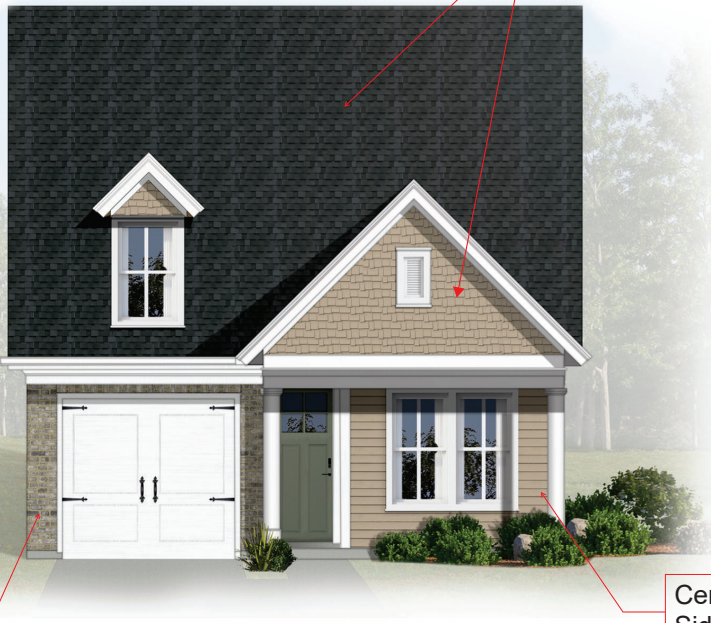
LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION

Asphalt shingles



FRONT ELEVATION

Brick

Cementitious lap Siding

Exterior Scheme C

- BODY & SHINGLES**
PRAIRIE GRASS SW7546
- TRIM:**
PURE WHITE SW7005
- BRICK:**
CHEROKEE BRICK AUGUSTA
- BRICK MORTAR:**
COOSA IVORY BUFF
- METAL ROOF:**
CHARCOAL
- ROOF:**
CAMBRIDGE CHARCOAL GREY
- GUTTERS/DOWNSPOUTS:**
WHITE
- FRONT DOOR OPTION 1:**
DRIZZLE SW6479
- FRONT DOOR OPTION 2:**
MOUNT ETNA SW7625
- FRONT DOOR OPTION 3:**
ROSEMARY SW6187

Exterior Scheme A

BODY: SPARE WHITE SW6203
 TRIM: PURE WHITE SW7005
 BRICK: CHEROKEE BRICK AUGUSTA
 BRICK MORTAR: COOSA IVORY BUFF
 METAL ROOF: CHARCOAL
 ROOF: CAMBRIDGE CHARCOAL GREY
 GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: ANJOU PEAR SW6381
 FRONT DOOR OPTION 2: MOUNT ETNA SW7625
 FRONT DOOR OPTION 3: UNDERSEAS SW6214

Exterior Scheme B

BODY: ACACIA HAZE SW9132
 TRIM: PURE WHITE SW7005
 BRICK: CHEROKEE BRICK AUGUSTA
 BRICK MORTAR: COOSA IVORY BUFF
 METAL ROOF: CHARCOAL
 ROOF: CAMBRIDGE CHARCOAL GREY
 GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: MOUNT ETNA SW7625
 FRONT DOOR OPTION 2: IRON ORE SW7069

Exterior Scheme D

BODY: DUTCH TILE BLUE SW0031
 TRIM: PURE WHITE SW7005
 BRICK: CHEROKEE BRICK AUGUSTA
 BRICK MORTAR: COOSA IVORY BUFF
 METAL ROOF: CHARCOAL
 ROOF: CAMBRIDGE CHARCOAL GREY
 GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: IRON ORE SW7069
 FRONT DOOR OPTION 2: ANJOUR PEAR SW6381

GWINNETT COUNTY
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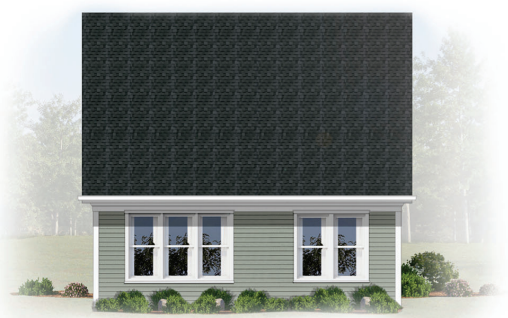
1. Front porch dimensions to meet minimum 4' depth x 6' width standards
2. Trash service: Roll-out bins

Single Family Detached Plan 13A (B Elevation)



LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION

Standing seam metal roof

Asphalt shingles












Cementitious board & batten

Cementitious lap Siding

FRONT ELEVATION

Exterior Scheme B

-  **BODY & SHINGLES**
ACACIA HAZE SW9132
-  **TRIM:**
PURE WHITE SW7005
-  **BRICK:**
CHEROKEE BRICK AUGUSTA
-  **BRICK MORTAR:**
COOSA IVORY BUFF
-  **METAL ROOF:**
CHARCOAL
-  **ROOF:**
CAMBRIDGE CHARCOAL GREY
-  **GUTTERS/DOWNSPOUTS:**
WHITE
-  **FRONT DOOR OPTION 1:**
MOUNT ETNA SW7625
-  **FRONT DOOR OPTION 2:**
IRON ORE SW7069

Exterior Scheme A

BODY: SPARE WHITE SW6203
 TRIM: PURE WHITE SW7005
 BRICK: CHEROKEE BRICK AUGUSTA
 BRICK MORTAR: COOSA IVORY BUFF
 METAL ROOF: CHARCOAL
 ROOF: CAMBRIDGE CHARCOAL GREY
 GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: ANJOU PEAR SW6381
 FRONT DOOR OPTION 2: MOUNT ETNA SW7625
 FRONT DOOR OPTION 3: UNDERSEAS SW6214

Exterior Scheme C

BODY: PRAIRIE GRASS SW7546
 TRIM: PURE WHITE SW7005
 BRICK: CHEROKEE BRICK AUGUSTA
 BRICK MORTAR: COOSA IVORY BUFF
 METAL ROOF: CHARCOAL
 ROOF: CAMBRIDGE CHARCOAL GREY
 GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: DRIZZLE SW6479
 FRONT DOOR OPTION 2: MOUNT ETNA SW7625
 FRONT DOOR OPTION 3: ROSEMARY SW6187

Exterior Scheme D

BODY: DUTCH TILE BLUE SW0031
 TRIM: PURE WHITE SW7005
 BRICK: CHEROKEE BRICK AUGUSTA
 BRICK MORTAR: COOSA IVORY BUFF
 METAL ROOF: CHARCOAL
 ROOF: CAMBRIDGE CHARCOAL GREY
 GUTTERS/DOWNSPOUTS: WHITE

FRONT DOOR OPTION 1: IRON ORE SW7069
 FRONT DOOR OPTION 2: ANJOU PEAR SW6381

**WINNETT COUNTY
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Clubhouse



LEFT ELEVATION

RIGHT ELEVATION



REAR ELEVATION



Asphalt shingles

Standing seam metal roof

Cementitious board & batten

Pressure treated wood

Metal gutters & downspouts

FRONT ELEVATION

Exterior Scheme



BODY & SHINGLES
WHITE



TRIM:
SPARE WHITE SW6203



BRICK:
PAINTED WHITE



BRICK MORTAR:
WHITE



METAL ROOF:
CHARCOAL



ROOF:
CAMBRIDGE CHARCOAL GREY



GUTTERS/DOWNSPOUTS:
URBANE BRONZE



FRONT DOOR OPTION 1:
Black



FRONT DOOR OPTION 2:
ANJOUR PEAR SW6381

Hayloft Garden Shed



LEFT ELEVATION



RIGHT ELEVATION



REAR ELEVATION

Community Mailboxes

Pressure treated wood

Cementitious lap Siding

Standing seam metal roof



Metal gutters & downspouts

FRONT ELEVATION

Exterior Scheme



**BODY & SHINGLES
WHITE**



**TRIM:
PURE WHITE SW7005**



**BRICK:
CHEROKEE BRICK AUGUSTA**



**BRICK MORTAR:
COOSA IVORY BUFF**



**METAL ROOF:
CHARCOAL**



**ROOF:
CAMBRIDGE CHARCOAL GREY**



**GUTTERS/DOWNSPOUTS:
WHITE**

**GWINNETT COUNTY
PLANNING AND DEVELOPMENT**

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1.26.26

Exhibit D: Letter of Intent and Applicant's Response to Standards

[attached]

David Belle Isle
Matthew P. Benson
Catherine W. Davidson
Gerald Davidson, Jr.*
Brian T. Easley
Rebecca B. Gober
John D. Hipes*
Christopher D. Holbrook
Jessica R. Kelly
Shane M. Lanham

Julia A. Maxwell
Jeffrey R. Mahaffey
John N. Mahaffey
Steven A. Pickens
Jack M. Ryan
Gabrielle H. Schaller
S. Tess Shaheen
Andrew D. Stancil
R. Lee Tucker, Jr.
Kassie M. Wilson

*Of Counsel

LETTER OF INTENT FOR REZONING APPLICATION
OF ONE STREET RESIDENTIAL, LLC

Mahaffey Pickens Tucker, LLP submits this Letter of Intent and attached rezoning application (the “Application”) on behalf of One Street Residential, LLC, (the “Applicant” or “One Street Residential”), to request the rezoning of an approximately 16.78-acre tract (the “Property”) situated along the southern side of Loganville Highway (State Route 20) at its intersection with Hope Hollow Road. The Property is designated as within the Traditional/Traditional+ Future Development Area of the Gwinnett County 2045 Unified Plan (the “2045 Plan”), it is located within the Grayson/Highway 20 Overlay District, and is currently zoned C-2 (General Business District).

The Applicant submits the Application requesting to rezone the Property to the R-IF (Residential In-Fill District) zoning classification of the Gwinnett County Unified Development Ordinance (the “UDO”) in order to develop the Property as an attractive mixed-residential community with a variety of housing types. The proposed community is designed and intended for active adults (55+) under One Street Residential’s Hayloft brand with amenities and floor plans curated for that specific age demographic. The proposed development would include single-family detached homes, single-family attached homes, courtyard homes, stacked duplexes, and flats buildings which would be owned, managed, and operated by a single entity which would lease homes to residents. As depicted in the building elevations submitted with the Application, homes in the community would be constructed of high-end building materials with attractive traditional and farmhouse architectural styles. The community would also feature a robust central amenity area with a community clubhouse including the on-site management office, fitness and yoga center, club room, reservable dining/board room, and a serving kitchen. Other amenities will include pickleball court(s), gaming lawn and/or putting green, raised garden beds and potting shed, dog park, walking trails, outdoor living room patio with grilling station, and other passive open space. The proposed community would also provide a traditional neighborhood aesthetic and streetscape with residential buildings pushed up along the rights-of-way of Hope Hollow Road and Loganville Highway with direct pedestrian connections to both internal and external sidewalks. A new five-foot-wide sidewalk would be constructed along the Property’s frontage on Hope Hollow Road and a 12-foot-wide multi-use trail would be provided along the Property’s Loganville Highway frontage. Garages would include EV charging capabilities with compatible 240-volt

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Alpharetta Office || 178 South Main Street, Suite 250, Alpharetta, Georgia 30009

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outlets. Vehicular access to the community would be provided by a proposed right-in/right-out driveway on Loganville Highway and a proposed full-access driveway on Hope Hollow Road aligned with the staff/bus-only entrance to Grayson High School. The proposed community was also designed to provide appropriate screening and separation from the adjacent single-family detached community to the west, which is zoned R-100. Even though the UDO does not require a zoning buffer between R-IF and single-family detached residential uses, the proposed development would provide a double staggered row of evergreen screen trees planted along the Property's northwesterly boundary line.

The proposed development is also compatible with surrounding land uses and zoning classifications. The surrounding area is characterized by a mix of land uses including single-family detached residential, single-family attached residential, commercial, and institutional uses. To the northwest across Highway 20 are the Independence villas and townhomes zoned TND, to the west at the intersection of Oak Grove Road and Highway 20 is land zoned C-2 which is the future home of the Encompass Health rehabilitation hospital. Just across Oak Grove Road from there are the Seaboard Junction attached senior villas zoned R-SR. To the south, is Grayson High School and to the east is additional land zoned C-2 for general commercial uses. The proposed mixed-residential community is compatible with these existing surrounding uses in the area and would also provide an appropriate transition from more intense commercial uses along Highway 20 to the single-family detached residential uses to the south.

The proposed development is also compatible with the policies of the 2045 Plan. Generally, the 2045 Plan promotes residential development and encourages residential development with a variety of housing types. Specifically, one of the 2045 Plan's "Key Takeaways" is that "Gwinnett will need more than single family development to accommodate projected population growth. Not all current residents want to live in single family homes, and future residents may also want more diverse housing options." Additionally, the 2045 Plan provides that "[in order to] meet projected market demand based on population growth, approximately 8,500 new units must be built in Gwinnett annually." In addressing the need for a diversity of housing types, the 2045 Plan states that the "continued predominance of single family unit construction will not meet this demand." The 2045 Plan also addresses several trends that specifically apply to Gwinnett County and help shed light on the demand for a variety of housing types. According to the 2045 Plan, Gwinnett County has seen an increase in the percentage of households without children, higher demand for rental housing, and more demand for missing middle housing. Further, "These alternative housing types, such as duplexes, garden apartments, or townhomes, help support a live-work economy where individuals of a variety of means and income levels can comfortably hold jobs and reside in the area. Allowing for greater options that offer new choices depending on residents' needs can also help ease the burden of rising home prices seen across the county."

One Street Residential's primary focus since 2009 has been developing, owning, and managing active-adult rental communities through its Hayloft brand, which was conceived after identifying a missing housing type offering for the 55+ consumer. Specifically, One Street Residential identified strong demand for 1- and 2-bedroom rental cottage homes which offer a "lock-and-leave" lifestyle without a long-term commitment (i.e. ownership). The cottage-style homes offer floor plans designed with active adults and empty-nesters in mind, but don't come with services they don't want/need or require traditional apartment living. The unique designs of

the homes coupled with the programmed amenities and facilitation by a professional management company caters specifically to this market and is something that is sorely needed in the community. The demographic demand for a Hayloft community in this location is strong and will provide a unique housing option that is commercially taxed, limits additional traffic, and does not provide any burden to local schools.

Building on the 2045 Plan's general encouragement of residential development and, specifically, missing middle housing types, the Traditional+ Future Development area specifically encourages the proposed zoning classification and the type of community that is proposed in the Application. First, the Traditional+ Future Development designation applies to the Property because of its access to sewer infrastructure as well as its location of less than one mile to a Village Center. There are three sewer manholes on the Property and it is located approximately one quarter mile from the Village Center at the intersection of Highway 20 and Oak Grove Road. The 2045 Plan lists R-IF as a limited recommended zoning classification and specifically encourages "townhomes/stacked flats," "quads/triplexes/duplexes," "single family cottages," and "single family" as "appropriate" land uses for the Traditional+ area. At 8.3 units per acre, the proposed development is also in line with the 2045 Plan's recommended "target density" of 7-8 units per acre for Traditional+. Moreover, the proposed development offers an opportunity for existing 55+ residents to age in place by providing housing options specifically designed for active adults. The 2045 Plan recognizes the need for "Increased Housing for Seniors" and provides that "[t]he County's reliance on single family is a positive for younger families within their prime working ages, but could represent a challenge for an aging population." Providing options for residents to age in place allows residents to remain in their community as they progress through various stages of life. Aging in place means that residents can stay connected to their friends, family, and other community groups even as their housing demands change.

In order to develop the Property as described in the Application, the Applicant also requests two variances to allow all residential units in the development to be provided on a single tax parcel without a requirement to subdivide the Property into individual lots. As described above, the proposed development would provide several different housing unit types and floorplans, which are available in different configurations. According to the UDO, the proposed housing types fit within the definitions of "Duplex Dwelling," "Multiplex Dwelling," and "Single-Family Detached Dwelling." Accordingly, the Applicant requests variances from UDO §210-140.13(E) and UDO §210-140.17(J) to eliminate requirements to provide the duplex and multiplex buildings on individual lots and, rather, allow all the community's building types to be provided on a single parcel. The Applicant also requests a variance from UDO §230-110.8(B)(1) to eliminate the requirement to provide a minimum of 50% brick or stone on the front façade of buildings. The proposed community has a very specific aesthetic as set forth in the building elevations submitted with the Application utilizing traditional and farmhouse architectural styles which rely less heavily on brick and stone facades. However, the homes in the community would feature enhanced architectural treatments over and above the UDO requirements and would be constructed with other long-life, low-maintenance building materials such as cementitious siding in board-and-batten, shake, and lap siding configurations.

The requested variances arise from conditions that are unique and peculiar to the land, structures, and buildings involved. The requests are necessary because the particular physical

surroundings, as well as the size, shape and topographical condition of the Property result in an unnecessary hardship for the owner, lessee, and/or occupants; as distinguished from a mere inconvenience, if the provisions of the UDO are literally enforced. The conditions requiring the requested relief are not ordinarily found in properties of the same zoning district as the subject property. The requests are the result of conditions created by the regulations of the UDO and not by an action or actions of the property owner or the Applicant. Granting the requested variances/waivers would not impair or injure other property or improvements in the neighborhood in which the Property is located, impair an adequate supply of light or air to adjacent property, substantially increase the congestion in the public streets, increase the danger of fire, create a hazard to air navigation, endanger the public safety, or substantially diminish or impair property values within the neighborhood. The variances/waivers requested are the minimum variances/waivers that will make possible the reasonable use of the land, building, or structures. Approval of the requested variances/waivers would further the general spirit and intent of the UDO and/or the purpose and intent of the 2045 Plan.

The Applicant and its representatives welcome the opportunity to meet with staff of the Gwinnett County Department of Planning & Development to answer any questions or to address any concerns relating to the matters set forth in this letter or in the Rezoning Application filed herewith. The Applicant respectfully requests your approval of these Applications.

Respectfully submitted this 23rd day of January, 2026.

MAHAFFEY PICKENS TUCKER, LLP

Shane Lanham

Shane M. Lanham
Attorneys for the Applicant

JUSTIFICATION FOR REZONING

The portions of the Gwinnett County Unified Development Ordinance (the “UDO”) which classify or may classify the property which is the subject of this Application (the “Property”) into any less intensive zoning classification other than as requested by the Applicant, are or would be unconstitutional in that they would destroy the Applicant's property rights without first paying fair, adequate and just compensation for such rights, in violation of Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States.

The application of the UDO, as applied to the subject Property, which restricts its use to the present zoning classification, is unconstitutional, illegal, null and void, constituting a taking of the Applicant's and the Owner’s property in violation of the Just Compensation Clause of the Fifth Amendment and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States, Article I, Section I, Paragraph I, and Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Equal Protection Clause of the Fourteenth Amendment to the Constitution of the United States denying the Applicant an economically viable use of its land while not substantially advancing legitimate state interests.

The Property is presently suitable for development under the R-IF classification as requested by the Applicant, and is not economically suitable for development under the present C-2 zoning classification of Gwinnett County. A denial of this Application would constitute an arbitrary and capricious act by the Gwinnett County Board of Commissioners without any rational basis therefore, constituting an abuse of discretion in violation of Article I, Section I, Paragraph I and Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States.

A refusal by the Gwinnett County Board of Commissioners to rezone the Property to the R-IF classification with such conditions as agreed to by the Applicant, so as to permit the only feasible economic use of the Property, would be unconstitutional and discriminate in an arbitrary, capricious and unreasonable manner between the Applicant and owners of similarly situated property in violation of Article I, Section I, Paragraph II of the Constitution of the State of Georgia

of 1983 and the Equal Protection Clause of the Fourteenth Amendment to the Constitution of the United States. Any rezoning of the subject Property to the R-IF classification, subject to conditions which are different from the conditions by which the Applicant may amend its application, to the extent such different conditions would have the effect of further restricting the Applicant's and the Owner's utilization of the subject Property, would also constitute an arbitrary, capricious and discriminatory act in zoning the Property to an unconstitutional classification and would likewise violate each of the provisions of the State and Federal Constitutions set forth hereinabove.

Opponents to the request set forth in the Application, or in any amendments to the Application, have waived their rights to appeal any decision of the Gwinnett County Board of Commissioners because they lack standing, have failed to exhaust administrative remedies, and/or because they failed to assert any legal or constitutional objections.

Accordingly, the Applicant respectfully requests that the rezoning application submitted by the Applicant relative to the Property be granted and that the Property be rezoned to the zoning classification as shown on the respective application.

This 19th day of February, 2026.

Respectfully submitted,

MAHAFFEY PICKENS TUCKER, LLP

Shane Lanham

Shane M. Lanham

Attorneys for Applicant

1550 North Brown Road
Suite 125
Lawrenceville, Georgia 30043
(770) 232-0000

Pursuant to requirements of the United Development Ordinance, the Board of Commissioners finds that the following standards are relevant in balancing the interest in promoting the public health, safety, morality, or general welfare against the right to the unrestricted use of property and shall govern the exercise of the zoning power.

- A. Whether a proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property:
Yes, approval of the Rezoning Application will permit a use that is suitable in view of the use and development of adjacent and nearby property. The proposed mixed-residential development is compatible with the surrounding mix of uses, including institutional, commercial, attached, and detached residential uses.
- B. Whether a proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property:
No, approval of the Rezoning Application will not adversely affect the existing use or usability of any of the nearby properties. Rather, the proposed development will complement surrounding land uses by providing additional housing options for current and future residents of Gwinnett County.
- C. Whether the property to be affected by a proposed rezoning has reasonable economic use as currently zoned:
Due to the size, location, layout, and dimensions of the subject property, the Applicant submits that the Property does not have a reasonable economic use as currently zoned.
- D. Whether the proposed rezoning will result in a use which could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools:
No, approval of the Rezoning Application will not result in an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools. The proposed development has direct access to Loganville Hwy and Hope Hollow Road. Additionally, water and sewer utilities are present in the immediate vicinity. As a community designed for active adults (55+), the development will have minimal impacts on traffic congestion and school enrollment.
- E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map:
Yes, approval of the Rezoning Application is in conformity with the policy and intent of the Gwinnett County 2045 Unified Plan, which specifically recommends R-IF as an appropriate zoning district.
- F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning:
The Applicant submits that the quality of the proposed development and the character of surrounding uses provide additional supporting grounds for approval of the application.

Exhibit E: Application and Disclosure of Campaign Contributions

[attached]

REZONING APPLICATION

AN APPLICATION TO AMEND THE OFFICIAL ZONING MAP OF GWINNETT COUNTY, GA.

APPLICANT INFORMATION	PROPERTY OWNER INFORMATION
Name: <u>One Street Residential, LLC c/o Mahaffey Pickens Tucker, LLP</u> Address: <u>1550 North Brown Road, Suite 125</u> City: <u>Lawrenceville</u> State: <u>GA</u> ZIP: <u>30043</u> Phone: <u>770.232.0000</u> Email: <u>slanham@mptlawfirm.com</u>	Name: <u>Grayson Development LLC</u> Address: <u>12645 Birmingham Hwy</u> City: <u>Alpharetta</u> State: <u>GA</u> ZIP: <u>30004</u> Phone: <u>770.232.0000</u> Email: <u>slanham@mptlawfirm.com</u>
Contact Person: <u>Shane Lanham</u> Phone: <u>770.232.0000</u> Contact's Email: <u>slanham@mptlawfirm.com</u>	
APPLICANT IS THE:	
<input checked="" type="checkbox"/> Owner's Agent <input type="checkbox"/> Property Owner <input type="checkbox"/> Contract Purchaser	
Current Zoning District(s): <u>C-2</u> Requested Zoning District: <u>R-IF</u> Parcel Number(s): <u>5157 341</u> Acreage: <u>16.78</u> Property Address(es): <u>3175 Loganville Highway</u> Proposed Development: <u>Mixed Residential 55+ Community</u> Variance(s): <u>1. Sec. 210-140.13, 2. Sec. 210-140.17(J), 3. Sec. 230-110.8(B.)(1)</u> Waiver(s): <u>N/A</u>	
RESIDENTIAL DEVELOPMENT No. of Dwelling Units: <u>139</u> Dwelling Unit Sq. Ft.: <u>786 to 2,206</u> Density: <u>8.3 du/ac</u> Floor Area Ratio (LRR, MRR, HRR): <u>N/A</u>	NON-RESIDENTIAL DEVELOPMENT No. of Buildings: _____ Total Building Sq. Ft.: _____ Floor Area Ratio: _____
MIXED-USE DEVELOPMENT	
No. of Dwelling Units: _____ Dwelling Unit Sq. Ft.: _____ Total Non-Residential Sq. Ft.: _____ Floor Area Ratio: _____	



GWINNETT COUNTY
PLANNING AND DEVELOPMENT
PROPERTY OWNER CERTIFICATION

The undersigned below, or as attached, is the owner of the property considered in this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon in less than six (6) months from the date of last action by the Board of Commissioners.

Property Owner

Name: [Signature]

Signature: Irwin W. Stolz, III

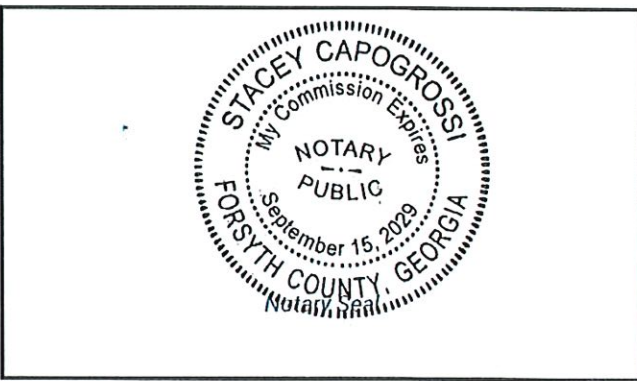
Date: 11/24/2025

Notary Public

Name: Stacey Capogrossi

Signature: [Signature]

Date: 11/24/2025





Disclosure of campaign contributions aggregating to \$250.00 or more to a member of the Board of Commissioners is required pursuant to Section 270-25.E. of the Unified Development Ordinance.

Name: Kassie Prater

Relationship to Application: Applicant Representative of Applicant

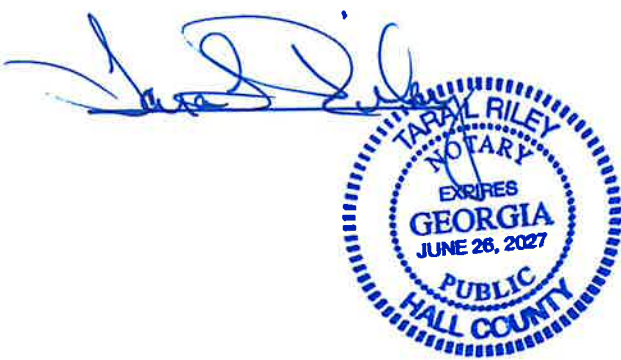
Campaign Contributions? Yes No

Contributions:

Official Name	Contribution Amount	Date of Contribution	Contributor
See Attached			

Signature: *Kassie Prater*

Date: 3/24/20



**CONFLICT OF INTEREST CERTIFICATION FOR REZONING/CHANGE IN
CONDITIONS AND SPECIAL USE PERMIT APPLICATIONS**

MAHAFFEY PICKENS TUCKER, LLP

<u>NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL</u>	<u>CONTRIBUTION</u>	<u>DATE</u>
Nicole Love Hendrickson, Chairwoman	\$3,300	10/01/2024
Ben Ku, Commissioner	\$2,000	04/25/2025
Matthew Holtkamp, Commissioner	\$2,000	06/19/2025
Ben Ku, Commissioner	\$1,300	02/03/2026


Shane M. Lanham, Attorney at Law, PC

<u>NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL</u>	<u>CONTRIBUTION</u>	<u>DATE</u>
Nicole Love Hendrickson, Chairwoman	\$500.00	10/02/2024

Updated 08/29/2025


CONFLICT OF INTEREST CERTIFICATION FOR REZONING

The undersigned below, making application for a rezoning, has complied with the Official Code of Georgia Section 36-67A-1, et. seq, Conflict of Interest in Zoning Actions, and has submitted or attached the required information on the forms provided.

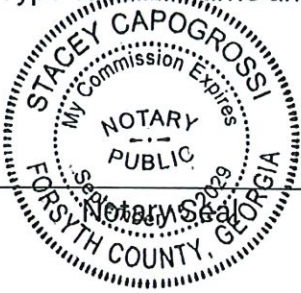

11/24/25
Irwin W. Stolz III
Member,
Grayson Development, LLC

Signature of Applicant _____ Date _____ Type of Print Name and Title _____

Signature of Applicant's Attorney or Representative _____ Date _____ Type of Print Name and Title _____


11/24/2025

Signature of Notary Public _____ Date _____



DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

Have you, within the two years immediately preceding the filing of this application, made campaign contributions aggregating \$250.00 or more to a member of the Board of Commissioners?

Yes
 No
 Grayson Development, LLC (Your Name)

If the answer is yes, please complete the following section:

NAME AND OFFICAL POSITION OF GOVERNMENT OFFICIAL	CONTRIBUTIONS (List all which aggregate to \$250 or More)	DATE CONTRIBUTION WAS MADE (Within last two years)

Attach additional sheets if necessary to disclose or describe all contributions.



The undersigned certifies that all Gwinnett County property taxes are current. In no such case shall an application or reapplication be processed without such property tax verification.

Applicant Information

Name: Kassie Prater

Signature: Kassie Prater

Date: 11/18/2025

Property Information

Enter all parcel IDs in the table below. [Search for Parcel ID](#)

Parcel ID(s) (by Applicant)	Property Taxes Paid (Yes or No) (Tax Commissioner Use Only)
5157 341	Yes

Tax Commissioner Affirmation

All Gwinnett County property taxes are current for the above referenced parcel(s) and confirmed by the signature below.

Name: Jessilyn McDuffie

Title: TSA

Signature: [Handwritten Signature]

Date: 11/20/25

Exhibit F: DWR Sewer Capacity Certification Letter

[attached]



October 21, 2025

Rob Rojas OneStreet Residential 104 Interstate N Pkwy E SE Atlanta, GA 30339

APPROVED DENIED CONDITIONALLY APPROVED Sewer Capacity Request #C2025-283-10 Expiration Date: 10/21/2026 Tie-In Manhole FID: 3842235

RE: Sewer Availability for Proposed Development – OneStreet Hope Hollow Parcel ID 5157 341

Dear Rob Rojas:

This letter supersedes the earlier Sewer Capacity Certification C2021-06-143 dated June 10, 2021 addressed to Amy Heicher of Ozora Development, LLC.

Gwinnett County Department of Water Resources (GCDWR) has reviewed the impact of the proposed development consisting of 139 single-family/duplex residences, amenity center, and pool on the above parcel and determined:

Gwinnett County has adequate sewer capacity to serve the proposed development.

This confirmation is based on your anticipated annual average daily flow of 24.5 gpm discharging to the sewer tie-in manhole at Facility ID 3842235.

Capacity Allocation: Sewer capacities are allocated only upon the issuance of a Development Permit, by Gwinnett County Planning & Development and after all applicable conditions established in this Sewer Capacity Certification, if any, are met.

Certification Expiration: The Sewer Capacity Certification is valid for 12 months from the date of this letter. If rezoning is approved, GCDWR will issue a revised Sewer Capacity Certification based on the approved units, as needed. The capacity request can be renewed for an additional 12 months. If a Development Permit has not been obtained by the expiration date above the developer may apply for renewal by providing evidence of active rezoning, plan review, or permitting efforts.

Once the Sewer Capacity Certification expires, the capacity is no longer allocated, and a new Sewer Capacity Certification request must be submitted for re-evaluation with no guarantee of available capacity. The evaluation will be based on the updated conditions of the sewer system.

Certification Invalidation: This certification is not valid if there are proposed changes to your development that could impact downstream sewer capacity (e.g. land use density, sewer tie-in



manhole, parcel changes, and/or rezoning). In this case the development must resubmit for a new Sewer Capacity Request with updated information prior to the expiration of the previous one.

Please contact us at 678-376-7026 if you have any questions.

Sincerely,

GWINNETT COUNTY DEPARTMENT OF WATER RESOURCES

Della Taylor, PE
Division Director, Infrastructure Support
678.376.2130

C: Gwinnett County DWR – Lorraine Campagne, Section Manager; Raghu Vemuru, Engineer V; Mikala Weston, Engineer III; Geniva Sylvain, Engineer III
Gwinnett County P&D – Charli Young, Planning Manager; Christina Dejarnette, Engineer II; Wendell Tumale Engineer III

Exhibit G: Traffic Impact Study

[attached]

**TRAFFIC IMPACT STUDY
FOR
PROPOSED RESIDENTIAL DEVELOPMENT ON
SR 20 (LOGANVILLE HIGHWAY) AND HOPE HOLLOW
ROAD
GWINNETT COUNTY, GEORGIA**



Prepared for:

***OneStreet Residential
104 Interstate N. Pkwy E SE
Atlanta, GA 30339***

Prepared By:



A&R Engineering Inc.

2160 Kingston Court, Suite O
Marietta, GA 30067
Tel: (770) 690-9255 Fax: (770) 690-9210
www.areneg.com

October 09, 2025
A & R Project # 25-187

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1.0 INTRODUCTION

The purpose of this study is to determine the traffic impact from the proposed residential development that will be located in the northwest corner of the intersection of SR 20 (Loganville Highway) and Hope Hollow Road in Gwinnett County, Georgia. The traffic analysis includes evaluation of the current operations and future conditions with the traffic generated by the development. The proposed residential development will consist of a 139-unit, age restricted 55+ community development. It proposes one full-access driveway on Hope Hollow Road aligned across from Grayson High School's bus and staff only driveway, as well as a right-in/right-out driveway on SR 20 (Loganville Highway).

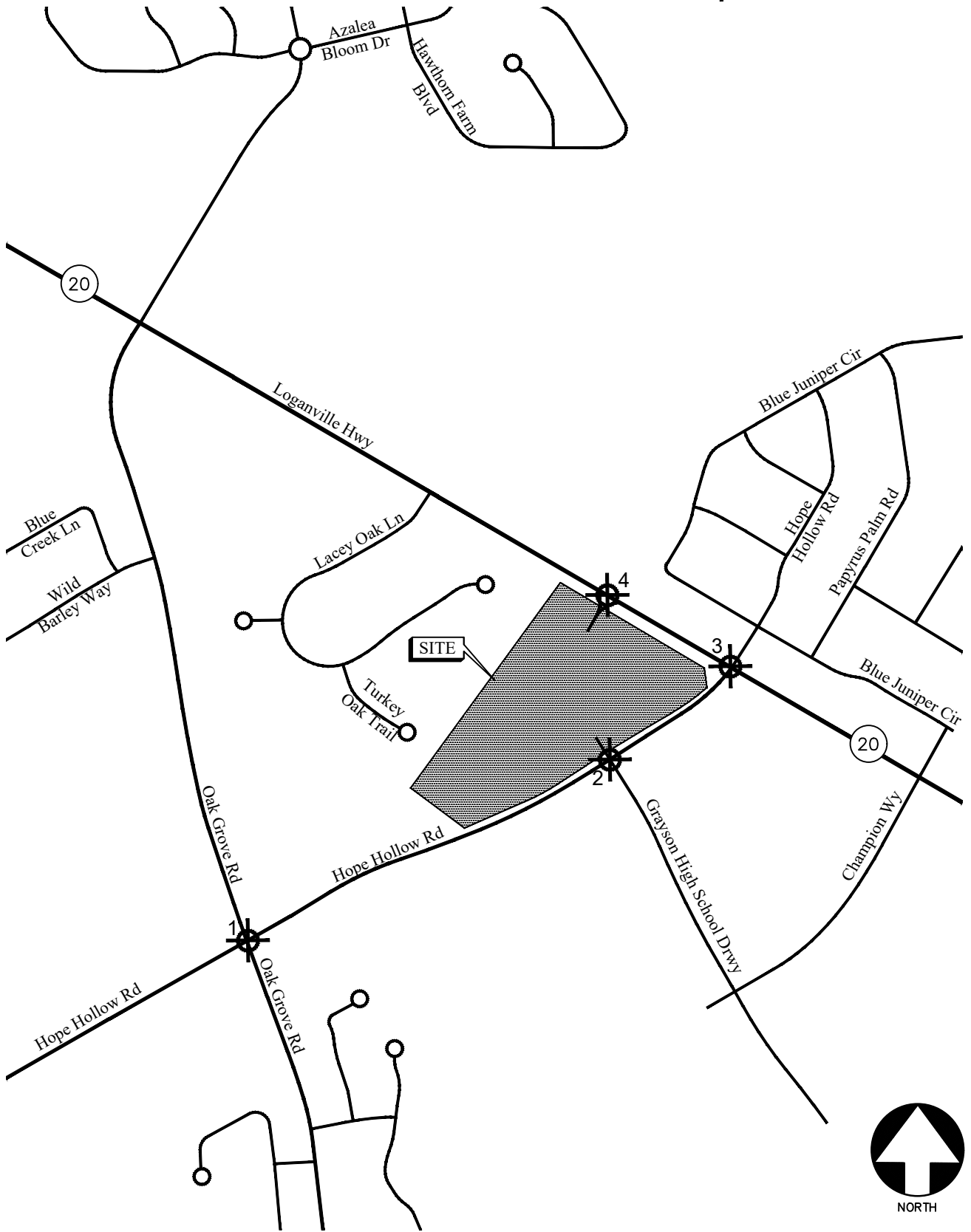


The AM, School Dismissal, and PM peak hours have been analyzed in this study. This study includes the evaluation of traffic operations at the intersections of:

1. Hope Hollow Road at Oak Grove Road
2. Hope Hollow Road at Grayson High School Driveway/Site Driveway 1
3. SR 20 (Loganville Highway) at Site Driveway 2
4. SR 20 (Loganville Highway) at Hope Hollow Road

Recommendations to improve traffic operations have been identified as appropriate and are discussed in detail in the following sections of the report. The location of the development and the surrounding roadway network are shown in Figure 1.

Study Intersection



LOCATION MAP

FIGURE 1
A&R Engineering Inc.

2.0 EXISTING FACILITIES / CONDITIONS

2.1 Roadway Facilities

The following is a brief description of each of the roadway facilities located in proximity to the site:

2.1.1 SR 20 (Loganville Highway)

SR 20 (Loganville Highway) is a north-south, four-lane, median divided roadway with a posted speed limit of 45 mph in the vicinity of the site. Georgia Department of Transportation (GDOT) traffic counts (Station ID's 135-0154 & 135-0152) indicate that the daily traffic volume on SR 20 (Loganville Highway) in 2024 was 21,500 vehicles per day east of The Esplanade Way and 32,800 vehicles per day north of Cooper Road. As per Gwinnett County Long Range Road classification (LRCC) map, SR 20 (Loganville Highway) is classified as a principal arterial roadway.

2.1.2 Oak Grove Road

Oak Grove Road is a north-south, two-lane, undivided roadway with a posted speed limit of 40 mph in the vicinity of the site. Gwinnett County classifies Oak Grove Road as a major collector roadway.

2.1.3 Hope Hollow Road

Hope Hollow Road is an east-west, two-lane undivided roadway with a posted speed limit of 35 mph west of SR 20 (Loganville Highway) and 25 mph east of SR 20 (Loganville Highway), in the vicinity of the site. Gwinnett County classifies Hope Hollow Road as a local roadway.

3.0 STUDY METHODOLOGY

In this study, the methodology used for evaluating traffic operations at each of the subject intersections is based on the criteria set forth in the Transportation Research Board’s Highway Capacity Manual, 6th edition (HCM 6). Synchro software, which utilizes the HCM methodology, was used for the analysis. The following is a description of the methodology employed for the analysis of unsignalized and signalized intersections.

3.1 Unsignalized Intersections

For unsignalized intersections controlled by a stop sign on minor streets, the level of service (LOS) for motor vehicles with controlled movements is determined by the computed control delay according to the thresholds stated in Table 1 below. LOS is determined for each minor street movement (or shared movement), as well as major street left turns. LOS is not defined for the intersection as a whole or for major street approaches. The LOS of any controlled movement which experiences a volume-to-capacity ratio greater than 1 is designated as “F” regardless of the control delay.

Control delay for unsignalized intersections includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. Several factors affect the control delay for unsignalized intersections, such as the availability and distribution of gaps in the conflicting traffic stream, critical gaps, and follow-up time for a vehicle in the queue.

Level of service is assigned a letter designation from “A” through “F”. Level of service “A” indicates excellent operations with little delay to motorists, while level of service “F” exists when there are insufficient gaps of acceptable size to allow vehicles on the side street to cross the main road without experiencing long total delays.

TABLE 1 — LEVEL-OF-SERVICE CRITERIA FOR UNSIGNALIZED INTERSECTIONS		
Control Delay (sec/vehicle)	LOS by Volume-to-Capacity Ratio*	
	v/c ≤ 1.0	v/c > 1.0
≤ 10	A	F
> 10 and ≤ 15	B	F
> 15 and ≤ 25	C	F
> 25 and ≤ 35	D	F
> 35 and ≤ 50	E	F
> 50	F	F

*The LOS criteria apply to each lane on a given approach and to each approach on the minor street. LOS is not calculated for major-street approaches or for the intersection.

Source: Highway Capacity Manual, 6th edition, Exhibit 20-2 *LOS Criteria: Motorized Vehicle Mode*

3.2 Signalized Intersections

According to HCM procedures, LOS can be calculated for the entire intersection, each intersection approach, and each lane group. HCM uses control delay alone to characterize LOS for the entire intersection or an approach. Control delay per vehicle is composed of initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. Both control delay and volume-to-capacity ratio are used to characterize LOS for a lane group. A volume-to-capacity ratio of greater than 1.0 for a lane group indicates failure from capacity perspective. Therefore, such a lane group is assigned LOS F regardless of the amount of control delay.

Table 2 below summarizes the LOS criteria from HCM for motorized vehicles at signalized intersection.

TABLE 2 — LEVEL-OF-SERVICE CRITERIA FOR SIGNALIZED INTERSECTIONS		
Control Delay (sec/vehicle) *	LOS for Lane Group by Volume-to-Capacity Ratio*	
	v/c ≤ 1.0	v/c > 1.0
≤ 10	A	F
> 10 and ≤ 20	B	F
> 20 and ≤ 35	C	F
> 35 and ≤ 55	D	F
> 55 and ≤ 80	E	F
> 80	F	F

*For approach-based and intersection wide assessments, LOS is defined solely by control delay

Source: Highway Capacity Manual, 6th edition, Exhibit 19-8 *LOS Criteria: Motorized Vehicle Mode*

LOS A is typically assigned when the volume-to-capacity (v/c) ratio is low and either progression is exceptionally favorable, or the cycle length is very short. LOS B is typically assigned when the v/c ratio is low and either progression is highly favorable, or the cycle length is short. However, more vehicles are stopped than with LOS A. LOS C is typically assigned when progression is favorable, or the cycle length is moderate. Individual *cycle failures* (one or more queued vehicles are not able to depart because of insufficient capacity during the cycle) may begin to appear at this level. Many vehicles still pass through the intersection without stopping, but the number of vehicles stopping is significant. LOS D is typically assigned when the v/c ratio is high and either progression is ineffective, or the cycle length is long. There are many vehicle-stops and individual cycle failures are noticeable. LOS E is typically assigned when the v/c ratio is high, progression is very poor, the cycle length is long, and individual cycle failures are frequent. LOS F is typically assigned when the v/c ratio is very high, progression is very poor, the cycle length is long, and most cycles fail to clear the queue.

4.0 ADDITIONAL INFORMATION – GWINNETT COUNTY LEVEL 2 TRAFFIC IMPACT STUDY

The following sections have been included per Gwinnett County’s Traffic Impact Study Guidelines (updated June 30, 2023) to meet the requirements of a Level 2 traffic study. Additional information regarding access management/spacing, intersection sight distance, or connectivity and circulation should be requested from the civil site engineer.

4.1 Future ADT Volumes

According to GDOT data, the daily traffic volume on SR 20 (Loganville Highway) in 2024 was 21,500 vehicles per day east of The Esplanade Way. Based on the proposed site traffic generation/distribution, it is estimated that the ADT on this section of SR 20 (Loganville Highway) would be approximately 22,682 vehicles per day following the completion of the development in 2027. Given how these estimated increased also factor in an assumed yearly growth rate of 1% from 2024 to 2027, it can be concluded that the impact of the proposed site on the ADTs for the roadway within the study network will be minimal.

4.2 Truck Volumes and Circulation

Since the proposed site will be residential use, it is not expected to have any notable impact on truck volumes/circulation in the study network.

4.3 Summary of Existing Pedestrian and Bicycle Facilities and Connectivity

Pedestrian crosswalks and sidewalks exist at the intersection of SR 20 (Loganville Highway) and Hope Hollow Road. This study does not include any recommendations for new pedestrian and/or bicycle facilities within or near the study network. Any additional information regarding planned pedestrian and/or bicycle facilities within the proposed residential development itself should be requested from the civil site engineer.

4.4 Existing Transit Routes and Stops/Proposed Transit Routes

None of Gwinnett County’s current transit routes or stops are located within a 0.5-mile radius of the proposed development. No transit routes have been proposed to service this development in the future.

4.5 Traffic Operation Analysis Requirements

A detailed description of the study methodology is available in section 3 on pages 4-5. A description of the site trip generation and distribution estimations is available on page 14.

4.6 Future Identified Projects (i.e., GCCTP, GDOT, SPLOST)

There are no planned/scheduled GDOT/Gwinnet County projects that will be completed within the study network or have an impact on the study network

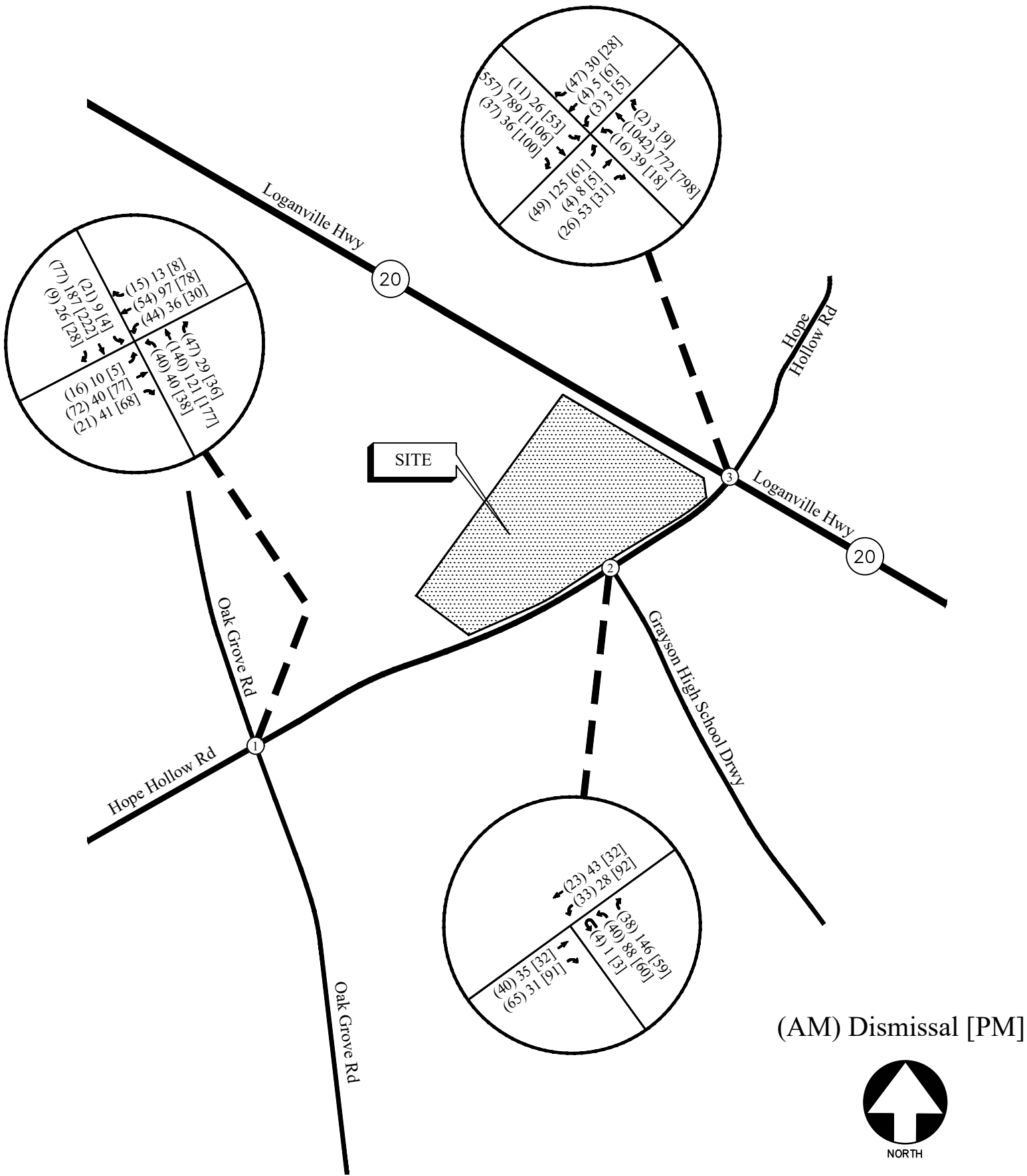
5.0 EXISTING 2025 TRAFFIC ANALYSIS

5.1 Existing Traffic Volumes

Existing traffic counts were obtained at the following study intersections:

1. Hope Hollow Road at Oak Grove Road
2. Hope Hollow Road at Grayson High School Driveway
3. SR 20 (Loganville Highway) at Hope Hollow Road




Turning movement counts were collected on Tuesday, September 30, 2025. All turning movement counts were recorded during the AM, School Dismissal and PM peak hours between 7:00 AM to 9:00 AM, 2:00 PM to 6:00 PM respectively. The four consecutive 15-minute interval volumes that summed to produce the highest volume at the intersections were then determined. These volumes make up the peak hour traffic volumes for the intersections counted and are shown in Figure 2. The existing traffic control and lane geometry for the intersections are shown in Figure 3.

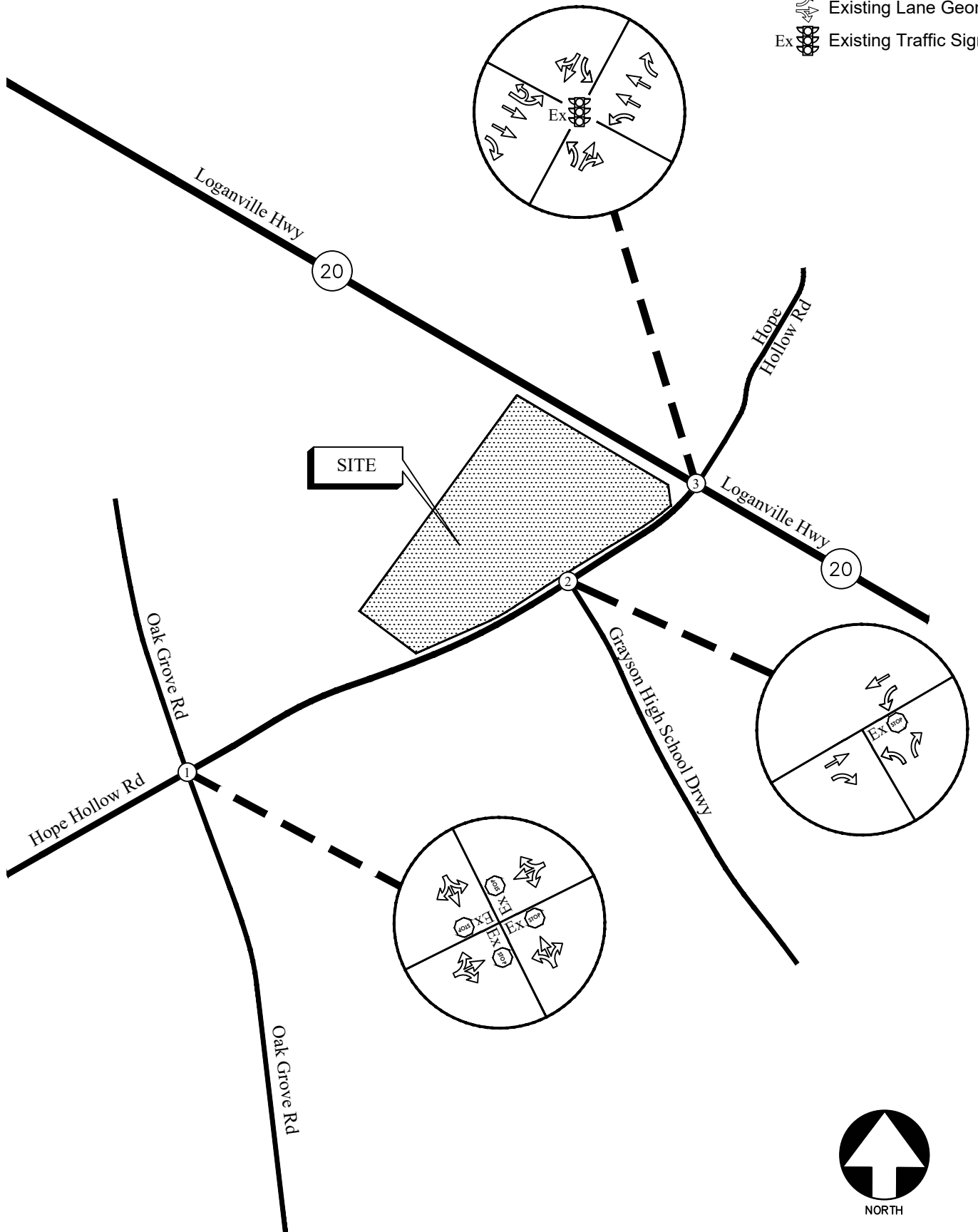


EXISTING WEEKDAY PEAK-HOUR VOLUMES

FIGURE 2
A&R Engineering Inc.

LEGEND

- Ex  Existing Signed Approach
-  Existing Lane Geometry
- Ex  Existing Traffic Signal



EXISTING TRAFFIC CONTROL AND LANE GEOMETRY

**FIGURE 3
A&R Engineering Inc.**

5.2 Existing Traffic Operations

Existing 2025 traffic operations were analyzed at the study intersections in accordance with the HCM methodology. The results of the analyses are shown in Table 3.

TABLE 3 – EXISTING INTERSECTION OPERATIONS					
Intersection		Traffic Control	LOS (Delay)		
			AM Peak	School Dismissal	PM Peak
1	<u>Hope Hollow Road at Oak Grove Road</u>	All Way Stop Controlled	<u>B (11.3)</u>	<u>B (10.2)</u>	<u>B (11.0)</u>
	-Eastbound Approach		B (10.4)	A (9.2)	B (10.1)
	-Westbound Approach		B (10.6)	B (10.2)	B (10.1)
	-Northbound Approach		B (12.7)	B (10.2)	B (11.4)
	-Southbound Approach	B (10.2)	B (10.7)	B (11.4)	
2	<u>Hope Hollow Road at Grayson High School Driveway</u>	Stop Controlled on NB Approach			
	-Westbound Left		A (7.5)	A (7.4)	A (7.4)
	-Northbound Approach	B (10.3)	B (10.2)	A (9.8)	
3	<u>SR 20 (Loganville Highway) at Hope Hollow Road</u>	Signalized	<u>A (7.9)</u>	<u>B (12.3)</u>	<u>A (7.7)</u>
	-Eastbound Approach		D (54.8)	D (51.2)	E (55.4)
	-Westbound Approach		D (52.5)	D (44.8)	D (52.4)
	-Northbound Approach		A (3.7)	A (5.2)	A (2.7)
	-Southbound Approach		A (5.0)	A (9.0)	A (5.7)

The results of the existing traffic operations analysis indicate that the signalized study intersection SR 20 (Loganville Highway) at Hope Hollow Road is operating at an overall level of service “B” or better in all the AM, School Dismissal and PM peak hours. The stop-controlled approaches at the unsignalized study intersections are operating at a level of service “B” or better in all the AM, School Dismissal and PM peak hours.

6.0 PROPOSED DEVELOPMENT

The proposed residential development will be located in the northwest corner of the intersection of SR 20 (Loganville Highway) and Hope Hollow Road in Gwinnett County, Georgia. The development will consist of 139 senior adult residential units and proposes one full-access driveway on Hope Hollow Road across from Grayson High School driveway, as well as a right-in/right-out driveway on SR 20 (Loganville Highway).



A site plan is shown in Figure 4.



6.1 Trip Generation

Trip generation estimates for the project were based on the rates and equations published in the 12th edition of the Institute of Transportation Engineers (ITE) Trip Generation Manual. This reference contains traffic volume count data collected at similar facilities nationwide. The trip generation was based on the ITE Land Use: 251 – Senior Adult Housing – Single Family. The calculated total trip generation for the proposed development is shown in Table 4A.

TABLE 4A – TRIP GENERATION (PROPOSED DEVELOPMENT)											
Land Use	Size	AM Peak Hour			School Dismissal Peak Hour			PM Peak Hour			24 Hour
		Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Two-Way
ITE 251 – Senior Adult Housing – Single Family	139 Units	17	33	50	30	27	57	35	22	57	779

6.1.1 Existing Zoning

For comparison purposes, site-generated traffic volumes for land-uses under existing zoning were estimated below. The trip generation was based on the ITE Land Uses: 650 – Free-Standing Emergency Room, 881 – Pharmacy/Drugstore with Drive-Through Window, 932 – High-Turnover (Sit-Down) Restaurant and 934 – Fast-Food Restaurant with Drive-Through Window. The estimated volumes for the existing zoning are shown in Table 4B.

TABLE 4B – TRIP GENERATION (EXISTING ZONING)											
Land Use	Size	AM Peak Hour			School Dismissal Peak Hour			PM Peak Hour			24 Hour
		Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Two-Way
ITE 650 –Free-Standing Emergency Room	3000 SF	2	1	3	2	2	4	2	3	5	75
Passby Trips (0%) 0%		0	0	0	0	0	0	0	0	0	0
ITE 881–Pharmacy/Drugstore with Drive-Through Window	8000 SF	16	15	31	37	41	78	41	41	82	942
Passby Trips (0%) 49%		0	0	0	-18	-20	-38	-20	-20	-40	-400
ITE 932–High-Turnover (Sit-Down) Restaurant	6000 SF	30	24	54	12	16	28	34	21	55	623
Passby Trips (0%) 43%		0	0	0	-5	-7	-12	-15	-9	-24	-240
ITE 934 – Fast-Food Restaurant with Drive-Through Window	11,400 SF	193	186	379	151	166	317	187	173	360	5,109
Passby Trips (50%) 55%		-97	-93	-190	-83	-91	-174	-103	-95	-198	-1980
Total without Reductions		241	226	467	202	225	427	264	238	502	6,749
Total with Reductions		144	133	277	96	107	203	126	114	240	4,129

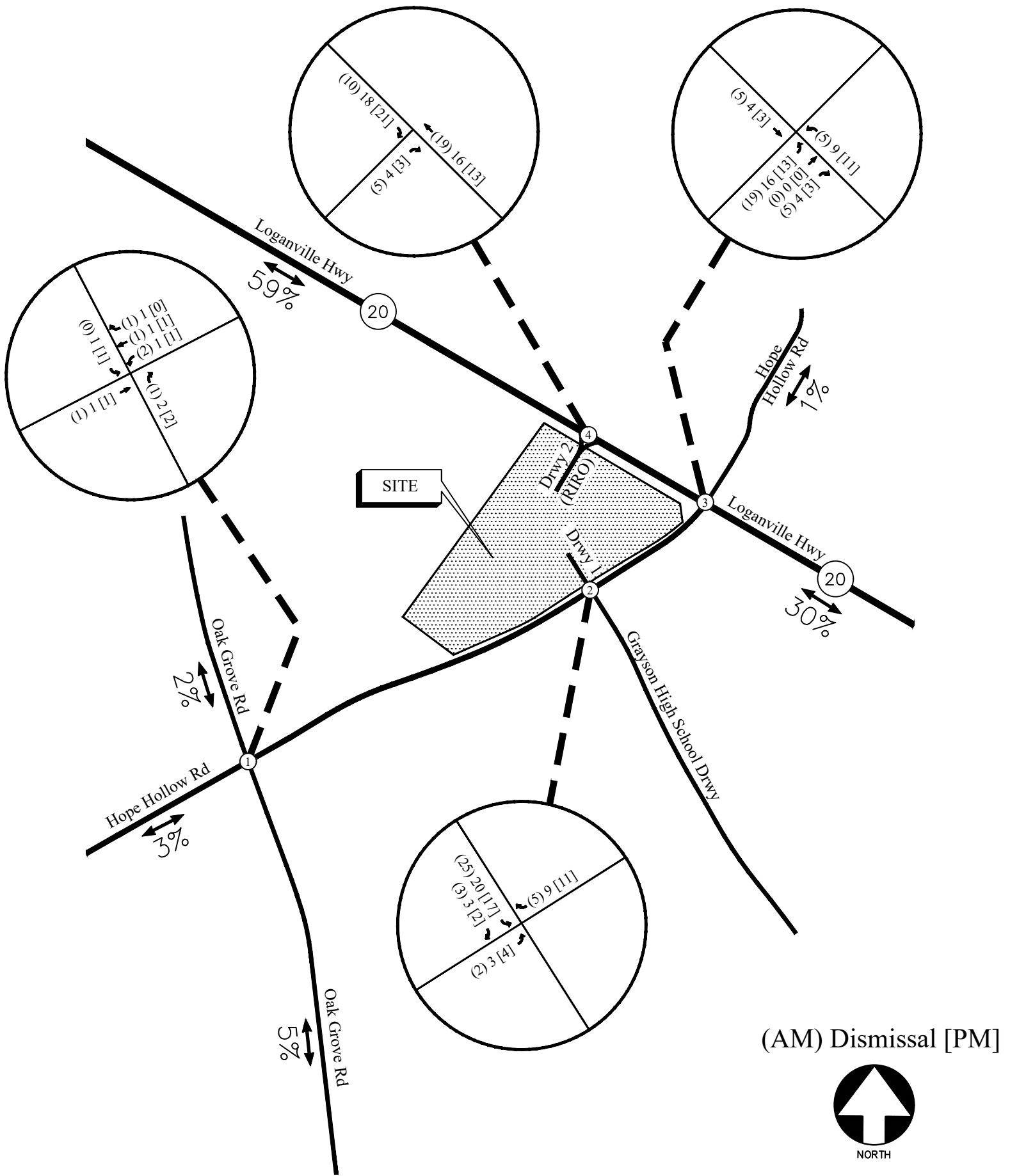
6.1.2 Trip Generation Comparison

A comparative analysis shows that the proposed residential development will generate 82% less trips during the AM peak hour, 72% less trips during the school dismissal peak hour, 76% less trips in the PM peak hour and a significant 81% less trips during the 24-hour period than the existing zoning. See Table 4C below for details.

Land Use	AM Peak Hour			School Dismissal Peak Hour			PM Peak Hour			24 Hour
	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Two-Way
Trip Generation – Existing Zoning	144	133	277	96	107	203	126	114	240	4,129
Trip Generation – Proposed Development	17	33	50	30	27	57	35	22	57	779
Difference (Proposed Development - Existing Zoning)	-127	-83	-210	-71	-80	-151	-85	-84	-169	-3,218
Difference in %	-88%	-75%	-82%	-69%	-75%	-72%	-72%	-81%	-76%	-81%

6.2 Trip Distribution

The trip distribution describes how traffic arrives and departs from the site. An overall trip distribution was developed for the site based on a review of the existing travel patterns in the area and the locations of major roadways and highways that will serve the development. The site-generated peak hour traffic volumes, shown in Table 4A, were assigned to the study area intersections based on this distribution. The outer-leg distribution and AM, School Dismissal and PM peak hour new traffic generated by the site is shown in Figure 5.



TRIP DISTRIBUTION AND NEW SITE-GENERATED
WEEKDAY PEAK HOUR VOLUMES

FIGURE 5
A&R Engineering Inc.

7.0 FUTURE 2027 TRAFFIC ANALYSIS

The future 2027 traffic operations are analyzed for the “Build” and “No-Build” conditions.

7.1 Future “No-Build” Conditions

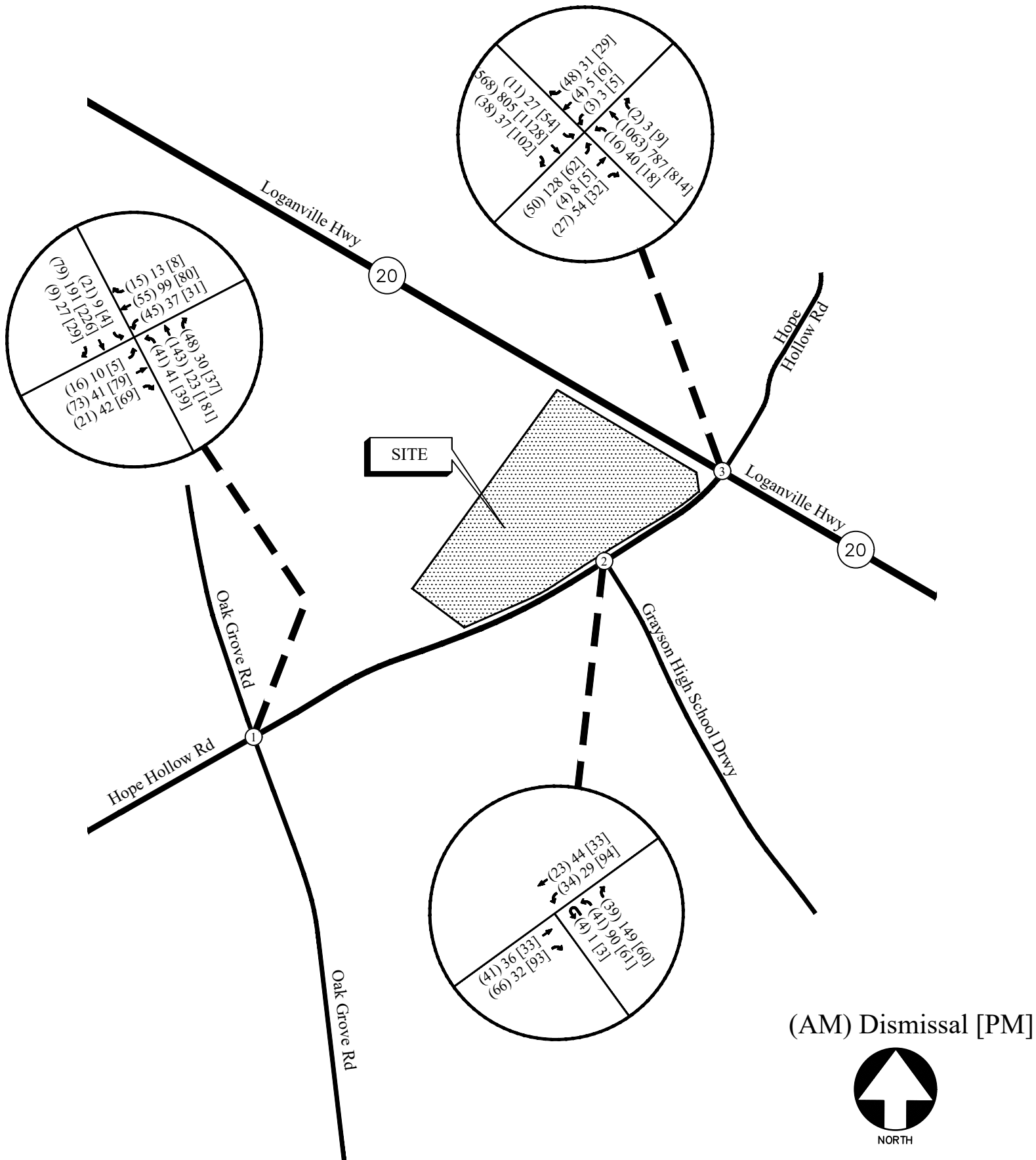
The “No-Build” (or background) conditions provide an assessment of how traffic will operate in the study horizon year without the study site being developed as proposed, with projected increases in through traffic volumes due to normal annual growth. The future “No-Build” volumes consist of the existing traffic volumes (Figure 2) plus increases for annual growth of through traffic.

7.1.1 Annual Traffic Growth

To evaluate future traffic operations in this area, a projection of normal traffic growth was applied to the existing volumes. The Georgia Department of Transportation recorded average daily traffic volumes at several locations in the vicinity of the site. Reviewing the growth over the last three years (2022-2024) revealed a traffic volume increase of approximately 1% in the area. This growth factor was applied to the existing traffic volumes to estimate the future year traffic volumes prior to the addition of site-generated traffic. The resulting future “No-Build” volumes on the roadway are shown in Figure 6.

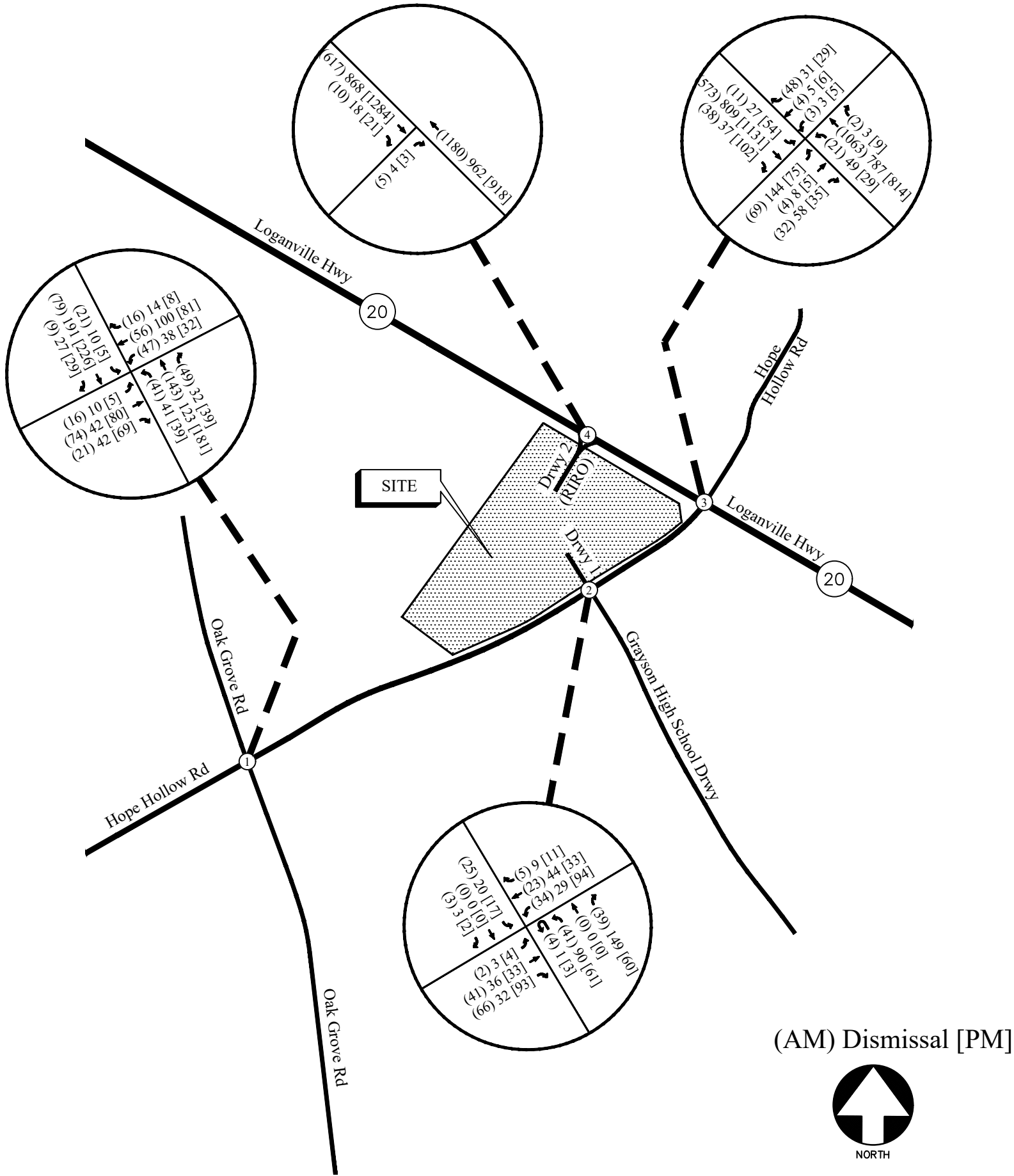
7.2 Future “Build” Conditions

The “Build” or development conditions include the estimated background traffic from the “No-Build” conditions plus the added traffic from the proposed development. To evaluate future traffic operations in this area, the additional traffic volumes from the site (Figure 5) were added to base traffic volumes (Figure 6) to calculate the future traffic volumes after the construction of the development. These total future “Build” traffic volumes are shown in Figure 7.



FUTURE (NO-BUILD) WEEKDAY PEAK HOUR VOLUMES

FIGURE 6
A&R Engineering Inc.



FUTURE (BUILD) WEEKDAY PEAK HOUR VOLUMES

FIGURE 7
A&R Engineering Inc.

7.3 Auxiliary Lane Analysis

Included below are analyses for turn lanes at the site driveways per Gwinnett County standards.

7.3.1 Left Turn Lane Analysis (Gwinnett County)

According to Gwinnett County Criteria and Guidelines for Left Turn Lanes, left turn lanes must be provided at each subdivision street that accesses a minor/major collector street or major thoroughfare if the characteristics of the major street and site density exceed certain thresholds. Per Gwinnett County left turn lane standards (Table 1 – Residential Developments), the minimum threshold for a left turn lane on a 35-mph road with an ADT below 6,000 is 120 lots/units. Since the proposed development will have 139 single-family senior adult houses and will therefore exceed the threshold, a left turn lane is warranted at the site driveway 1.

7.3.2 Deceleration Turn Lane Analysis (Gwinnett County)

According to Gwinnett County Unified Development Ordinance (UDO) Standards, a right turn deceleration lane shall be required at each subdivision street entrance that is provided street access to a Minor/Major Collector Street or Major Thoroughfare (900-30.2). Since Hope Hollow Road is not classified as a collector street or thoroughfare by Gwinnett County, a right turn lane is not warranted at the site driveway 1.

7.3.3 Deceleration Turn Lane Analysis (GDOT)

For a four-lane roadway with an AADT greater than 10,000 vehicles and a posted speed limit of 45 mph, the daily site-generated right turn movements threshold to warrant a deceleration lane is 75 right-turning vehicles per day as per GDOT standards. The projected daily right turn volumes at the site driveway 2 are shown in Table 5.

TABLE 5 – GDOT REQUIREMENTS FOR DECELERATION LANES					
Intersection	Right-turn traffic (% total entering)	Right-turn Volume (vehicles/day)	Roadway Speed/ # of lanes / ADT	GDOT Threshold (vehicles/day)	Warrants met?
SR 20 (Loganville Highway) @ Site Driveway 2 (Right-In/Right-Out)	59% (southbound)	269 (Total Trips) ÷ 2 × 0.59 = (779) ÷ 2 × 0.59 = 230	45 mph / 4-Lane / ≥ 10,000	75	Yes

*Pass-by reductions included, mixed use reductions not included

A right turn lane is warranted at the proposed site driveway 2 (right-in/right-out) per GDOT standards.

7.4 Future Traffic Operations







The future “No-Build” and “Build” traffic operations were analysed using the volumes in Figure 6 and Figure 7, respectively. The results of the future traffic operations analysis are shown below in Table 6. Recommendations on traffic control and lane geometry are shown in Figure 8.

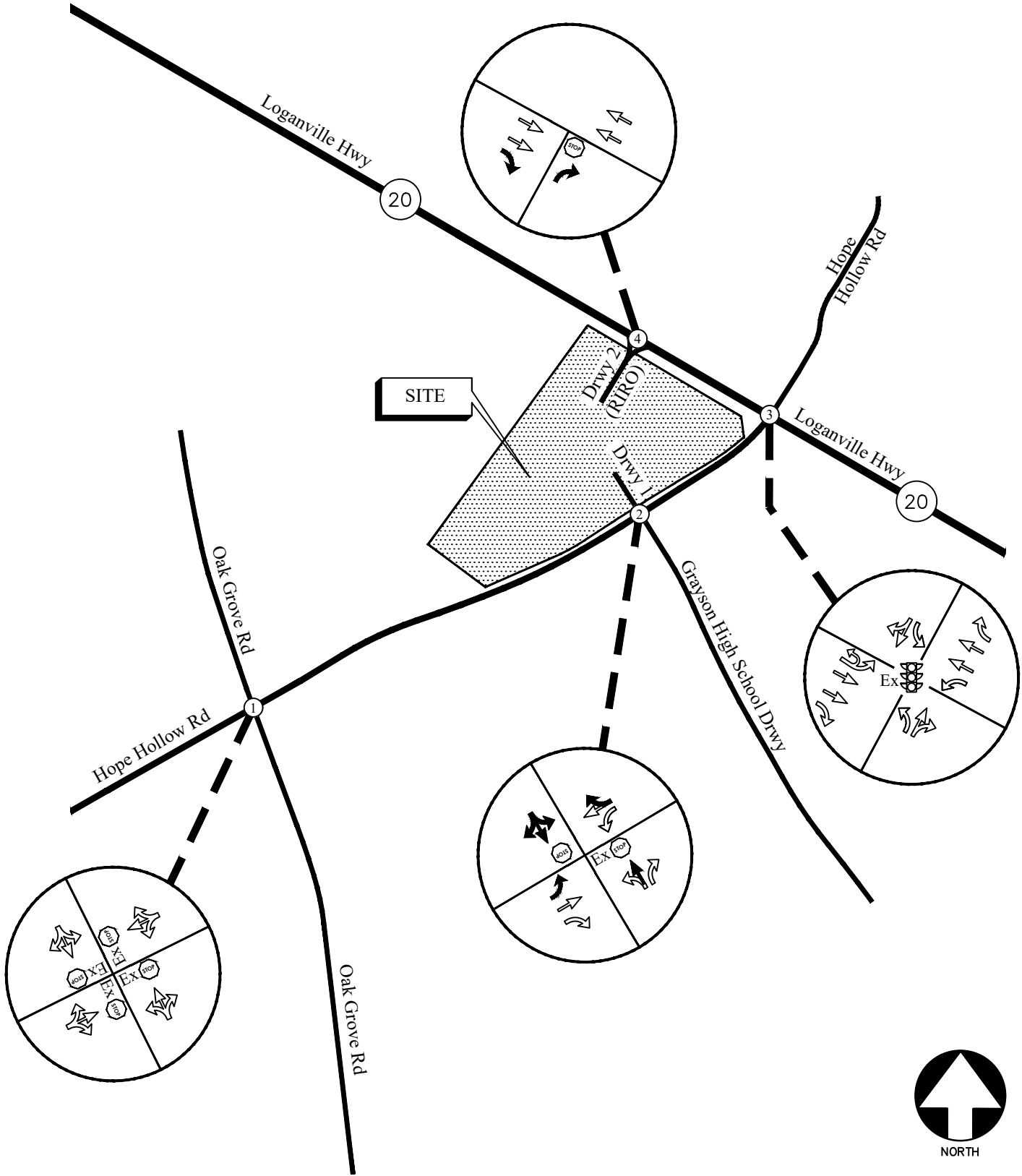
TABLE 6 – FUTURE 2027 INTERSECTION OPERATIONS							
Intersection		Future Condition: LOS (Delay)					
		NO-BUILD			BUILD		
		AM	Dismissal	PM	AM	Dismissal	PM
1	<u>Hope Hollow Road at Oak Grove Road</u>	<u>B (11.5)</u>	<u>B (10.4)</u>	<u>B (11.2)</u>	<u>B (11.6)</u>	<u>B (10.5)</u>	<u>B (11.2)</u>
	-Eastbound Approach	B (10.5)	A (9.3)	B (10.2)	B (10.5)	A (9.3)	B (10.3)
	-Westbound Approach	B (10.7)	B (10.3)	B (10.3)	B (10.8)	B (10.4)	B (10.3)
	-Northbound Approach	B (13.0)	B (10.5)	B (11.6)	B (13.2)	B (10.5)	B (11.7)
	-Southbound Approach	B (10.3)	B (10.9)	B (11.7)	B (10.3)	B (11.0)	B (11.7)
2	<u>Hope Hollow Road at Grayson High School Driveway/Site Driveway 1</u>						
	-Eastbound Left	-	-	-	A (7.3)	A (7.4)	A (7.3)
	-Westbound Left	A (7.5)	A (7.4)	A (7.4)	A (7.5)	A (7.4)	A (7.4)
	-Northbound Approach	B (10.3)	B (10.3)	A (9.9)	B (10.5)	B (10.5)	B (10.1)
	-Southbound Approach	-	-	-	B (12.3)	B (13.5)	B (11.3)
3	<u>SR 20 (Loganville Highway) at Hope Hollow Road</u>	<u>A (8.0)</u>	<u>B (12.5)</u>	<u>A (7.8)</u>	<u>A (9.1)</u>	<u>B (13.4)</u>	<u>A (8.6)</u>
	-Eastbound Approach	D (54.7)	D (51.0)	E (55.3)	D (53.8)	D (50.2)	D (54.7)
	-Westbound Approach	D (52.2)	D (44.5)	D (52.2)	D (49.9)	D (43.1)	D (50.9)
	-Northbound Approach	A (3.9)	A (5.4)	A (2.8)	A (4.5)	A (6.0)	A (3.1)
	-Southbound Approach	A (5.1)	A (9.2)	A (5.9)	A (5.9)	B (10.2)	A (6.7)
4	<u>SR 20 (Loganville Highway) at Right-In/Right-Out Driveway</u>						
	-Eastbound Approach	-	-	-	B (10.5)	B (11.7)	B (14.5)

* Delay exceeds 300 seconds

The results of both the “No-Build” and “Build” traffic operations analyses indicate that the signalized study intersection SR 20 (Loganville Highway) at Hope Hollow Road will continue to operate at an overall satisfactory level of service “B” or better in all the AM, School Dismissal and PM peak hours. The stop-controlled approaches at the unsignalized study intersections will also continue to operate at a level of service “B” or better in all the AM, School Dismissal and PM peak hours. The impact of the site generated traffic on the study network is minimal.

LEGEND

- | | | | |
|--|--------------------------|---|--------------------------|
| Ex  | Existing Signed Approach |  | Proposed Signed Approach |
|  | Existing Lane Geometry |  | Proposed Lane Geometry |
| Ex  | Existing Traffic Signal |  | Proposed Traffic Signal |



FUTURE TRAFFIC CONTROL AND LANE GEOMETRY

**FIGURE 8
A&R Engineering Inc.**

8.0 CONCLUSIONS AND RECOMMENDATIONS

Traffic impacts were evaluated for the proposed residential development that will be located in the northwest corner of the intersection of SR 20 (Loganville Highway) and Hope Hollow Road in Gwinnett County, Georgia. The development will consist of 139 senior adult residential units and proposes one full-access driveway on Hope Hollow Road across from Grayson High School driveway, as well as a right-in/right-out driveway on SR 20 (Loganville Highway).

Existing and future operations after completion of the project were analyzed at the intersections of:

1. Hope Hollow Road at Oak Grove Road
2. Hope Hollow Road at Grayson High School Driveway / Site Driveway 1
3. SR 20 (Loganville Highway) at Hope Hollow Road
4. SR 20 (Loganville Highway) at Site Driveway 2 (Right-In/Right-Out)

The analysis included the evaluation of future operations for “No-Build” and “Build” conditions, with the differences between “No-Build” and “Build” accounting for an increase in traffic due to the proposed development.

The results of both the “No-Build” and “Build” traffic operations analyses indicate that the signalized study intersection SR 20 (Loganville Highway) at Hope Hollow Road will continue to operate at an overall satisfactory level of service “B” or better in all the AM, School Dismissal and PM peak hours. The stop-controlled approaches at the unsignalized study intersections will also continue to operate at a level of service “B” or better in all the AM, School Dismissal and PM peak hours. The impact of the site generated traffic on the study network is minimal.

8.1 Recommendations for Site Access Configuration

The following access configuration is recommended for the proposed site driveway intersections:

- Site Driveway 1: Full Access Driveway on Hope Hollow Road across from Grayson High School Driveway
 - One entering lane and one exiting lanes.
 - Stop-sign controlled on the driveway approach and Grayson High School Driveway with Hope Hollow Road remaining free flow.
 - A left turn lane for entering traffic on Hope Hollow Road.
 - Provide/confirm adequate sight distance per AASHTO standards.
- Site Driveway 2: Right-In/Right-Out Driveway on SR 20 (Loganville Highway)
 - One entering lane and one exiting lanes.
 - Stop-sign controlled on the driveway approach with SR 20 (Loganville Highway) remaining free flow.
 - Deceleration lane for entering traffic on SR 20 (Loganville Highway).
 - Provide/confirm adequate sight distance per AASHTO standards.

Exhibit H: Public Participation Plan and Report

[attached]

Public Participation Plan

- 1. Who do you intend to contact in addition to property owners within 1,000 feet of the site, if any?**

Gwinnett County Planning Commissioners and Board of Commissioners

- 2. Where do you plan to host the Public Participation Meeting? Ensure that the meeting is held in person at a convenient location for interested parties.**

Chestnut Grove Baptist Church

2299 Rosebud Road SW, Grayson, GA 30017

- 3. What date and time do you plan to host the Public Participation Meeting? Ensure that the meeting is held at a convenient time for interested parties, e.g. not during typical work hours on weekdays.**

February 23, 2026 at 6:30

- 4. What is your method for providing opportunities for discussion with interested parties at the Public Participation Meeting?**

The Applicant will make a presentation about the proposed development and then open the floor for questions/comments from attendees.

Public Participation Report

1. **List all groups that you notified of the requested application. Include a copy of the notification package and stamped Postal Service Form 3877 (attached) with itemized name and address of each addressee.**

Please see attached.

2. **Provide the date, time, and location of all meeting(s) that the applicant and/or representative attended to discuss an application with interested parties.**

February 23, 2026 at 6:30 p.m. at Chestnut Grove Baptist Church, 2299 Rosebud Road SW, Grayson, Georgia 30017

3. **Provide the number of people who participated in the meeting(s). Include the sign-in sheet(s) with meeting date, time, location, and attendee names.**

Six (6) people participated in the meeting.

4. **What issues and concerns were expressed by attendees at the meeting(s)?**

The attendees indicated concerns about the effects of the detention pond, the noise from the amenities, how long the development process will take, and whether someone else could come in and develop the property after the rezoning not in accordance with the plan proposed tonight.

5. **What are the applicant's responses to the issues and concerns that were expressed at the meetings? What changes to the development will be made?**

The Applicant team understood the concern about the detention pond location, but explained that based on the topography it has to be placed where it is. Additionally, explained that it will not always be wet but rather is intended to dry as it drains.

The Applicant team understands concerns about noise from the amenities, and explained that these are not heavily used amenities nor are any events or large groups using them, so there should be a negligible amount of noise generated. The Applicant team provided an overview of the development process, and explained that we are looking at 18-24 months build time following receiving an LDP, and the time to receive LDP approval. So, looking at around Jan. 2028 for opening. Lastly, the Applicant team explained that if for some reason One Street did not develop the property following rezoning, Board of Commissioners can impose zoning conditions that will run with the land for the development of the property that would prevent a substantial change in the development of the property from that proposed by One Street. If anyone tried to develop the property, it would have to be in compliance with the zoning conditions and allowed under the zoning classification. Otherwise, the developer would have to go through this process all over again.

Exhibit I: Internal and External Agency Review Comments

[attached]



**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		April 2026	
Department/Agency Name:		Parks & Recreation	
Reviewer Name:		Adanegn Woldemichael	
Reviewer Title:		Construction Manager I	
Reviewer Email Address:		Adanegn.woldemichael@gwinnett county .com	
Case Number:		REZ2026-00015	
Case Address:		3175 LOGANVILLE HWY, LOGANVILLE, GA 30052	
Comments:		<input checked="" type="checkbox"/>	<input type="checkbox"/>
		YES	NO
1			
2			
3			
4			
5			
6			
7			
Recommended Zoning Conditions:		<input checked="" type="checkbox"/>	<input type="checkbox"/>
		YES	NO
1	Provide a 12-foot-wide trail/ side path along Loganville Hwy as per the Gwinnett County Trails Masterplan.		
2			
3			
4			
5			
6			



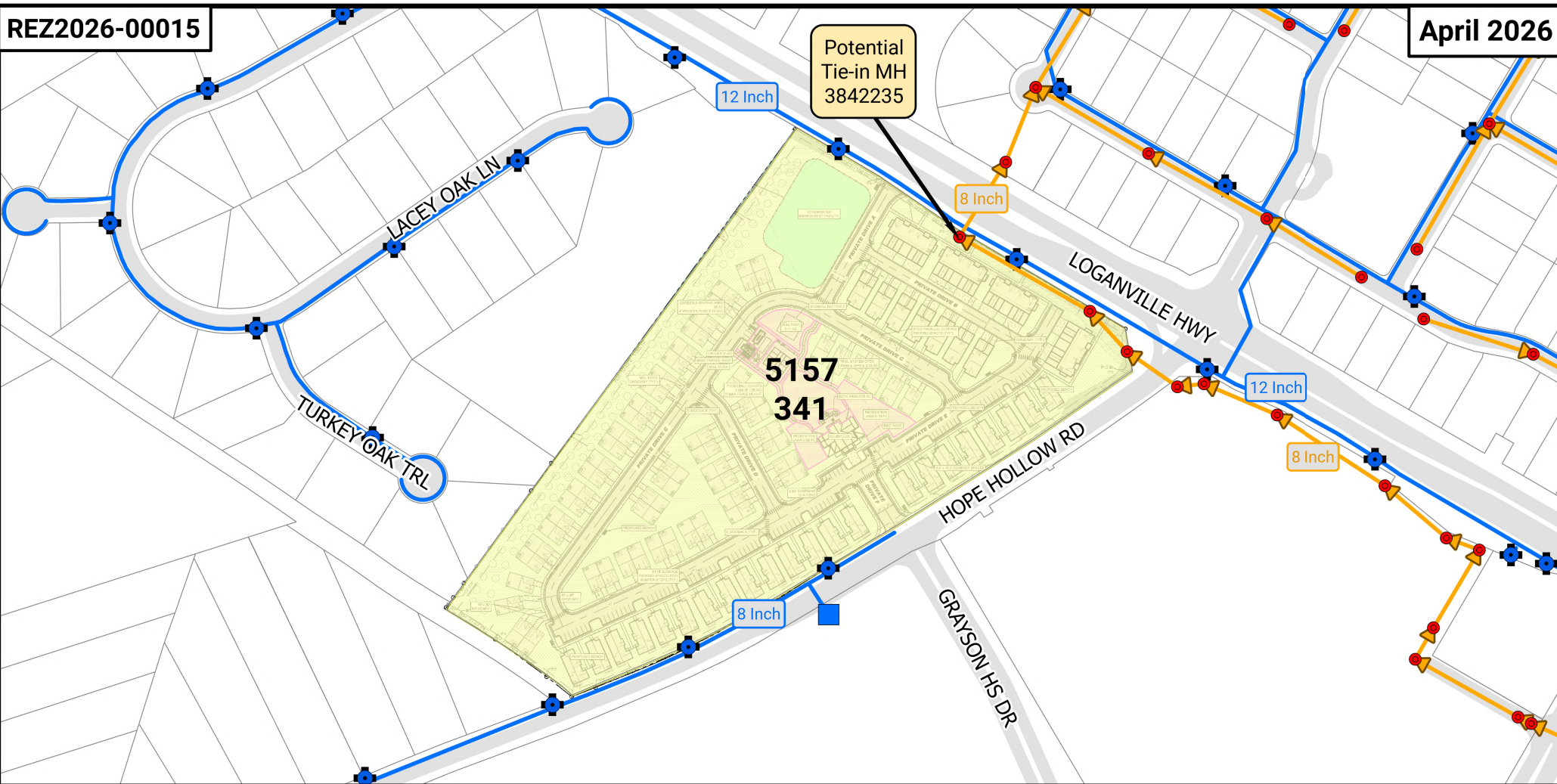
**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		02/20/2026	
Department/Agency Name:		Transportation	
Reviewer Name:		Brent Hodges	
Reviewer Title:		Construction Manager 2	
Reviewer Email Address:		Brent.Hodges@gwinnettcounty.com	
Case Number:		REZ2026-00015	
Case Address:		3175 Loganville Highway, Loganville, 30052	
Comments:		<input checked="" type="checkbox"/> YES	<input type="checkbox"/> NO
1	Loganville Highway (SR 20) is a principal arterial. The Average Daily Traffic (ADT) Volume is 21,500. Hope Hollow Road is a local road. The Average Daily Traffic (ADT) volume is 2,763.		
2	The nearest Ride Gwinnett facility (Stop 4087 / Route 40) is located 6.9 miles away at 368 Grayson Highway (Grayson Highway and VFW #5255).		
3	Per section 360-30 of the UDO as well as the criteria and guidelines for left turn lanes, the developer shall construct an EB left turn lane from Hope Hollow Road, into the site.		
4	Per section 360-100 of the UDO, the developer shall construct a 12' concrete trail along the Loganville Highway site frontage. Per section 360-90 of the UDO, the developer shall construct a 5' concrete sidewalk along the Hope Hollow Road frontage.		
5	The developer shall coordinate with Grayson High School and the Gwinnett County Schools on the proposed site driveway along Hope Hollow Road.		
6			
7			
Recommended Zoning Conditions:		<input type="checkbox"/> YES	<input checked="" type="checkbox"/> NO
1			
2			
3			
4			
5			
6			



**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		
Department/Agency Name:	DWR	
Reviewer Name:	Lorraine Campagne	
Reviewer Title:	Section Manager	
Reviewer Email Address:	Lorraine.Campagne@gwinnettcounty.com	
Case Number:	REZ2026-00015	
Case Address:	3175 Loganville Hwy	
Comments:		<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO
1	Water: The proposed development is required to connect to both an existing 12-inch water main located on the southern right-of-way of Loganville Highway and an existing 8-inch water main located on the northern right-of-way of Hope Hollow Road.	
2	Sewer: A Sewer Capacity Certification (C2025-283-10) was approved in October 2025 for 139 single family/duplex residences. The proposed development plans to connect to an existing 8-inch gravity sewer located on the subject site.	
3		
4		
5		
6		
7		
Recommended Zoning Conditions:		<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO
1		
2		
3		
4		
5		
6		

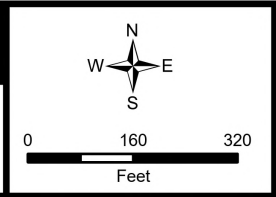


LEGEND

Flow Management	Hydrant	Sewer Collector
Pump Station	City	Sewer Interceptor
Regional	Water Main	Sewer Force Main
Manhole	Proposed Water Main	Proposed Sewer Collector

3175 Loganville Hwy
C-2 to R-IF

Water & Sewer Utility Map



Water Comments: The proposed development is required to connect to both an existing 12-inch water main located on the southern right-of-way of Loganville Highway and an existing 8-inch water main located on the northern right-of-way of Hope Hollow Road.

Sewer Comments: A Sewer Capacity Certification (C2025-283-10) was approved in October 2025 for 139 single family/duplex residences. The proposed development plans to connect to an existing 8-inch gravity sewer located on the subject site.

Water Availability: Water demands imposed by the proposed development may require upsizing or extensions of existing water mains in order to meet Gwinnett County Standards and fire flow demands. Any cost associated with such required improvements will be the responsibility of the development. Current Gwinnett County Standards require a minimum of 12" pipe size for commercial developments and a minimum of 8" pipe size for residential developments. Additionally, connection to a minimum of 12" and 8" mains are required for commercial and residential developments, respectively. It is the responsibility of the developer's engineer to confirm pressure and volumes are available for the development.

Sewer Availability: A Sewer Capacity Certification must be obtained from Gwinnett County to confirm the existing system can serve the development. Sewer demands imposed by the proposed development may require upsizing and/or extensions of existing sewer mains, and/or upsizing of an existing pump station, and/or installation of a new pump station. Any cost associated with such required improvements will be the responsibility of the development. The developer shall provide easements for future sewer connection to all locations designated by Gwinnett County during plan review.

Water and Sewer Design and Construction Requirements: Extensions of the water and/or sanitary sewer systems within the subject development must conform to this department's policies, Gwinnett County's ordinances, and the Water Main and Sanitary Sewer Design and Construction Standards and Specifications, dated April 5th, 2016. Subsequent to design, construction, inspection, and final acceptance of the required utilities, service would then become available under the applicable utility permit rate schedules.

Private Road Developments: Any development with private roads must comply with the Standard Policy Requirement for the Installation of Water and Sanitary Sewer Mains within Private Developments. This policy stipulates minimum easement requirements and location of public mains and appurtenances, among other requirements.

Exhibit J: Maps

[attached]



LACEY OAK LN

LOGANVILLE HWY

BLUE JUNIPER CIR

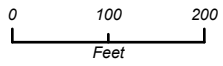
TURKEY OAK TRL

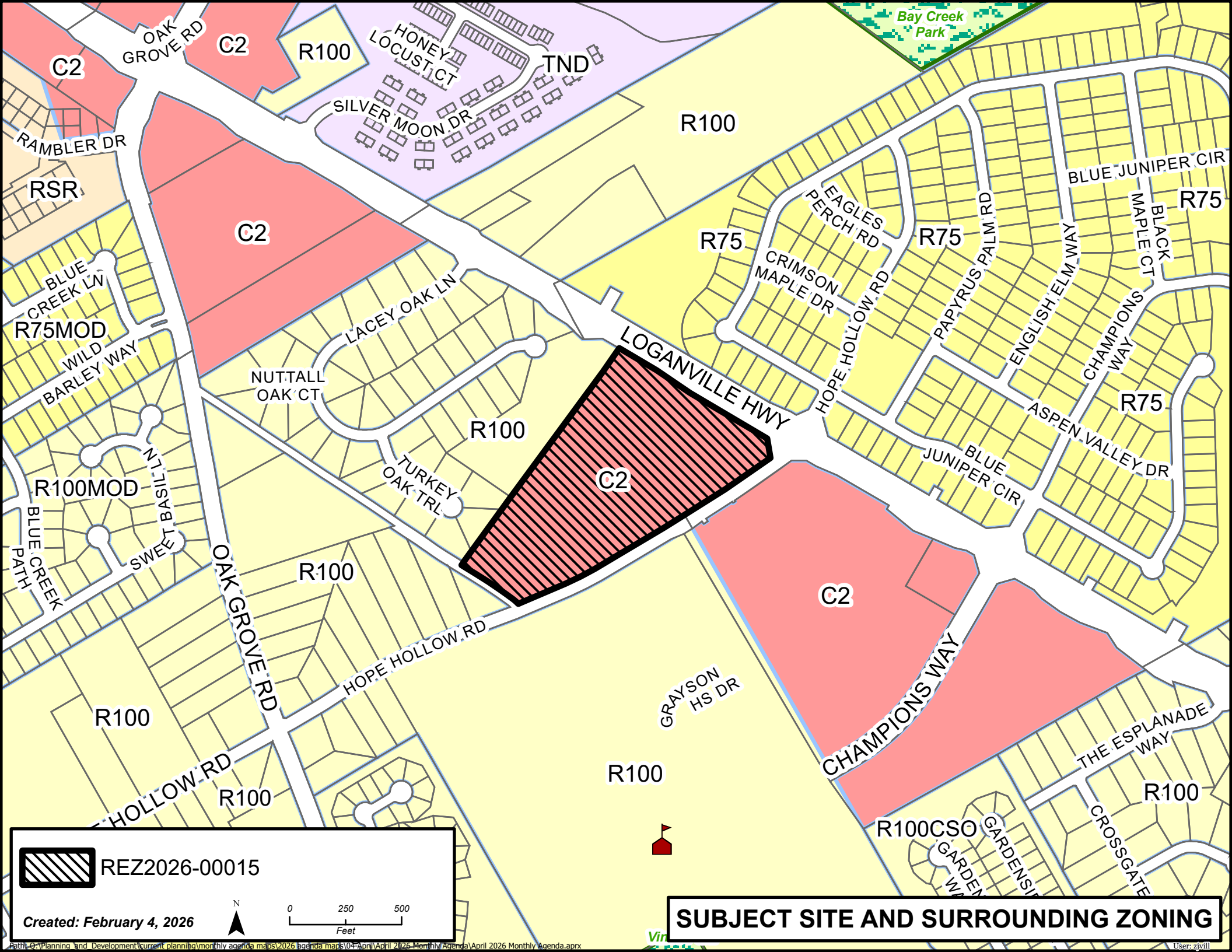
HOPE HOLLOW RD

GRAYSON HS DR

 REZ2026-00015

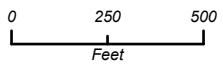
Created: February 3, 2026



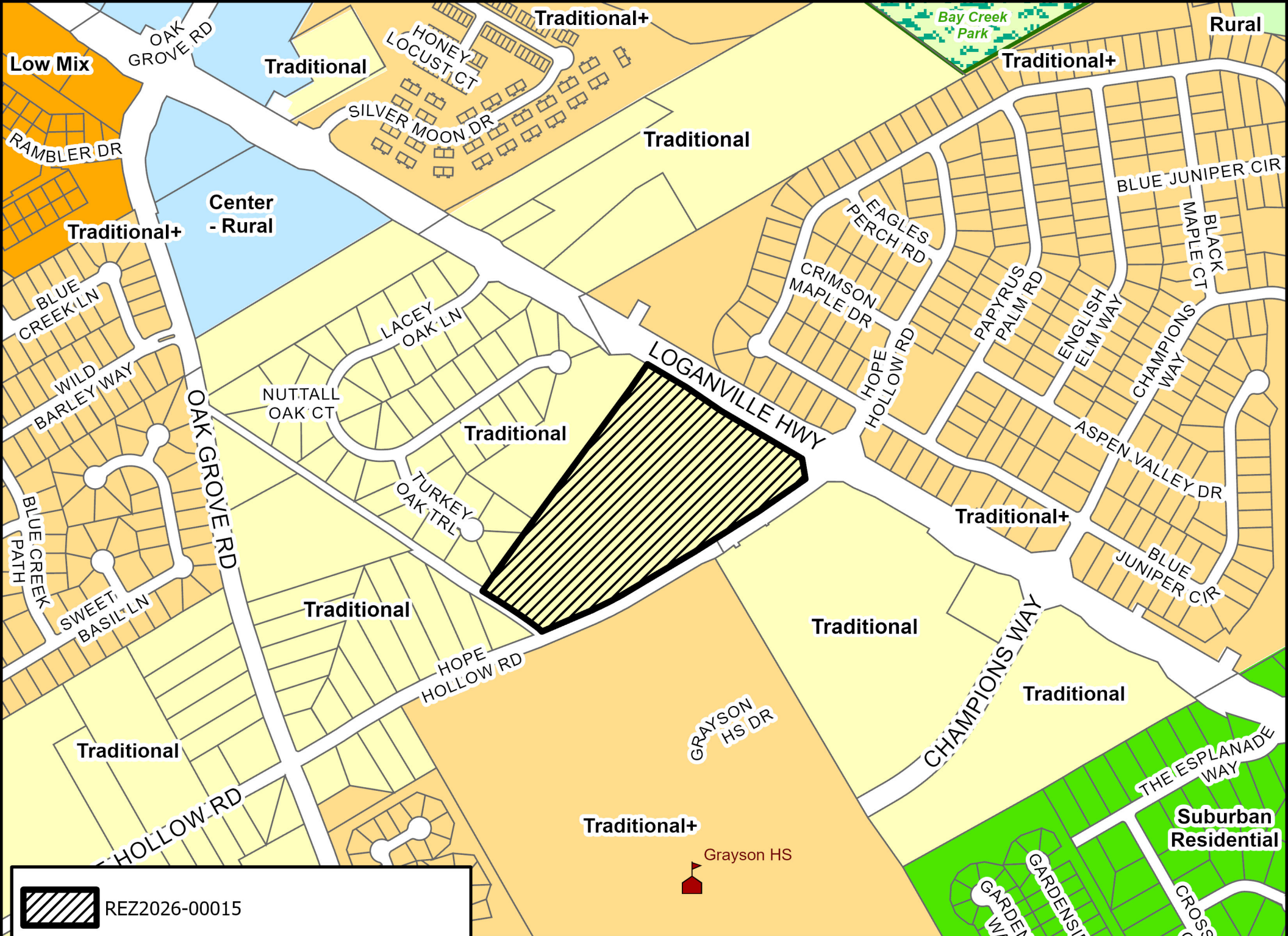


 REZ2026-00015

Created: February 4, 2026

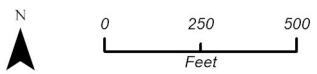


SUBJECT SITE AND SURROUNDING ZONING



 REZ2026-00015

Created: February 3, 2026

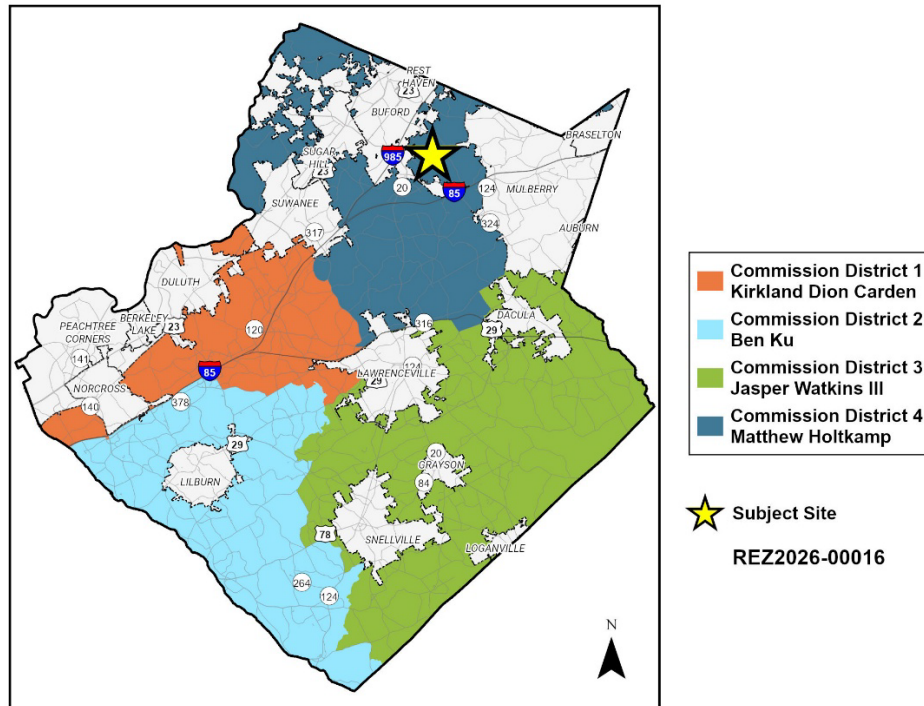


2045 UNIFIED PLAN FUTURE DEVELOPMENT CLASSIFICATION

PLANNING AND DEVELOPMENT DEPARTMENT CASE REPORT

Case Number: REZ2026-00016
Current Zoning: RA-200 (Agricultural-Residence District)
Request: Rezoning to **OSC** (Open Space Conservation District)
Addresses: 2538 and 2570 Kilgore Road
Map Numbers: R7184 005 and 014
Site Area: 30.28
Units: 75
Proposed Development: Single-Family Detached Subdivision
Commission District: District 4 – Commissioner Holtkamp
Future Development Type: Suburban Residential

Staff Recommendation: **APPROVAL WITH CONDITIONS**



Applicant: Simes Signature Properties, Inc.
c/o Mahaffey Pickens Tucker, LLP
1550 North Brown Road, Suite 125
Lawrenceville, GA 30043

Owners: Simes Signature Properties, Inc.
Quintero Land Realty 1, LLC
23 Morgan Street
Buford, GA 30518

James W. Roberts
2538 Kilgore Road
Buford, GA 30519

Contact: Gabrielle Schaller

Phone: 770.232.0000

Zoning History

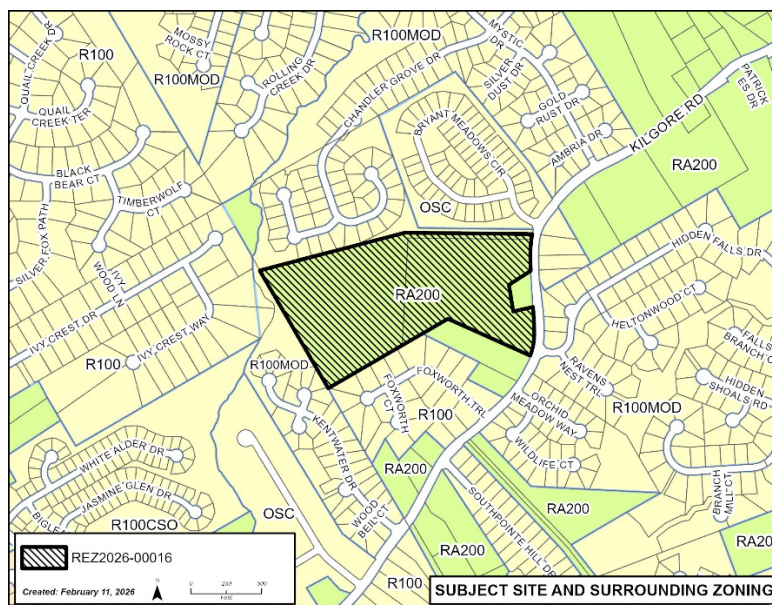
The subject property is zoned RA-200 (Agriculture-Residence District). No prior zoning requests are on record for this property.

Existing Site Condition

The subject property is a 30.28-acre assemblage of two parcels located along Kilgore Road, west of its intersection with Camp Branch Road. The site contains two single-family residences and accessory buildings, as well as dense vegetation, streams, floodplain, and wetlands. There is no sidewalk along the road frontage. The nearest Ride Gwinnett transit stop is approximately 1.8 miles from the subject property.

Surrounding Use and Zoning

The property is surrounded by single-family residences within subdivisions and on large lots. The Bryant Meadows and Chandler Grove subdivisions are to the north. A residence on a large lot is to the east, and the Hidden Falls subdivision and residences on large lots are located farther east, across Kilgore Road. The Foxworth Estates subdivision and residences on large lots are to the south. The Kentwater Manor subdivision is to the west. The following is a summary of surrounding uses and zoning:



Location	Land Use	Zoning	Density
Proposed	Single-Family Residential	OSC	2.48 units per acre
North	Single-Family Residential (Bryant Meadows)	OSC	2.63 units per acre
	Single-Family Residential (Chandler Grove)	R-100 MOD	2.20 units per acre
East	Single-Family Residential	RA-200	1.10 units per acre
	Single-Family Residential	R-100	1.41 units per acre
	Single-Family Residential (Hidden Falls)	R-100 MOD	1.63 units per acre
South	Single-Family Residential	RA-200	0.51 units per acre
	Single-Family Residential (Foxworth Estates)	R-100	1.63 units per acre
West	Single-Family Residential (Kentwater Manor)	R-100 MOD	2.16 units per acre

Project Summary

The applicant requests rezoning from RA-200 to OSC for a single-family detached subdivision, including:

- 75 single-family detached residences on lots ranging from 5,021 to 10,447 square feet, yielding a density of 2.48 units per acre.
- Two-story residences with two-car garages and a minimum heated floor area of 2,400 square feet.
- Exterior building materials of brick and cementitious board and batten siding in the front façade, and cementitious siding on the side and rear façades.
- 16.47 acres of open space, including 4.01 acres of usable open space comprised of pocket parks, community garden, dog park, and a centrally-located grassed area with walking trail, picnic tables, and benches.
- Two stormwater management facilities located in the center and rear of the development.
- One full-access entrance with a deceleration lane and a 5-foot-wide sidewalk along Kilgore Road.

Zoning and Development Standards

The following is a summary of applicable standards from the Unified Development Ordinance (UDO):

Standard	Required	Proposed	Meets Standard?
Project Area	Minimum 10 acres	30.28 acres	YES
Density	Maximum 2.5 units per acre	2.48 units per acre	YES
Lot Size	Minimum 5,000 square feet	Minimum 5,021 square feet	YES
Lot Width	Minimum 50'	Minimum 50'	YES
Lot Coverage	Maximum 70%	Maximum 70%	YES
Front Yard	Minimum 10'	Minimum 10'	YES
Rear Yard	Minimum 20'	Minimum 20'	YES
Side Yard	Minimum 5'	Minimum 5'	YES
Building Height	Maximum 35'	Maximum 35'	YES
Open Space	Minimum 40% or 12.11 acres	54% or 16.47 acres	YES
Usable Open Space	Minimum 10% or 3.03 acres	13% or 4.01 acres	YES

Public Participation

The applicant held a community meeting on November 3, 2025, at the Fairfield Inn in Buford. There were 12 community members in attendance, who raised questions regarding traffic, open space conservation, water/sewer, and proposed residence sizes. The applicant responded to these concerns by referencing the traffic study, open space requirements, and elevations.

Staff Analysis

Rezoning Request Analysis: The standards governing the exercise of zoning power are outlined in Section 270-20.6 of the UDO. Staff makes the following findings related to the zoning requests:

A. Whether the proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

The proposed rezoning would allow a single-family residential subdivision in an area that is characterized by single-family residences within subdivisions and on large lots. The proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

B. Whether the proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property.

The proposed rezoning would allow a residential subdivision that is within the range of densities of surrounding subdivisions. The size and exterior building materials of proposed residences are also compatible with surrounding subdivisions. The proposed rezoning will not adversely affect the existing use or usability of adjacent or nearby property.

C. Whether the property to be affected by the proposed rezoning has a reasonable economic use as currently zoned.

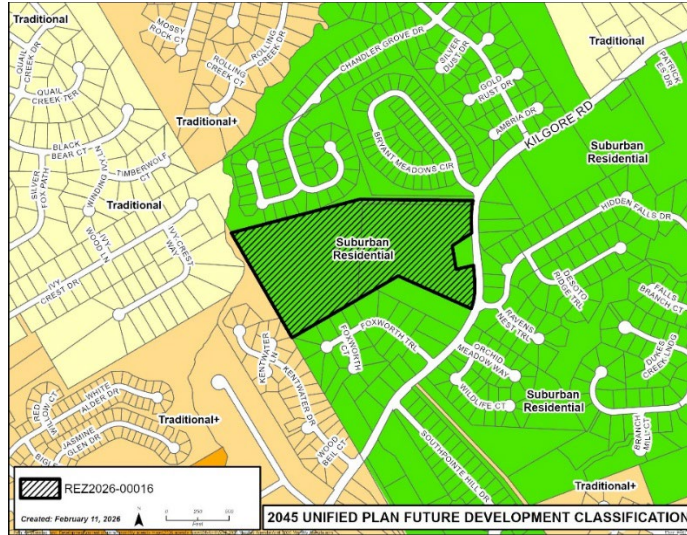
The property has a reasonable economic use as currently zoned.

D. Whether the proposed rezoning will result in a use which will or could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools.

An increase in impacts on public facilities would be anticipated in the form of traffic, utility demand, and stormwater runoff; however, these impacts could be mitigated with appropriate conditions, site development requirements, and planning. The traffic impact study estimates that the development would generate 57 morning and 75 evening peak hour trips. The traffic impact study did not recommend any roadway improvements. An increased impact is anticipated on school enrollment.

E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map; and

The Unified Plan's Future Development Map indicates "Suburban Residential" for the subject property, which envisions single-family detached residential development. Single-family residential is a recommended use and OSC is a recommended zoning district. The proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map.



F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning.

There are no other conditions giving support for approval or disapproval of the proposal.

Staff Recommendation

Based on the staff’s evaluation of the request and the standards governing the exercise of zoning power, the Department of Planning and Development recommends **APPROVAL WITH CONDITIONS** of the rezoning request.

Staff Recommended Conditions

Approval as OSC (Open Space Conservation Subdivision) for a single-family detached subdivision, subject to the following conditions:

1. The development shall be limited to single-family detached dwellings and accessory uses and structures, not exceeding 75 units.
2. The proposed development shall be constructed in general conformance with Exhibit B: Site Plan dated received March 3, 2026, and Exhibit D: Building Elevations dated received January 9, 2026, with revisions required by zoning conditions and the Unified Development Ordinance, subject to the review and approval of the Department of Planning and Development.
3. The minimum heated floor area shall be 2,400 square feet.
4. Usable open space shall be provided in general conformance with Exhibit B: Site Plan dated received March 3, 2026, including, but not limited to, pocket parks, community gardens, dog park, and a centrally-located grassed area with walking trail, picnic tables, and benches, subject to the review and approval of the Department of Planning and Development.
5. A full-access entrance, deceleration lane, and a 5-foot-wide sidewalk shall be provided along Kilgore Road, prior to the issuance of a Certificate of Occupancy, subject to the review and approval of the Gwinnett County Department of Transportation.

Exhibits:

- A. Site Visit Photos
- B. Site Plan
- C. Existing Features and Site Analysis Plan
- D. Building Elevations
- E. Letter of Intent and Applicant's Response to Standards
- F. Application and Disclosure of Campaign Contributions
- G. DWR Sewer Capacity Certification Letter
- H. Traffic Impact Study
- I. Public Participation Plan and Report
- J. Internal and External Agency Review Comments
- K. Maps

Exhibit A: Site Visit Photos



View of site from Kilgore Road



View of existing residence on site



View of utility lines and driveway on site



View of Kilgore Road, site on left



View of residence surrounded by site



View of residences in Chandler Grove



View of residences in Bryant Meadows



View of residences in Hidden Falls



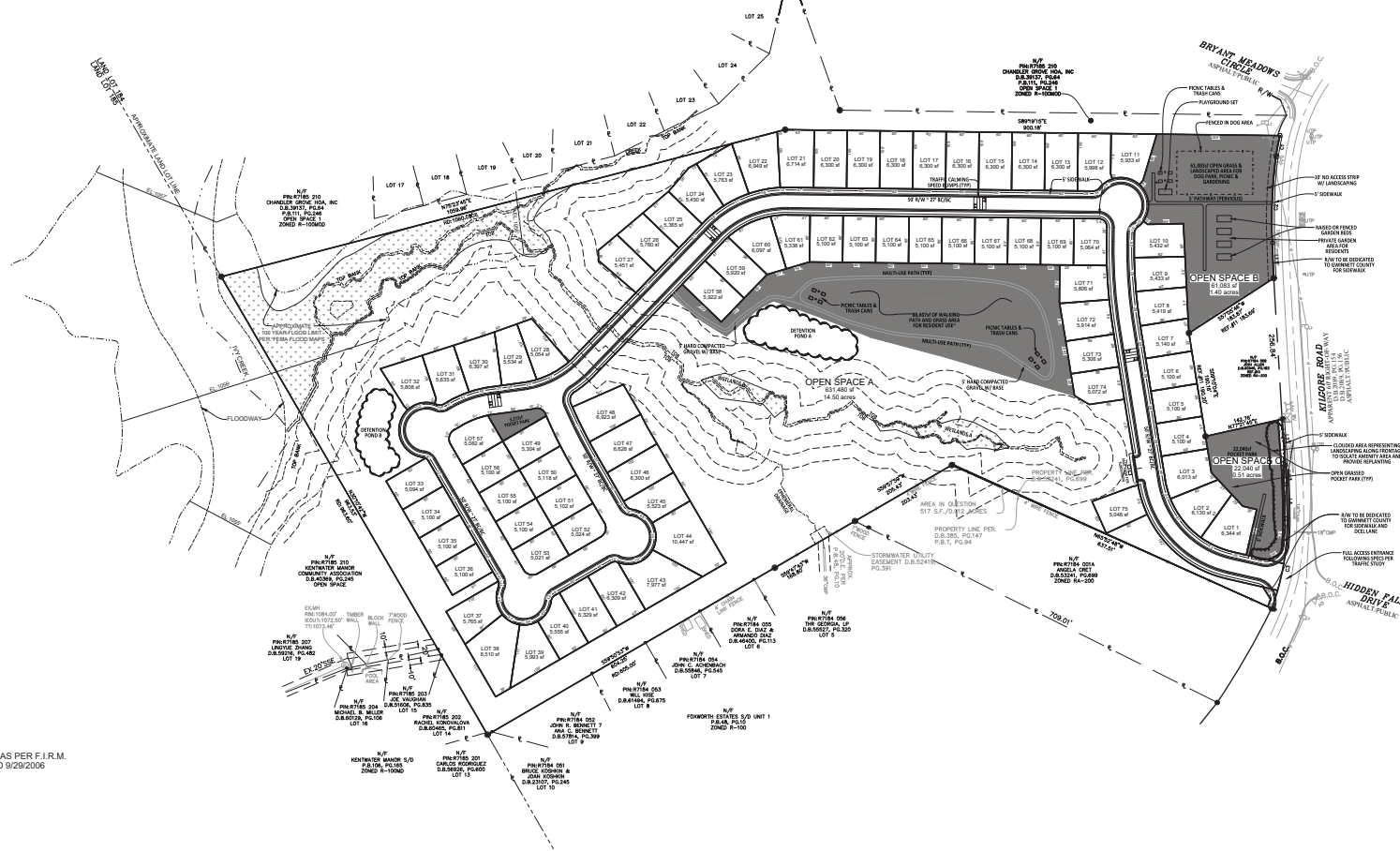
View of residences in Foxworth Estates



View of residences in Kentwater Manor

Exhibit B: Site Plan

[attached]



FLOOD NOTE:
 FLOOD HAZARD AREA DOES EXIST ON SITE AS PER F.I.R.M. COMMUNITY PANEL No. 13135C0026F, DATED 9/29/2006

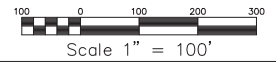
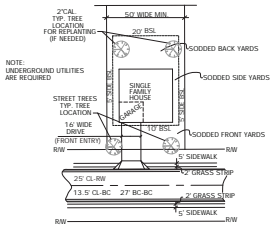
BUILDING SETBACKS:
 FRONT: 10' FROM R/W OF ROADS
 SIDE: 5' FROM PROPERTY LINES
 REAR: 20' FROM PROPERTY LINES

OPEN SPACE
 -MINIMUM 40% OPEN SPACE REQUIRED (30,282'0.40 = 12.11AC)
 -TOTAL PROVIDED OPEN SPACE: 16.47AC (64.39%)
 -TOTAL REQUIRED USABLE OPEN SPACE: 3.03AC (10.0%)
 -TOTAL PROVIDED USABLE OPEN SPACE: 4.01AC (13.24%)
 -HATCHED AREA REPRESENTS USABLE OPEN SPACES

GENERAL NOTES
 -DOMESTIC WATER SUPPLY TO BE GWINNETT COUNTY
 -SANITARY SEWER TO BE GWINNETT COUNTY
 -CURRENT ZONING: RA200
 -TO BE REZONED: OSC
 -NEW PUBLIC ROADWAY: 3.128 LF (3.82AC PROPOSED RAW)
 -MINIMUM 50' LOT WIDTH
 -ALL SIDEWALKS TO BE 5' CONCRETE
 -ALL MULTIPATHS TO BE 5' CONCRETE

BOUNDARY SURVEY
 -BOUNDARY REFERENCED FROM PLAT PREPARED BY LAND DEVELOPMENT SURVEYORS, DATED 4/16/2025 (PB 167 PG 270)

DENSITY
 -SITE ACREAGE: 30.282
 -TOTAL UNITS: 75
 -PROPOSED DENSITY: 2.477 UNITS/AC
 -MAXIMUM DENSITY: 2.5 UNITS/AC



GWINNETT COUNTY
 PLANNING AND DEVELOPMENT
RECEIVED
 3.3.2026

<p>REVISION:</p> <p>DATE:</p> <p>ALL UTILITY CONNECTIONS AND REWORKING ON THIS PROJECT SHALL BE BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING ALL UTILITY OWNERS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL UTILITY DEPT'S. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL UTILITY DEPT'S. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL UTILITY DEPT'S. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL UTILITY DEPT'S. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL UTILITY DEPT'S.</p>	<p>REZONE PLAN FOR: KILGORE ESTATES LOCATED IN: PARCELS 7186074 & 7186085 GWINNETT COUNTY, GEORGIA</p>
<p>Sullins Engineering, LLC CIVIL ENGINEERS - LAND PLANNERS</p>	
<p>DATE: 3/3/2026</p> <p>JOB #: 3559</p> <p>SHEET 1</p>	

Exhibit C: Existing Features and Site Analysis Plan

[attached]

*****BOTH OWNERS CERTIFY THERE HAS BEEN NO TIMBER HARVESTING ON THESE PROPERTIES IN THE PREVIOUS 24 MONTHS.*****

CONSERVATION AREA

BEGIN PERENNIAL STREAM STREAM 5

WETLAND B (0.12 ACRE)

UPPER LIMIT INTERMITTENT STREAM STREAM 4

WETLAND C (1.37 ACRE)

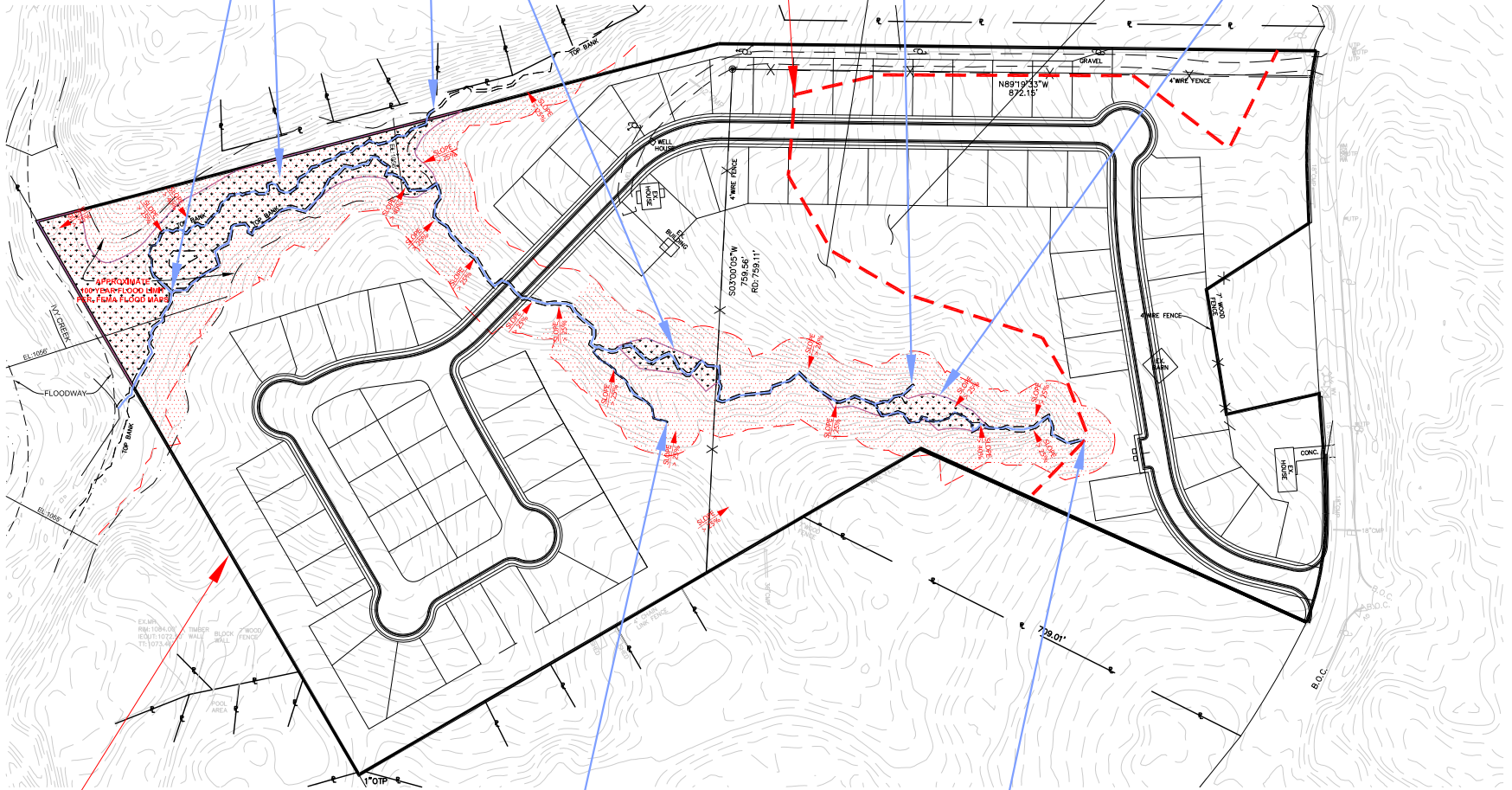
VEGETATION BOUNDARY

WOODED AREAS – HARDWOOD SPECIES (ACER RUBRUM, LIRIODENDRON TULIPIFERA, LIQUIDAMBAR STYRACIFLUA, QUERCUS NIGRA) AND PINES (PINUS TAEDA).

OPEN AREAS – VEGETATED WITH CYNODON DACTYLON AND FESTUCA ARUNDINACEA

UPPER LIMIT INTERMITTENT STREAM STREAM 2

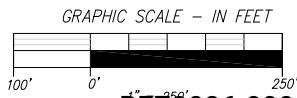
WETLAND A (0.15 ACRE)



PROPERTY BOUNDARY

UPPER LIMIT INTERMITTENT STREAM STREAM 3

UPPER LIMIT INTERMITTENT STREAM STREAM 1



REZ2026-00016

Kilgore Road Tracts
Existing Features Map

DRAWN BY	AJT
CHECKED BY	AJT

Tallman Environmental, Inc.

2095 Highway 211 NW 2F #116
Brasellon, Georgia 30517
(678) 468-2288
Tallman.net

Tallman Environmental

December 7, 2025

Meridith Simes and Cameron Henderson
469.664.5126
msimes@hillwoodrealty.com
cameron@atlaslanddev.com

Subject: Existing Features Site Analysis
2538 and 2570 Kilgore Road
Parcel ID R7184 005 and 014 (30.4 acres)
Buford, Gwinnett County, Georgia

Meridith and Cameron:

On December 4, 2025, Tallman Environmental completed a site reconnaissance to assess the subject property for existing features of natural, ecological, and cultural significance. The 30.59-acre subject property is located at 2538 and 2570 Kilgore Road in Buford, Gwinnett County, Georgia. The property is centered at latitude 34.086083° north and longitude -83.966849° west and is identified by Gwinnett County parcel numbers R7184 005 and 014. At the time of the December 4, 2025 site reconnaissance, the subject property was comprised of vacant residential parcels, with open land and forestland. Surrounding areas included residential subdivisions and single-family residential properties.

The subject property is located in the Piedmont Ecoregion of Georgia. Community types include open land (26.3%), Piedmont pine-oak woodlands (71.6%), and floodplain forests (2.1%) (*Guide to the Natural Communities of Georgia*, 2013). Areas of open land were vegetated with *Cynodon dactylon* and *Festuca arundinacea*. Woodlands and forested areas consisted of *Acer rubrum*, *Liriodendron tulipifera*, *Liquidambar styraciflua*, *Quercus alba*, *Quercus montana*, *Quercus falcata*, and *Pinus taeda*. Elevations on the subject property range from 1,040 feet to 1,160 feet and the subject property generally slopes centrally towards onsite, unnamed tributaries to Ivy Creek. No rock outcroppings were observed on the subject property.

The subject property is located within the Upper Chattahoochee watershed (Hydrologic Unit Code 03130001). The northwest portion of the subject property is mapped within an area designated as Zone AE (Special Flood Hazard Areas) according to the FEMA flood insurance rate map (Panel Number 13135C 0026F, dated September 29, 2006).

Farmland classification identifies soil map units as prime farmland, farmland of statewide importance, farmland of local importance, or unique farmland. It identifies the location and extent of the soils that are best suited to food, feed, fiber, forage, and oilseed crops. A large portion of the subject property is mapped with prime farmland soils. However, because the subject property is located in an urban area (based on the 2020 Census Bureau *Urban Areas of the United States and Puerto Rico* map), it is exempt from the Farmland Protection Policy Act.

2095 Highway 211 NW 2F #116, Braselton, Georgia 30517
call or text 24/7 at 678.468.2288 aj@tallmans.net

Tallman Environmental

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
AmC2	Appling sandy loam, 6 to 10 percent slopes, moderately eroded	10.0	33.2%
ApB	Appling-Hard Labor complex, 2 to 6 percent slopes	1.3	4.3%
ARE	Ashlar, Rion, and Wateree soils, 10 to 25 percent slopes	0.1	0.4%
AsF	Ashlar-Wateree complex, 15 to 45 percent slopes, stony	0.2	0.7%
Cfs	Chewacla silt loam, 0 to 2 percent slopes, frequently flooded	5.0	16.5%
PfC2	Pacolet sandy loam, 6 to 10 percent slopes, moderately eroded	0.0	0.1%
ToA	Toccoa fine sandy loam, 0 to 4 percent slopes, frequently flooded	0.0	0.1%
WrE2	Wedowee sandy loam, 10 to 25 percent slopes, eroded	13.5	44.7%
Totals for Area of Interest		30.2	100.0%

On July 10, 2025, the subject property was investigated for the presence of streams and other hydrologic features that might be considered state waters. Indicators used to identify jurisdictional streams included the following: wretched vegetation within the channel, sediment sorting, mean high water mark, evidence of recent persistent flow, and/or evidence of subsurface hydrology. Hydrologic features were also evaluated using the North Carolina, Division of Water Quality Methodology for Identification of Intermittent and Perennial Streams and Their Origins (Version 4.11, September 2010). At the time of the July 10, 2025 site reconnaissance, unnamed tributaries to Ivy Creek were observed flowing on the northwestern and central portions of the subject property.

A preliminary assessment of wetlands and other jurisdictional waters of the U.S. was also conducted on the property on July 10, 2025. Three areas meeting the hydrology, vegetation, and soil criteria were identified at the time of the site reconnaissance. Forested wetlands [PFOIA, Palustrine (P), Forested (FO), Broad-leaved Deciduous (1), Temporarily Flooded (A)] and riverine waters [R2UBH, Riverine (R), lower perennial/well developed floodplain (2), Unconsolidated Bottom (UB), permanently flooded (H)] were depicted on the National Wetland Inventory map for the subject property.

An Information for Planning and Conservation (IPaC) report was obtained from the U.S. Fish and Wildlife Service (USFWS) and is included as an attachment to this letter. The purpose of the IPaC report is to identify species that may occur or could potentially occur on the subject property.

The subject property is not known to support any populations of *Danaus plexippus* (Monarch Butterfly) or *Grus americana* (Whooping Crane). The Monarch Butterfly is a candidate species that relies on a milkweed host plant, which was not observed on the subject property at the time of the reconnaissance. The Whooping Crane's wintering range includes portions of Georgia but there is no evidence of use of the subject property by migrating Whooping Cranes, which are included in the IPaC listing as a non-essential experimental population.

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call or text 24/7 at 678.468.2288 aj@tallmans.net

Tallman Environmental

Perimyotis subflavus (Tricolored Bat) is proposed to be listed as endangered by the USFWS. Based on conversations with USFWS biologists, the Tricolored Bat occurs statewide in Georgia and uses forested landscapes during its "active" seasons of spring, summer, and fall. They roost in leaf foliage and nearly all forested areas could be considered suitable habitat. They can also be found in transportation structures, abandoned buildings, and military bunkers. In the winter, Tricolored Bats hibernate in caves and mines where these habitats are available, and culverts across most of Georgia. They also overwinter in bridges, but use of bridges is expected to a lesser extent than use of culverts. In bottomland hardwood forests, Tricolored Bats have been documented in tree hollows. In much of Georgia, winters are mild enough that Tricolored Bats are likely active on the landscape to some extent year-round.

To avoid any potential impact to the Tricolored Bat, we suggest implementing seasonal tree clearing restrictions during the non-volant (unable to fly) pup season (May 1 to July 31) as well as winter hibernation season (December 1 to February 28) in most Georgia counties. At the writing of this report, the Tricolored Bat has not been listed as endangered.

According to the Georgia Biodiversity Portal, *Myotis grisescens* (Gray Bat) is likely the most cave-dependent mammal in the US. It roosts and hibernates almost exclusively in caves in the southeast (though some specimens were recently collected in a drainage tunnel under the University of Georgia football stadium). Habitat requirements are specific - less than 5% of available caves in the southeast have temperature, humidity, and structure suitable for gray bat occupation. Wintering caves are deep and vertical and serve as cold air reservoirs. Summer caves are almost always located within a half mile of a river or reservoir that serves as a foraging site. The bats will forage 12 miles or more from the roost sites and seem to prefer traveling within forested areas, probably because they are less vulnerable to predation from owls there. Young bats just learning to fly need forest cover in the vicinity of the maternity cave in which to forage and take shelter. In Georgia, gray bats are known to occupy only three caves regularly during the summer in Chattooga, Walker, and Catoosa counties. There are no known cave sites on the subject property.

A critical habitat is a federally designated geographic area that contains features essential for the conservation of a threatened or endangered species and that may require special management and protection. Critical habitat may include an area that is not currently occupied by the species but that will be needed for its recovery. The IPaC report did not identify critical habitats on the subject property.

The subject property was also evaluated for migratory bird use. The subject property did not appear to serve as a breeding area for migratory birds during the July or the December 2025 reconnaissance. However, the subject property may require reevaluation for migratory birds if construction is delayed.

A National Wildlife Refuge is a federally designated, protected area of the United States managed by the United States Fish and Wildlife Service. The National Wildlife Refuge System is the system of public lands and waters set aside to conserve America's fish, wildlife, and plants. Gwinnett County tax assessor records indicate private ownership of the subject property.

The Georgia Department of Natural Resources' Wildlife Resources Division (WRD) maintains records for rare species and communities in conservation databases. WRD tracks species and natural communities that are protected at state and federal levels or not legally protected but considered of special conservation concern by WRD biologists.

Tallman Environmental

According to the Georgia Department of Natural Resources 2015 State Wildlife Action Plan:

Rarity ranks are used to characterize elements and to facilitate conservation planning. These ranks are assigned after reviewing pertinent status information at the state level and globally. Rarity ranks are based on a scale of 1 to 5; the higher the number, the more secure that species is thought to be at the state (or global) level. Therefore, an S1 species is considered very rare or imperiled in the state, while an S5 species is considered common and secure. A species with a rarity rank of G5 S1 is globally secure but occurs in very small numbers in the state. Thus (sic) it is not of global conservation concern but may be considered a priority for conservation within the state, depending on other factors. This ranking system helps to assure that conservation efforts are directed to those species needing the most help in order to maintain biological diversity in a state or region.

No animal species were identified in the conservation database. One plant species was identified in the conservation database:

Cypripedium acaule, Pink Lady's-slipper

State Rank: S4 (Apparently Secure - uncommon but not rare; some cause for long-term concern due to declines or other factors)

GA: Unusual

US: No Federal Protection

State Wildlife Action Plan: Yes

Element Occurrences: 229

Habitat: Upland oak-hickory-pine forests; piney woods

The subject property was reconnoitered for the presence of Pink Lady's-slipper; no plants were observed at the time of the reconnaissance. Please refer to the attached photographs documenting conditions at the time of the site reconnaissance.

Tallman Environmental reviewed the Georgia Natural, Archaeological, and Historic Resource Geographic Information System (GNAHRGIS) database for records pertaining to surveyed historic structures listed on or eligible for listing on the National Register of Historic Places (NRHP). An Area of Potential Effect (APE) of ¼ mile (1,320 feet) from the subject property was established for the purposes of this review. Based on review of the database records, no NRHP-listed structures were mapped on the subject property. A residence (Resource 201036) was mapped at 2580 Kilgore Road, approximately 950 feet north-northeast the subject property. National Register Eligibility was not determined; the residence is no longer standing, having been developed into a single-family residential subdivision. No artifacts or lithic scatters were observed on the subject property at the time of the reconnaissance; no subsurface investigation was conducted.

R.S. Webb & Associates (RSWA) was contracted to conduct a cultural resources literature review for the subject property in November 2025. According to the report dated December 4, 2025 (R.S. Webb & Associates Project No. 25-827-011):

Based on the information reviewed during the current literature search, there are no NRHP-listed properties, recorded archeological sites, recorded historic resources or mapped cemeteries located within the project area. Historic maps and aerial photographs indicate agricultural use of the project area through the middle 20th century, but referenced buildings located in or adjacent to the project area date to the late 20th century.

2095 Highway 211 NW 2F #116, Braselton, Georgia 30517
call or text 24/7 at 678.468.2288 aj@tallmans.net

Tallman Environmental

Please refer to the RSWA report, included as an attachment, for methodology and investigations leading to these findings.

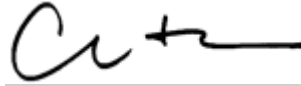
Please feel free to contact me if you have any questions or concerns. We thank you for the opportunity to coordinate with you on this project.

Sincerely,

TALLMAN ENVIRONMENTAL



AJ Tallman
Project Manager



Connie Tallman
Biologist

Attachments:

Existing Features Analysis Map
Vicinity Map
Topographic Map
Soils Map
FEMA FIRM Map
National Wetland Inventory Map Site
Photographs
IPaC Trust Resource Report
Georgia Rare Natural Elements Listing
GNAHRGIS Database Map
Cultural Resources Literature Review
Waters Delineation Letter

WOODED AREAS - VEGETATED WITH *ACER RUBRUM*,
LIRIODENDRON TULIPIFERA, *LIQUIDAMBAR*
STYRACIFLUA, *QUERCUS ALBA*, *QUERCUS*
MONTANA, *QUERCUS FALCATA*, *PINUS TAEDA*

OPEN AREAS - VEGETATED WITH
CYNODON DACTYLON AND *FESTUCA*
ARUNDINACEA

BEGIN
 PERENNIAL STREAM
 STREAM 5

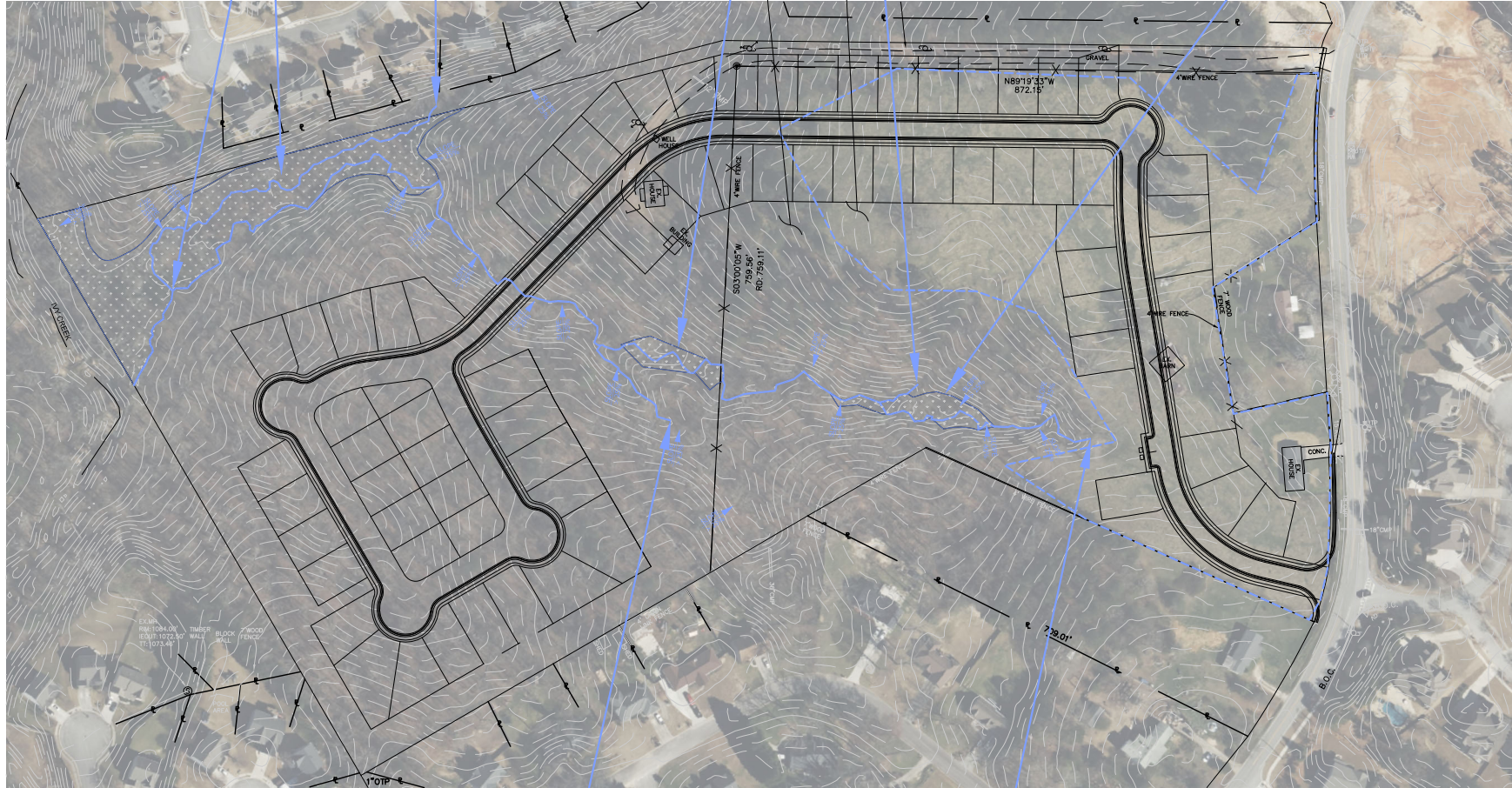
WETLAND C
 (1.37 ACRE)

UPPER LIMIT
 INTERMITTENT STREAM
 STREAM 4

WETLAND B
 (0.12 ACRE)

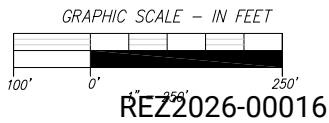
UPPER LIMIT
 INTERMITTENT STREAM
 STREAM 2

WETLAND A
 (0.15 ACRE)



UPPER LIMIT
 INTERMITTENT STREAM
 STREAM 3

UPPER LIMIT
 INTERMITTENT STREAM
 STREAM 1



Kilgore Road Tracts
 Existing Features Analysis Map

DRAWN BY	AJT
CHECKED BY	AJT

Tallman Environmental, Inc.
 2095 Highway 211 NW 2F #116
 Braselton, Georgia 30517
 (678) 468-2288
 Tallman.net

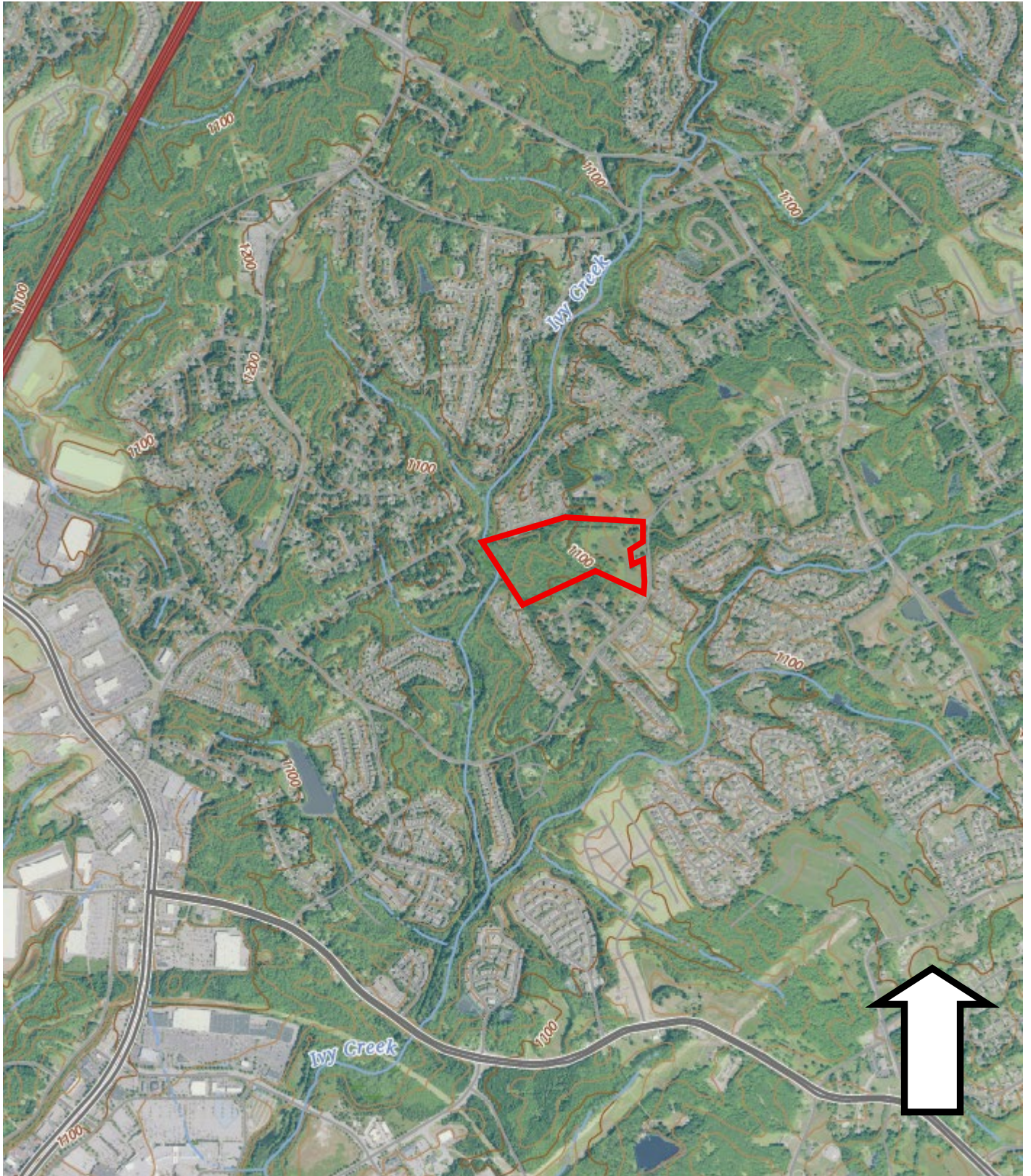


SOURCE: Google Earth

Figure 1
Scale: 1"=250'

Site Location Map
2538 and 2570 Kilgore Road
Buford, Gwinnett County, Georgia

Tallman Environmental, Inc.
2095 Highway 211 NW 2F #116
Braselton, Georgia 30517
aj@tallmans.net
678.468.2288



SOURCE: Hog Mountain, Georgia 7.5-minute topographic quadrangle map (2024)

Figure 2
Scale: 1"=2,000'

Topographic Map
2538 and 2570 Kilgore Road
Buford, Gwinnett County, Georgia

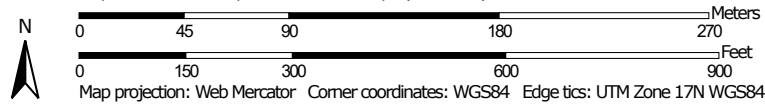
Tallman Environmental, Inc.
2095 Highway 211 NW, 2F #116
Braselton, Georgia 30517
aj@tallmans.net
678.468.2288

Soil Map—Gwinnett County, Georgia




Soil Map may not be valid at this scale.

Map Scale: 1:3,240 if printed on A landscape (11" x 8.5") sheet.





MAP LEGEND

Area of Interest (AOI)

 Area of Interest (AOI)




















Soils







 Soil Map Unit Polygons

 Soil Map Unit Lines


 Soil Map Unit Points

Special Point Features






-  Blowout
-  Borrow Pit
-  Clay Spot
-  Closed Depression
-  Gravel Pit
-  Gravelly Spot
-  Landfill
-  Lava Flow
-  Marsh or swamp
-  Mine or Quarry
-  Miscellaneous Water
-  Perennial Water
-  Rock Outcrop
-  Saline Spot
-  Sandy Spot
-  Severely Eroded Spot
-  Sinkhole
-  Slide or Slip
-  Sodic Spot

-  Spoil Area
-  Stony Spot
-  Very Stony Spot
-  Wet Spot
-  Other
-  Special Line Features


Water Features

 Streams and Canals

Transportation

-  Rails
-  Interstate Highways
-  US Routes
-  Major Roads
-  Local Roads

Background

 Aerial Photography

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:15,800.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service
 Web Soil Survey URL:
 Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Gwinnett County, Georgia
 Survey Area Data: Version 16, Sep 2, 2025

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Mar 14, 2022—Jun 21, 2022

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

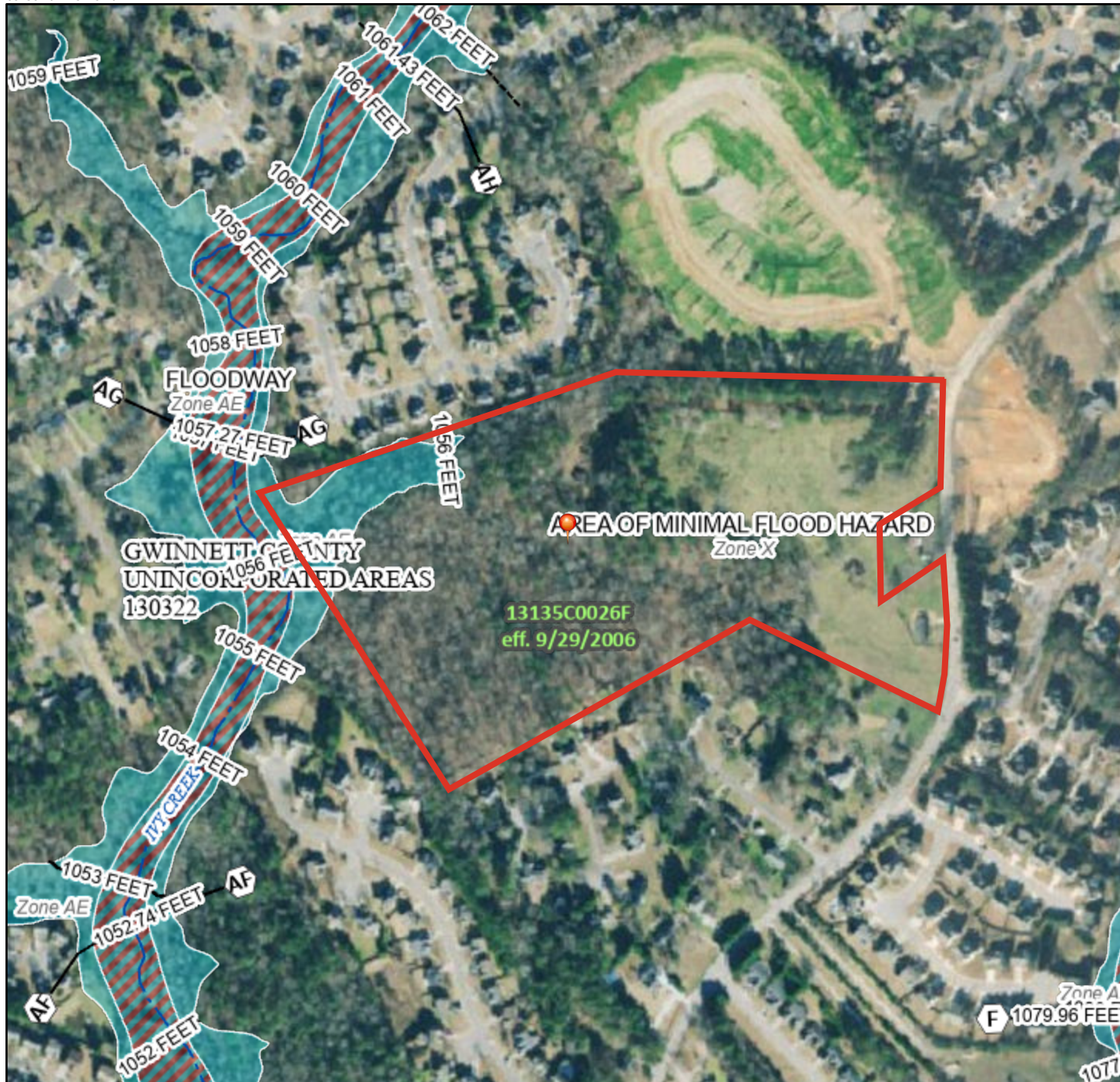
Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
AmC2	Appling sandy loam, 6 to 10 percent slopes, moderately eroded	10.0	33.2%
ApB	Appling-Hard Labor complex, 2 to 6 percent slopes	1.3	4.3%
ARE	Ashlar, Rion, and Wateree soils, 10 to 25 percent slopes	0.1	0.4%
AsF	Ashlar-Wateree complex, 15 to 45 percent slopes, stony	0.2	0.7%
Cfs	Chewacla silt loam, 0 to 2 percent slopes, frequently flooded	5.0	16.5%
PfC2	Pacolet sandy loam, 6 to 10 percent slopes, moderately eroded	0.0	0.1%
ToA	Toccoa fine sandy loam, 0 to 4 percent slopes, frequently flooded	0.0	0.1%
WrE2	Wedowee sandy loam, 10 to 25 percent slopes, eroded	13.5	44.7%
Totals for Area of Interest		30.2	100.0%

National Flood Hazard Layer FIRMMette



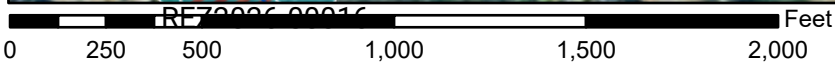
83°58'20"W 34°5'26"N



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

- | | | |
|-----------------------------|--|---|
| SPECIAL FLOOD HAZARD AREAS | | Without Base Flood Elevation (BFE)
Zone A, V, A99 |
| | | With BFE or Depth Zone AE, AO, AH, VE, AR |
| | | Regulatory Floodway |
| OTHER AREAS OF FLOOD HAZARD | | 0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X |
| | | Future Conditions 1% Annual Chance Flood Hazard Zone X |
| | | Area with Reduced Flood Risk due to Levee. See Notes. Zone X |
| | | Area with Flood Risk due to Levee Zone D |
| OTHER AREAS | | NO SCREEN Area of Minimal Flood Hazard Zone X |
| | | Effective LOMRs |
| GENERAL STRUCTURES | | Area of Undetermined Flood Hazard Zone D |
| | | Channel, Culvert, or Storm Sewer |
| | | Levee, Dike, or Floodwall |
| OTHER FEATURES | | 20.2 Cross Sections with 1% Annual Chance Water Surface Elevation |
| | | 17.5 Coastal Transect |
| | | Base Flood Elevation Line (BFE) |
| | | Limit of Study |
| | | Jurisdiction Boundary |
| MAP PANELS | | Digital Data Available |
| | | No Digital Data Available |
| | | Unmapped |
- The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.



83°57'42"W 34°4'56"N

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 12/8/2025 at 12:08 AM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.



U.S. Fish and Wildlife Service, National Standards and Standards Team,
wetlands_team@fws.gov

December 5, 2025

Wetlands

- Estuarine and Marine Deepwater
- Freshwater Emergent Wetland
- Lake
- Freshwater Forested/Shrub Wetland
- Freshwater Pond
- Other
- Estuarine and Marine Wetland
- Riverine

This map is for general reference only. The US Fish and Wildlife Service is not responsible for the accuracy or currentness of the base data shown on this map. All wetlands related data should be used in accordance with the layer metadata found on the Wetlands Mapper web site.

PHOTOGRAPHIC LOG

Site Location:

2538 and 2570 Kilgore Road, Buford, Gwinnett County, Georgia

Photo No.
1

Date:
12/4/25

Direction Photo Taken:

South

Description:

Structures at 2570 Kilgore Road



Photo No.
2

Date:
12/4/25

Direction Photo Taken:

Northeast

Description:

Wellhouse associated with Photo 1 residence



PHOTOGRAPHIC LOG

Site Location:

2538 and 2570 Kilgore Road, Buford, Gwinnett County, Georgia

Photo No.
3

Date:
12/4/25

Direction Photo Taken:

West

Description:

Residence at 2538 Kilgore Road



Photo No.
4

Date:
12/4/25

Direction Photo Taken:

Northwest

Description:

Outbuilding associated with Photo 3 residence



PHOTOGRAPHIC LOG

Site Location:

2538 and 2570 Kilgore Road, Buford, Gwinnett County, Georgia

Photo No.
5

Date:
12/4/25

Direction Photo Taken:

East

Description:

Open land on the Subject Property



Photo No.
6

Date:
12/4/25

Direction Photo Taken:

South

Description:

Forested area on the Subject Property



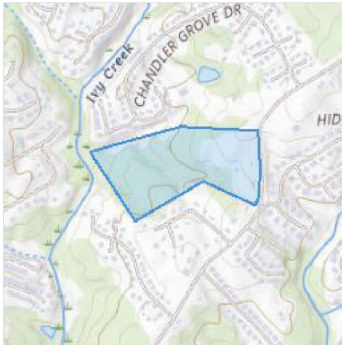
IPaC resource list

This report is an automatically generated list of species and other resources such as critical habitat (collectively referred to as *trust resources*) under the U.S. Fish and Wildlife Service's (USFWS) jurisdiction that are known or expected to be on or near the project area referenced below. The list may also include trust resources that occur outside of the project area, but that could potentially be directly or indirectly affected by activities in the project area. However, determining the likelihood and extent of effects a project may have on trust resources typically requires gathering additional site-specific (e.g., vegetation/species surveys) and project-specific (e.g., magnitude and timing of proposed activities) information.

Below is a summary of the project information you provided and contact information for the USFWS office(s) with jurisdiction in the defined project area. Please read the introduction to each section that follows (Endangered Species, Migratory Birds, USFWS Facilities, and NWI Wetlands) for additional information applicable to the trust resources addressed in that section.

Location

Gwinnett County, Georgia



Local office

Georgia Ecological Services Field Office

☎ (706) 613-9493

📠 (706) 613-6059

✉ gaes_assistance@fws.gov

355 East Hancock Avenue

Room 320

Athens, GA 30601-2523

Endangered species

This resource list is for informational purposes only and does not constitute an analysis of project level impacts.

The primary information used to generate this list is the known or expected range of each species. Additional areas of influence (AOI) for species are also considered. An AOI includes areas outside of the species range if the species could be indirectly affected by activities in that area (e.g., placing a dam upstream of a fish population even if that fish does not occur at the dam site, may indirectly impact the species by reducing or eliminating water flow downstream). Because species can move, and site conditions can change, the species on this list are not guaranteed to be found on or near the project area. To fully determine any potential effects to species, additional site-specific and project-specific information is often required.

Section 7 of the Endangered Species Act **requires** Federal agencies to "request of the Secretary information whether any species which is listed or proposed to be listed may be present in the area of such proposed action" for any project that is conducted, permitted, funded, or licensed by any Federal agency. A letter from the local office and a species list which fulfills this requirement can **only** be obtained by requesting an official species list from either the Regulatory Review section in IPaC (see directions below) or from the local field office directly.

For project evaluations that require USFWS concurrence/review, please return to the IPaC website and request an official species list by doing the following:

1. Draw the project location and click CONTINUE.
2. Click DEFINE PROJECT.
3. Log in (if directed to do so).
4. Provide a name and description for your project.
5. Click REQUEST SPECIES LIST.

Listed species¹ and their critical habitats are managed by the [Ecological Services Program](#) of the U.S. Fish and Wildlife Service (USFWS) and the fisheries division of the National Oceanic and Atmospheric Administration (NOAA Fisheries²).

Species and critical habitats under the sole responsibility of NOAA Fisheries are **not** shown on this list. Please contact [NOAA Fisheries](#) for [species under their jurisdiction](#).

1. Species listed under the [Endangered Species Act](#) are threatened or endangered; IPaC also shows species that are candidates, or proposed, for listing. See the [listing status page](#) for more information. IPaC only shows species that are regulated by USFWS (see FAQ).
2. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

The following species are potentially affected by activities in this location:

Mammals

NAME	STATUS
Gray Bat <i>Myotis grisescens</i> Wherever found No critical habitat has been designated for this species. https://ecos.fws.gov/ecp/species/6329	Endangered
Tricolored Bat <i>Perimyotis subflavus</i> Wherever found No critical habitat has been designated for this species. https://ecos.fws.gov/ecp/species/10515	Proposed Endangered

Birds

NAME	STATUS
Whooping Crane <i>Grus americana</i> No critical habitat has been designated for this species. https://ecos.fws.gov/ecp/species/758	EXPN

Insects

NAME

STATUS

Monarch Butterfly *Danaus plexippus*

Proposed Threatened

Wherever found

There is **proposed** critical habitat for this species. Your location does not overlap the critical habitat.

<https://ecos.fws.gov/ecp/species/9743>

Critical habitats

Potential effects to critical habitat(s) in this location must be analyzed along with the endangered species themselves.

There are no critical habitats at this location.

You are still required to determine if your project(s) may have effects on all above listed species.

Bald & Golden Eagles

Bald and Golden Eagles are protected under the Bald and Golden Eagle Protection Act ² and the Migratory Bird Treaty Act (MBTA) ¹. Any person or organization who plans or conducts activities that may result in impacts to Bald or Golden Eagles, or their habitats, should follow appropriate regulations and consider implementing appropriate avoidance and minimization measures, as described in the various links on this page.

Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
- Nationwide avoidance and minimization measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

There are Bald Eagles and/or Golden Eagles in your [project](#) area.

Measures for Proactively Minimizing Eagle Impacts

For information on how to best avoid and minimize disturbance to nesting bald eagles, please review the [National Bald Eagle Management Guidelines](#). You may employ the timing and activity-specific distance recommendations in this document when designing your project/activity to avoid and minimize eagle impacts. For bald eagle information specific to Alaska, please refer to [Bald Eagle Nesting and Sensitivity to Human Activity](#).

The FWS does not currently have guidelines for avoiding and minimizing disturbance to nesting Golden Eagles. For site-specific recommendations regarding nesting Golden Eagles, please consult with the appropriate Regional [Migratory Bird Office](#) or [Ecological Services Field Office](#).

If disturbance or take of eagles cannot be avoided, an [incidental take permit](#) may be available to authorize any take that results from, but is not the purpose of, an otherwise lawful activity. For assistance making this determination for Bald Eagles, visit the [Do I Need A Permit Tool](#). For assistance making this determination for golden eagles, please consult with the appropriate Regional [Migratory Bird Office](#) or [Ecological Services Field Office](#).

Ensure Your Eagle List is Accurate and Complete

If your project area is in a poorly surveyed area in IPaC, your list may not be complete and you may need to rely on other resources to determine what species may be present (e.g. your local FWS field office, state surveys, your own surveys). Please review the [Supplemental Information on Migratory Birds and Eagles](#), to help you properly interpret the report for your specified location, including determining if there is sufficient data to ensure your list is accurate.

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to bald or golden eagles on your list, see the "Probability of Presence Summary" below to see when these bald or golden eagles are most likely to be present and breeding in your project area.

Review the FAQs

The FAQs below provide important additional information and resources.

Bald Eagle *Haliaeetus leucocephalus*

Breeds Sep 1 to Jul 31

This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities.

<https://ecos.fws.gov/ecp/species/1626>

Probability of Presence Summary

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read "[Supplemental Information on Migratory Birds and Eagles](#)", specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Each green bar represents the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during a particular week of the year. (A year is represented as 12 4-week months.) A taller bar indicates a higher probability of species presence. The survey effort (see below) can be used to establish a level of confidence in the presence score. One can have higher confidence in the presence score if the corresponding survey effort is also high.

How is the probability of presence score calculated? The calculation is done in three steps:

1. The probability of presence for each week is calculated as the number of survey events in the week where the species was detected divided by the total number of survey events for that week. For example, if in week 12 there were 20 survey events and the Spotted Towhee was found in 5 of them, the probability of presence of the Spotted Towhee in week 12 is 0.25.
2. To properly present the pattern of presence across the year, the relative probability of presence is calculated. This is the probability of presence divided by the maximum probability of presence across all weeks. For example, imagine the probability of presence in week 20 for the Spotted Towhee is 0.05, and that the probability of presence at week 12 (0.25) is the maximum of any week of the year. The relative probability of presence on week 12 is $0.25/0.25 = 1$; at week 20 it is $0.05/0.25 = 0.2$.
3. The relative probability of presence calculated in the previous step undergoes a statistical conversion so that all possible values fall between 0 and 10, inclusive. This is the probability of presence score.

To see a bar's probability of presence score, simply hover your mouse cursor over the bar.

Breeding Season (■)

Yellow bars denote a very liberal estimate of the time-frame inside which the bird breeds across its entire range. If there are no yellow bars shown for a bird, it does not breed in your project area.

Survey Effort (|)

Vertical black lines superimposed on probability of presence bars indicate the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps. The number of surveys is expressed as a range, for example, 33 to 64 surveys.

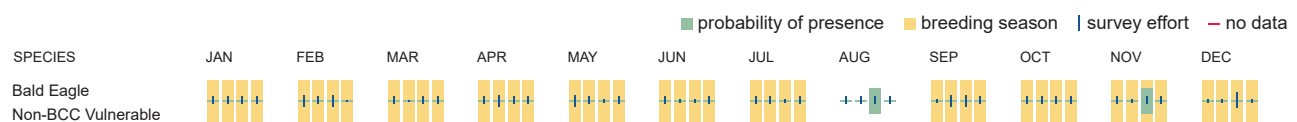
To see a bar's survey effort range, simply hover your mouse cursor over the bar.

No Data (-)

A week is marked as having no data if there were no survey events for that week.

Survey Timeframe

Surveys from only the last 10 years are used in order to ensure delivery of currently relevant information. The exception to this is areas off the Atlantic coast, where bird returns are based on all years of available data, since data in these areas is currently much more sparse.



Bald & Golden Eagles FAQs

What does IPaC use to generate the potential presence of bald and golden eagles in my specified location?

The potential for eagle presence is derived from data provided by the [Avian Knowledge Network \(AKN\)](#). The AKN data is based on a growing collection of [survey, banding, and citizen science datasets](#) and is queried and filtered to return a list of those birds reported as occurring in the 10km grid cell(s) which your project intersects, and that have been identified as warranting special attention because they are an eagle ([Bald and Golden Eagle Protection Act](#) requirements may apply).

Proper interpretation and use of your eagle report

On the graphs provided, please look carefully at the survey effort (indicated by the black vertical line) and for the existence of the "no data" indicator (a red horizontal line). A high survey effort is the key component. If the survey effort is high, then the probability of presence score can be viewed as more dependable. In contrast, a low survey effort line or no data line (red horizontal) means a lack of data and, therefore, a lack of certainty about presence of the species. This list is not perfect; it is simply a starting point for identifying what birds have the potential to be in your project area, when they might be there,

and if they might be breeding (which means nests might be present). The list and associated information help you know what to look for to confirm presence and helps guide you in knowing when to implement avoidance and minimization measures to eliminate or reduce potential impacts from your project activities or get the appropriate permits should presence be confirmed.

How do I know if eagles are breeding, wintering, or migrating in my area?

To see what part of a particular bird's range your project area falls within (i.e. breeding, wintering, migrating, or resident), you may query your location using the [RAIL Tool](#) and view the range maps provided for birds in your area at the bottom of the profiles provided for each bird in your results. If an eagle on your IPaC migratory bird species list has a breeding season associated with it (indicated by yellow vertical bars on the phenology graph in your "IPaC PROBABILITY OF PRESENCE SUMMARY" at the top of your results list), there may be nests present at some point within the timeframe specified. If "Breeds elsewhere" is indicated, then the bird likely does not breed in your project area.

Interpreting the Probability of Presence Graphs

Each green bar represents the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during a particular week of the year. A taller bar indicates a higher probability of species presence. The survey effort can be used to establish a level of confidence in the presence score.

How is the probability of presence score calculated? The calculation is done in three steps:

The probability of presence for each week is calculated as the number of survey events in the week where the species was detected divided by the total number of survey events for that week. For example, if in week 12 there were 20 survey events and the Spotted Towhee was found in 5 of them, the probability of presence of the Spotted Towhee in week 12 is 0.25.

To properly present the pattern of presence across the year, the relative probability of presence is calculated. This is the probability of presence divided by the maximum probability of presence across all weeks. For example, imagine the probability of presence in week 20 for the Spotted Towhee is 0.05, and that the probability of presence at week 12 (0.25) is the maximum of any week of the year. The relative probability of presence on week 12 is $0.25/0.25 = 1$; at week 20 it is $0.05/0.25 = 0.2$.

The relative probability of presence calculated in the previous step undergoes a statistical conversion so that all possible values fall between 0 and 10, inclusive. This is the probability of presence score.

Breeding Season ()

Yellow bars denote a very liberal estimate of the time-frame inside which the bird breeds across its entire range. If there are no yellow bars shown for a bird, it does not breed in your project area.

Survey Effort ()

Vertical black lines superimposed on probability of presence bars indicate the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data ()

A week is marked as having no data if there were no survey events for that week.

Survey Timeframe

Surveys from only the last 10 years are used in order to ensure delivery of currently relevant information. The exception to this is areas off the Atlantic coast, where bird returns are based on all years of available data, since data in these areas is currently much more sparse.

Migratory birds

The Migratory Bird Treaty Act (MBTA)¹ prohibits the take (including killing, capturing, selling, trading, and transport) of protected migratory bird species without prior authorization by the Department of Interior U.S. Fish and Wildlife Service (Service).

1. The [Migratory Birds Treaty Act](#) of 1918.
2. The [Bald and Golden Eagle Protection Act](#) of 1940.

Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
- Nationwide avoidance and minimization measures for birds
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

Measures for Proactively Minimizing Migratory Bird Impacts

Your IPaC Migratory Bird list showcases [birds of concern](#), including [Birds of Conservation Concern \(BCC\)](#), in your project location. This is not a comprehensive list of all birds found in your project area. However, you can help proactively minimize significant impacts to all birds at your project location by implementing the measures in the [Nationwide avoidance and minimization measures for birds](#) document, and any other project-specific avoidance and minimization measures suggested at the link [Measures for avoiding and minimizing impacts to birds](#) for the birds of concern on your list below.

Ensure Your Migratory Bird List is Accurate and Complete

If your project area is in a poorly surveyed area, your list may not be complete and you may need to rely on other resources to determine what species may be present (e.g. your local FWS field office, state surveys, your own surveys). Please review the [Supplemental Information on Migratory Birds and Eagles document](#), to help you properly interpret the report for your specified location, including determining if there is sufficient data to ensure your list is accurate.

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the "Probability of Presence Summary" below to see when these birds are most likely to be present and breeding in your project area.

Review the FAQs

The FAQs below provide important additional information and resources.

NAME	BREEDING SEASON
<p>Bald Eagle <i>Haliaeetus leucocephalus</i></p> <p>This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities.</p> <p>https://ecos.fws.gov/ecp/species/1626</p>	Breeds Sep 1 to Jul 31
<p>Chimney Swift <i>Chaetura pelagica</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p>	Breeds Mar 15 to Aug 25
<p>Chuck-will's-widow <i>Antrostomus carolinensis</i></p> <p>This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA</p>	Breeds May 10 to Jul 10
<p>Eastern Whip-poor-will <i>Antrostomus vociferus</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p>	Breeds May 1 to Aug 20
<p>Grasshopper Sparrow <i>Ammodramus savannarum perpallidus</i></p> <p>This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA</p> <p>https://ecos.fws.gov/ecp/species/8329</p>	Breeds Jun 1 to Aug 20
<p>Kentucky Warbler <i>Geothlypis formosa</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p>	Breeds Apr 20 to Aug 20
<p>Prairie Warbler <i>Setophaga discolor</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p>	Breeds May 1 to Jul 31
<p>Prothonotary Warbler <i>Protonotaria citrea</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p>	Breeds Apr 1 to Jul 31
<p>Red-headed Woodpecker <i>Melanerpes erythrocephalus</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p>	Breeds May 10 to Sep 10
<p>Rusty Blackbird <i>Euphagus carolinus</i></p> <p>This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA</p>	Breeds elsewhere
<p>Wood Thrush <i>Hylocichla mustelina</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p>	Breeds May 10 to Aug 31

Probability of Presence Summary

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read "[Supplemental Information on Migratory Birds and Eagles](#)", specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Each green bar represents the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during a particular week of the year. (A year is represented as 12 4-week months.) A taller bar indicates a higher probability of species presence. The survey effort (see below) can be used to establish a level of confidence in the presence score. One can have higher confidence in the

presence score if the corresponding survey effort is also high.

How is the probability of presence score calculated? The calculation is done in three steps:

1. The probability of presence for each week is calculated as the number of survey events in the week where the species was detected divided by the total number of survey events for that week. For example, if in week 12 there were 20 survey events and the Spotted Towhee was found in 5 of them, the probability of presence of the Spotted Towhee in week 12 is 0.25.
2. To properly present the pattern of presence across the year, the relative probability of presence is calculated. This is the probability of presence divided by the maximum probability of presence across all weeks. For example, imagine the probability of presence in week 20 for the Spotted Towhee is 0.05, and that the probability of presence at week 12 (0.25) is the maximum of any week of the year. The relative probability of presence on week 12 is $0.25/0.25 = 1$; at week 20 it is $0.05/0.25 = 0.2$.
3. The relative probability of presence calculated in the previous step undergoes a statistical conversion so that all possible values fall between 0 and 10, inclusive. This is the probability of presence score.

To see a bar's probability of presence score, simply hover your mouse cursor over the bar.

Breeding Season (■)

Yellow bars denote a very liberal estimate of the time-frame inside which the bird breeds across its entire range. If there are no yellow bars shown for a bird, it does not breed in your project area.

Survey Effort (|)

Vertical black lines superimposed on probability of presence bars indicate the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps. The number of surveys is expressed as a range, for example, 33 to 64 surveys.

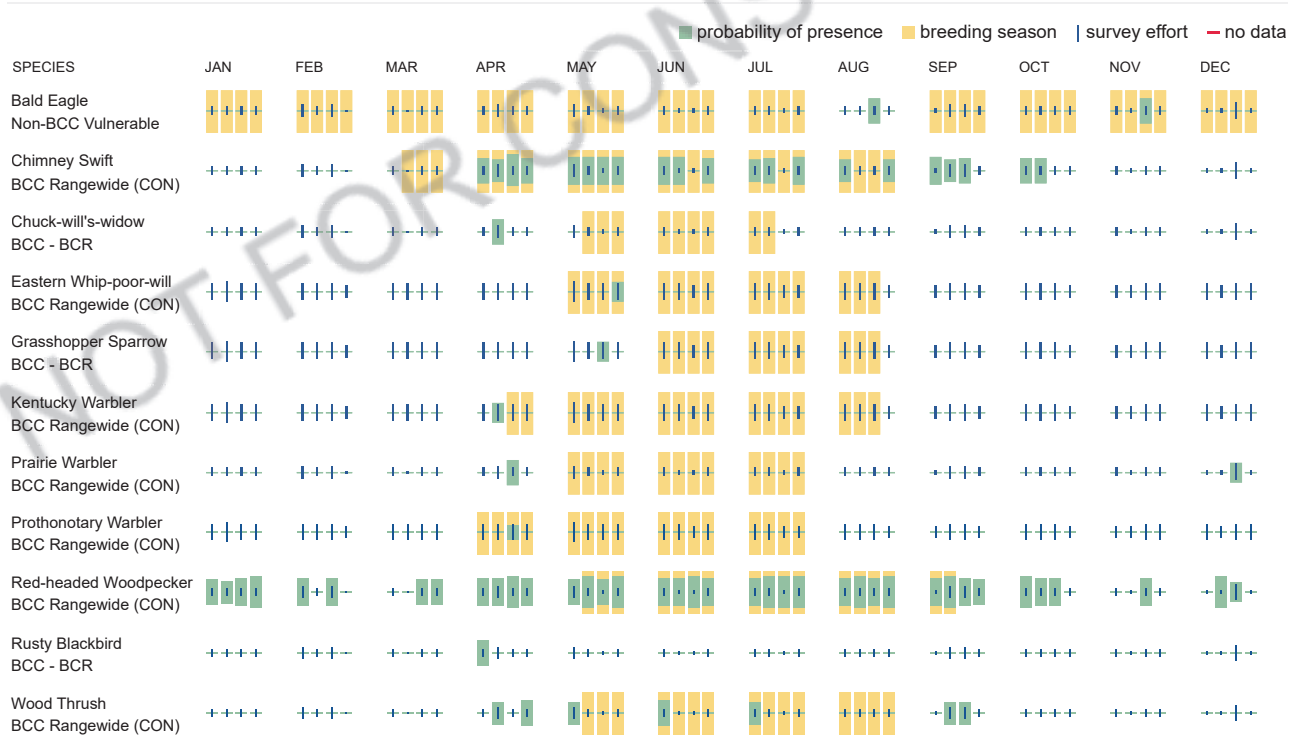
To see a bar's survey effort range, simply hover your mouse cursor over the bar.

No Data (-)

A week is marked as having no data if there were no survey events for that week.

Survey Timeframe

Surveys from only the last 10 years are used in order to ensure delivery of currently relevant information. The exception to this is areas off the Atlantic coast, where bird returns are based on all years of available data, since data in these areas is currently much more sparse.



Migratory Bird FAQs

Tell me more about avoidance and minimization measures I can implement to avoid or minimize impacts to migratory birds.

[Nationwide Avoidance & Minimization Measures for Birds](#) describes measures that can help avoid and minimize impacts to all birds at any location year-round. When birds may be breeding in the area, identifying the locations of any active nests and avoiding their destruction is one of the most effective ways to minimize impacts. To see when birds are most likely to occur and breed in your project area, view the Probability of Presence Summary. [Additional measures](#) or [permits](#) may be advisable depending on the type of activity you are conducting and the type of infrastructure or bird species present on your project site.

What does IPaC use to generate the list of migratory birds that potentially occur in my specified location?

The Migratory Bird Resource List is comprised of [Birds of Conservation Concern \(BCC\)](#), and other species that may warrant special attention in your project location, such as those listed under the Endangered Species Act or the [Bald and Golden Eagle Protection Act](#) and those species marked as "Vulnerable". See the FAQ "What are the levels of concern for migratory birds?" for more information on the levels of concern covered in the IPaC migratory bird species list.

The migratory bird list generated for your project is derived from data provided by the [Avian Knowledge Network \(AKN\)](#). The AKN data is based on a growing collection of [survey, banding, and citizen science datasets](#) and is queried and filtered to return a list of those birds reported as occurring in the 10km grid cell(s) with which your project intersects. These species have been identified as warranting special attention because they are BCC species in that area, an eagle ([Bald and Golden Eagle Protection Act](#) requirements may apply), or a species that has a particular vulnerability to offshore activities or development.

Again, the Migratory Bird Resource list includes only a subset of birds that may occur in your project area. It is not representative of all birds that may occur in your project area. To get a list of all birds potentially present in your project area, and to verify survey effort when no results present, please visit the [Rapid Avian Information Locator \(RAIL\) Tool](#).

Why are subspecies showing up on my list?

Subspecies profiles are included on the list of species present in your project area because observations in the AKN for **the species** are being detected. If the species are present, that means that the subspecies may also be present. If a subspecies shows up on your list, you may need to rely on other resources to determine if that subspecies may be present (e.g. your local FWS field office, state surveys, your own surveys).

What does IPaC use to generate the probability of presence graphs for the migratory birds potentially occurring in my specified location?

The probability of presence graphs associated with your migratory bird list are based on data provided by the [Avian Knowledge Network \(AKN\)](#). This data is derived from a growing collection of [survey, banding, and citizen science datasets](#).

Probability of presence data is continuously being updated as new and better information becomes available. To learn more about how the probability of presence graphs are produced and how to interpret them, go to the Probability of Presence Summary and then click on the "Tell me about these graphs" link.

How do I know if a bird is breeding, wintering, or migrating in my area?

To see what part of a particular bird's range your project area falls within (i.e. breeding, wintering, migrating, or resident), you may query your location using the [RAIL Tool](#) and view the range maps provided for birds in your area at the bottom of the profiles provided for each bird in your results. If a bird on your IPaC migratory bird species list has a breeding season associated with it (indicated by yellow vertical bars on the phenology graph in your "IPaC PROBABILITY OF PRESENCE SUMMARY" at the top of your results list), there may be nests present at some point within the timeframe specified. If "Breeds elsewhere" is indicated, then the bird likely does not breed in your project area.

What are the levels of concern for migratory birds?

Migratory birds delivered through IPaC fall into the following distinct categories of concern:

1. "BCC Rangewide" birds are [Birds of Conservation Concern \(BCC\)](#) that are of concern throughout their range anywhere within the USA (including Hawaii, the Pacific Islands, Puerto Rico, and the Virgin Islands);
2. "BCC - BCR" birds are BCCs that are of concern only in particular Bird Conservation Regions (BCRs) in the continental USA; and
3. "Non-BCC - Vulnerable" birds are not BCC species in your project area, but appear on your list either because of the [Bald and Golden Eagle Protection Act](#) requirements (for eagles) or (for non-eagles) potential susceptibilities in offshore areas from certain types of development or activities (e.g. offshore energy development or longline fishing).

Although it is important to avoid and minimize impacts to all birds, efforts should be made, in particular, to avoid and minimize impacts to the birds on this list, especially BCC species. For more information on avoidance and minimization measures you can implement to help avoid and minimize migratory bird impacts, please see the FAQ "Tell me more about avoidance and minimization measures I can implement to avoid or minimize impacts to migratory birds".

Details about birds that are potentially affected by offshore projects

For additional details about the relative occurrence and abundance of both individual bird species and groups of bird species within your project area off the Atlantic Coast, please visit the [Northeast Ocean Data Portal](#). The Portal also offers data and information about other taxa besides birds that may be helpful to you in your project review. Alternately, you may download the bird model results files underlying the portal maps through the [NOAA NCCOS Integrative Statistical Modeling and Predictive Mapping of Marine Bird Distributions and Abundance on the Atlantic Outer Continental Shelf](#) project webpage.

Proper interpretation and use of your migratory bird report

The migratory bird list generated is not a list of all birds in your project area, only a subset of birds of priority concern. To learn more about how your list is generated and see options for identifying what other birds may be in your project area, please see the FAQ "What does IPaC use to generate the migratory birds potentially occurring in my specified location". Please be aware this report provides the "probability of presence" of birds within the 10 km grid cell(s) that overlap your project; not your exact project footprint. On the graphs provided, please look carefully at the survey effort (indicated by the black vertical line) and for the existence of the "no data" indicator (a red horizontal line). A high survey effort is the key component. If the survey effort is high, then the probability of presence score can be viewed as more dependable. In contrast, a low survey effort bar or no data bar means a lack of data and, therefore, a lack of certainty about presence of the species. This list does not represent all birds present in your project area. It is simply a starting point for identifying what birds of concern have the potential to be in your project area, when they might be there, and if they might be breeding (which means nests might be present). The list and associated information help you know what to look for to confirm presence and helps guide implementation of avoidance and minimization measures to eliminate or reduce potential impacts from your project activities, should presence be confirmed. To learn more about avoidance and minimization measures, visit the FAQ "Tell me about avoidance and minimization measures I can implement to avoid or minimize impacts to migratory birds".

Interpreting the Probability of Presence Graphs

Each green bar represents the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during a particular week of the year. A taller bar indicates a higher probability of species presence. The survey effort can be used to establish a level of confidence in the presence score.

REF2026-00016 Page 39 of 106
How is the probability of presence score calculated? The calculation is done in three steps:

BF

The probability of presence for each week is calculated as the number of survey events in the week where the species was detected divided by the total number of survey events for that week. For example, if in week 12 there were 20 survey events and the Spotted Towhee was found in 5 of them, the probability of presence of the Spotted Towhee in week 12 is 0.25.

To properly present the pattern of presence across the year, the relative probability of presence is calculated. This is the probability of presence divided by the maximum probability of presence across all weeks. For example, imagine the probability of presence in week 20 for the Spotted Towhee is 0.05, and that the probability of presence at week 12 (0.25) is the maximum of any week of the year. The relative probability of presence on week 12 is $0.25/0.25 = 1$; at week 20 it is $0.05/0.25 = 0.2$.

The relative probability of presence calculated in the previous step undergoes a statistical conversion so that all possible values fall between 0 and 10, inclusive. This is the probability of presence score.

Breeding Season ()

Yellow bars denote a very liberal estimate of the time-frame inside which the bird breeds across its entire range. If there are no yellow bars shown for a bird, it does not breed in your project area.

Survey Effort ()

Vertical black lines superimposed on probability of presence bars indicate the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data ()

A week is marked as having no data if there were no survey events for that week.

Survey Timeframe

Surveys from only the last 10 years are used in order to ensure delivery of currently relevant information. The exception to this is areas off the Atlantic coast, where bird returns are based on all years of available data, since data in these areas is currently much more sparse.

Facilities

National Wildlife Refuge lands

Any activity proposed on lands managed by the [National Wildlife Refuge](#) system must undergo a 'Compatibility Determination' conducted by the Refuge. Please contact the individual Refuges to discuss any questions or concerns.

There are no refuge lands at this location.

Fish hatcheries

There are no fish hatcheries at this location.

Wetlands in the National Wetlands Inventory (NWI)

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

This location overlaps the following wetlands:

FRESHWATER
FORESTED/SHRUB WETLAND
[PFO1A](#)

A full description for each wetland code can be found at the [National Wetlands Inventory website](#)

NOTE: This initial screening does **not** replace an on-site delineation to determine whether wetlands occur. Additional information on the NWI data is provided below.

The Service's objective of mapping wetlands and deepwater habitats is to produce reconnaissance level information on the location, type and size of these resources. The maps are prepared from the analysis of high altitude imagery. Wetlands are identified based on vegetation, visible hydrology and geography. A margin of error is inherent in the use of imagery; thus, detailed on-the-ground inspection of any particular site may result in revision of the wetland boundaries or classification established through image analysis.

The accuracy of image interpretation depends on the quality of the imagery, the experience of the image analysts, the amount and quality of the collateral data and the amount of ground truth verification work conducted. Metadata should be consulted to determine the date of the source imagery used and any mapping problems.

Wetlands or other mapped features may have changed since the date of the imagery or field work. There may be occasional differences in polygon boundaries or classifications between the information depicted on the map and the actual conditions on site.

Data exclusions

Certain wetland habitats are excluded from the National mapping program because of the limitations of aerial imagery as the primary data source used to detect wetlands. These habitats include seagrasses or submerged aquatic vegetation that are found in the intertidal and subtidal zones of estuaries and nearshore coastal waters. Some deepwater reef communities (coral or tubercid worm reefs) have also been excluded from the inventory. These habitats, because of their depth, go undetected by aerial imagery.

Data precautions

Federal, state, and local regulatory agencies with jurisdiction over wetlands may define and describe wetlands in a different manner than that used in this inventory. There is no attempt, in either the design or products of this inventory, to define the limits of proprietary jurisdiction of any Federal, state, or local government or to establish the geographical scope of the regulatory programs of government agencies. Persons intending to engage in activities involving modifications within or adjacent to wetland areas should seek the advice of appropriate Federal, state, or local agencies concerning specified agency regulatory programs and proprietary jurisdictions that may affect such activities.

NOT FOR CONSULTATION



Cypripedium acaule Ait. Pink Lady's-slipper



Pink Lady's Slipper, *Cypripedium acaule*, by Richard and Teresa Ware. Guide to the Wildflowers, Trees and Shrubs of North Georgia and Adjacent States. <http://ngaflora.com>. Image may be subject to copyright.

Report Sighting

Federal Protection: No US federal protection

State Protection: Unusual

Global Rank: G5

State Rank: S4

Element Locations Tracked in Biotics: Yes

SWAP 2015 Species of Greatest Conservation Need (SGCN): No

SWAP 2025 Species of Greatest Conservation Need (SGCN): Yes

2025 SGCN Priority Tier: High Conservation Concern

Element Occurrences (EOs) in Georgia: 229

Habitat Summary for element in Georgia: Upland oak-hickory-pine forests; piney woods

Description

Perennial **herb** with two large basal leaves and a single flower stalk topped with a showy pink flower. **Leaves** are 3.5 - 12 inches (9 - 30) long and 1 - 6 inches (2.5 - 15 cm) wide, green and hairy on the upper surface, gray on the lower surface, with bluntly pointed tips and raised, parallel veins. The **flower stalk** is up to 2 feet (61 cm) tall. The **flower** is up to 2.6 inches (6.7 cm) long, with a showy, pink (rarely white), pouch- or slipper-shaped lip petal and two narrow, twisted, reddish-brown or green outwardly spreading petals. A large, green sepal curves over the top of the flower. **Fruit** is an oval capsule about 1/4 inches (3 - 4 cm) long, filled with thousands of dustlike seeds.

Similar Species

In flower, Pink Lady's Slipper resembles no other Georgia plant. Non-flowering plants may be recognized by the large pair of leaves (single in juvenile plants), rising directly from the ground without an aboveground stem; the leaves have conspicuously raised, parallel veins and are covered with sticky hairs. Lily-leaved Tway-blade (*Liparis liliifolia*) and Showy Orchis (*Galearis spectabilis*) also have paired leaves at ground level; their leaves are smooth, glossy, and somewhat succulent with a prominent midvein but without the conspicuous parallel veins. Speckled Wood-lily (*Clintonia umbellulata*) also has broad basal leaves; it occurs in similar habitats. Its leaves are usually more than 2 in number, and are hairless and glossy, with a deeply channeled midvein and no other conspicuous veins.

Related Rare Species

Yellow Lady's Slipper (*Cypripedium parviflorum*) is state-listed as Rare. It occurs in Georgia's Blue Ridge counties in rich, moist cove forests. For more information, see:

<http://explorer.natureserve.org/servlet/NatureServe?searchName=Cypripedium%20parviflorum>

Kentucky Lady's Slipper (*Cypripedium kentuckiense*) is state-listed as Endangered. It occurs in two counties in Georgia's upper Coastal Plain on seepy forested slopes with a mixed hardwood canopy. For more information, see: https://georgiabiodiversity.org/portal/profile?group=plants&es_id=21658

Habitat

Upland pine and mixed pine-hardwood forests with acidic soils; in the mountains, it often occurs near edges of Rhododendron thickets and mountain bogs.

Life History

Pink Lady's Slipper flowers are pollinated by bees that are attracted to their color and fragrance. A bee enters the colorful, inflated lip through a one-way opening and quickly discovers that there is neither pollen nor nectar available, and that it cannot readily escape. The bee can exit the flower only through two openings at the back of the lip. As it leaves from one of these openings, its back brushes against the female part of the flower, depositing a packet of pollen from the last PLS flower that it visited, and picking up a new packet of pollen from the male part of the flower. The packet is taken to the next visited flower; the pollen is held in small packets that stick to the bee's back where it is not accessible to the bee and can't be scraped off by the bee's legs. Bees soon learn to avoid these flowers because they fail to provide nectar or pollen; as a result, few flowers are pollinated.

Although flowers remain on the plants for several weeks to increase the chances of pollination, fewer than 10% of plants in a population will produce fruit in a given year. Fortunately, each fruit contains thousands of seeds. But the seeds are tiny, containing no stored food reserves, and must land on a patch of soil containing a specific fungus that provides nutrients for germination and subsequent plant growth. Pink Lady's Slipper plants dug from the wild and transplanted into gardens rarely reproduce due to the lack of this fungus. They also fail to thrive and will die after a couple of years, unless the specific fungus is present at the transplant site.

Survey Recommendations

Surveys are best conducted during flowering (April–June); they go dormant soon after fruiting (May–July).

Range

Throughout eastern North America, the upper Midwest, and adjacent Canadian provinces.

Threats

Logging, clearing, or development of upland forests, poaching, and exotic invasive species, especially Japanese honeysuckle.

Georgia Conservation Status

Pink Lady's Slipper is relatively abundant and has been observed in almost all north Georgia counties, although many of these populations, especially in the metro Atlanta area, have been destroyed. Large numbers of plants are known on national forest land. It is state-listed primarily because of the threat posed by poachers, who dig the plants for their gardens or for commercial sale. Plants dug from the wild and transplanted into gardens rarely survive for very long.

Conservation Management Recommendations

Periodically apply prescribed fire; occasional fire in upland forests will maintain the pine-dominated forests and patchy hardwood canopy that favor Pink Lady's Slipper. Prosecute plant poachers. Protect known sites from clearcutting and development. Eradicate exotic pest plants such as Japanese honeysuckle. Never buy plants of this species without determining that it came from an ethical source.

SWAP 2025 Conservation Actions:

- **Action 1:** Reassess the conservation status of SGCN before the next revision of Georgia's State Wildlife Action Plan

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Authors of Account

Linda G. Chafin

Date Compiled or Updated

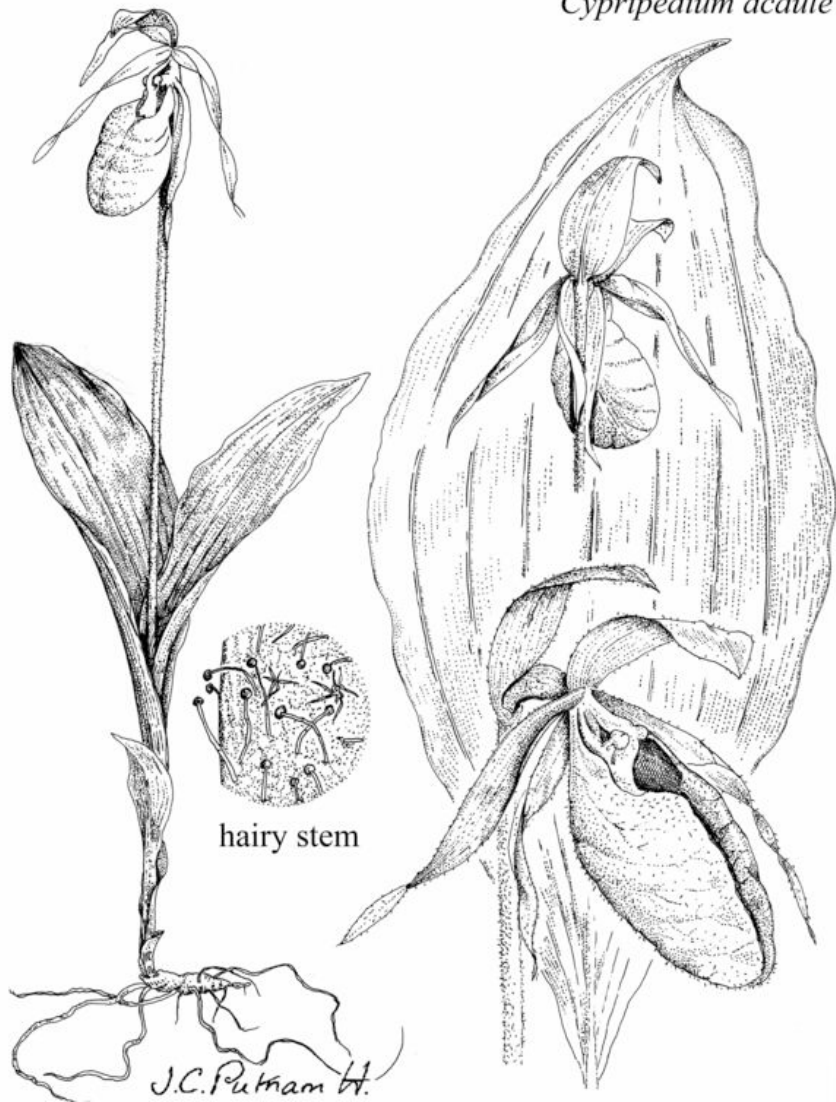
L. Chafin, April 2007: original account

K. Owers, Jan. 2010: updated status and ranks, added pictures

Z. Abouhamdan, April 2016: removed broken link

L. Chafin, Feb 2020: updated original account.

PINK LADY'S-SLIPPER
Cypripedium acaule



Cypripedium acaule, illustration by Jean C. Putnam Hancock.
Image may be subject to copyright.



Cypripedium acaule by Richard and Teresa Ware.
<http://ngaflora.com>. Image may be subject to copyright.



©Bill and Pam Anderson

Pink Lady's Slipper, Cypripedium acaule, by Bill and Pam Anderson. Image may be subject to copyright.

[Login](#)

Kilgore Road GNAHRGIS

Click the +/- zoom tool or use mouse to zoom into the map in the area of interest to display historic resource points.

The map shows a residential neighborhood with streets such as Atlanta-Chinesville Hwy, E Maddox Rd NE, and various local roads. A red square marker is placed on the map, and a 'Resource Info' popup window is open over it. The popup window contains the following information:

Resource Info	
View Details	
Id:	201036
Resource Type:	Single-family house types - One-story
Name(s):	
Location:	2580 Kilgore Rd, N side, 1/2 m W of Camp Branch Rd; field #61
City:	Buford
Zip Code:	30519-0000
Architectural Style:	No Academic style
Construction Year:	1900
Current Use:	Domestic (Residential) Use - Single Dwelling (Single Living Unit)
Original Use:	Domestic (Residential) Use - Single Dwelling (Single Living Unit)
National Register (NR) Individually Listed?:	No
National Register Eligible?:	Not Determined

Zoom to

R.S. Webb & Associates

Cultural Resource Management Consultants
2800 Holly Springs Parkway, Suite 200 - P.O. Drawer 1319
Holly Springs, Georgia 30142
Phone: 770-345-0706 - Fax: 770-345-0707

December 4, 2025

Mr. A.J. Tallman
Tallman Environmental
2095 Highway 211 NW, 2F #116
Braselton, Georgia 30517

Subject: Findings - Cultural Resources Literature Search
2538 and 2570 Kilgore Road Development Tract (R7184005 and R7184014)
Buford, Gwinnett County, Georgia
R.S. Webb & Associates Project No. 25-827-011

Dear Mr. Tallman:

BACKGROUND

During November 2025, R.S. Webb & Associates (RSWA) conducted a cultural resources literature review for the 2538/2570 Kilgore Road development tract in Gwinnett County, Georgia. The project area is located on the USGS Hog Mountain, Georgia 7.5-minute quadrangle (Figure 1). The literature search was conducted to identify previously recorded cultural resources located within or near the project area. A cultural resource is defined as a discrete area of human activity that is at least 50 years old.

The current study is based on locational information provided to RSWA by Tallman Environmental. The project area is located on the west side of Kilgore Road, southwest of its intersection with Camp Branch Road. The study tract encompasses approximately 30.4 acres within Gwinnett County Tax Parcels R7184005 and R7184014.

METHODOLOGY

RSWA performed the literature and records search by querying Georgia's Natural, Archaeological, and Historic Resources Geographic Information System (GNAHRGIS) database for relevant: Georgia Archaeological Site File (GASF) data; National Register of Historic Places (NRHP) property locations; and state-recognized historic resource locations. RSWA reviewed previously gathered (i.e., in-house) Gwinnett County historic resources survey data for this project. National Park Service (NPS) online NRHP databases were also searched for listed properties in the project area vicinity. RSWA examined an online listing of awarded Georgia Centennial Farms (1993-2025) and reviewed historic maps and aerial photographs courtesy of the Digital Library of Georgia, Alabamamaps.ua.edu and/or Historicaerials.com. The Civil War Sites Advisory Commission (CWSAC) battlefield maps/documents (1993, 2010), the military atlas of the Civil War (Davis *et al.* 1983) and *The Campaign for Atlanta* (Scaife 1993) were consulted for the locations of Civil War-era military actions, or associated sites, and/or features.

RESULTS

National Register of Historic Places: GNAHRGIS and NPS online databases shows no NRHP-listed properties within 1.0 kilometer (km) of the project area.

Georgia Centennial Farms: The online listing of Georgia Centennial Farms indicates that no Centennial Farms are located within 1.0 km of the project area.

Archeological Surveys and Resources: The GNAHRGIS database identifies two previous cultural resource projects being performed within 1.0 km of the project area. Of these, one is located within 500 m of the project area. This is a proposed cell tower survey with a study radius boundary located adjacent to the northeastern corner of the project area (Thomas 2006) (Figure 1).

The GASF/GNAHRGIS database shows no recorded archeological sites within 1.0 km of the project area.

Gwinnett County Historic Structures Surveys: Gwinnett County historic resources were surveyed in 1978 and 2006. Only the results of the 2006 survey are included in GNAHRGIS.

Maps for the 1978 survey show two recorded buildings (Resource Nos. 377 and 378) located within 1.0 km of the project area, but each building is situated more than 900 m from the project area. GNAHRGIS shows four historic resource locations within 1.0 km, three of which (Resource Nos. 200990, 201036 and 201041) are situated along Kilgore Road within approximately 500 m of the project area (Figure 1). All of these three buildings have been razed or removed from their recorded location. Prior to its removal, the closest recorded building, Resource No. 201036, was located approximately 40 m northeast of the project area.

Historic Cemeteries: Based on the USGS topographic map review, no mapped cemeteries are located within 1.0 km of the project area.

Gwinnett County Land Lottery (1819): The project area is located in Land Lot 184 of District 7 of original Gwinnett County. The District Plat of Survey and District Survey Field Notebooks were examined for structures, improvements or other features recorded within or adjacent to the project area. The land survey for District 7 was performed by John Allen from May 1819 to August 1819. These records indicate no improvements within Land Lot 184.

Civil War Features and Actions: Gwinnett County did not see heavy fighting during the war. However, in July of 1864, Union cavalry raided Lawrenceville to “strip” this area of horses and mules. There was a skirmish with a small force of Confederate cavalry at Lawrenceville, approximately 13 km south of the project area. After the Confederates were driven from the area, Union forces moved southwest and turned toward Loganville, arriving there on July 23rd. Based on the Official Military Atlas of the Civil War maps, Union troops also marched southeast from Lawrenceville through Windsor toward Covington. Clearly, mustering, marching, bivouacking, skirmishing and/or raiding occurred in Gwinnett County during the summer of 1864, but no such activities are known to have taken place within or near the project area (CWSAC 1993, 2010; Davis *et al.* 1983; Georgia Historical Commission 1957; Scaife 1993).

Structures and Features on Historic Maps and Aerial Photographs: Late 19th century topographic maps do not depict Kilgore Road or features in or near the project area. County highway maps from the middle-20th century show Kilgore Road with two buildings adjacent to and near the project area (Figure 1). A third building was located between them and adjacent to the project area by 1965. This is also reflected on topographic maps from 1964 and 1973, which show two additional buildings located in the project area.

Aerial photography from 1940 and 1955 shows agricultural use of the eastern half of the project area with buildings located adjacent to (east) and near (south) the project area (Figure 1). Only the southeastern part of the project area remained in cultivation as of 1963, and by 1972, three additional buildings were present in and adjacent to the project area (Figure 1). A new dwelling with access along the north project area boundary was established between 1978 and 1982. The pre-1940 building located adjacent to the east project area boundary was replaced between 1982 and 1988 and multi-family residential developments to the north, west and south appeared by 1993.

CONCLUSIONS

Based on the information reviewed during the current literature search, there are no NRHP-listed properties, recorded archeological sites, recorded historic resources or mapped cemeteries located within the project area. Historic maps and aerial photographs indicate agricultural use of the project area through the middle 20th century, but referenced buildings located in or adjacent to the project area date to the late 20th century.

CLOSING COMMENTS

Mr. Tallman, we appreciate the opportunity to work with Tallman Environmental on this project. If you have any questions concerning our findings, please contact Mr. Steve Webb at 770-345-0706.

Sincerely,
R.S. WEBB & ASSOCIATES



Grant D. Hudson
Historian



Robert S. Webb
President and Senior Principal Archeologist

Attachment: Figure 1

REFERENCES

Civil War Sites Advisory Commission

1993 Study Area USGS Quadrangle Maps. In: *Civil War Site Advisory Commission Report on the Nation's Civil War Battlefields*. National Park Service, Washington, D.C.

2010 *Update to the Civil War Sites Advisory Commission Report on the Nations's Civil War Battlefields, State of Georgia*. National Park Service, Washington, D.C.

Davis, G.B., L. J. Perry and J. W. Kirkley, compiled by C. D. Cowles

1983 *Atlas to Accompany the Official Records of the Union and Confederate Armies*. Reprint of the 1891-1895 edition. The Fairfax Press, New York.

Georgia Historical Commission

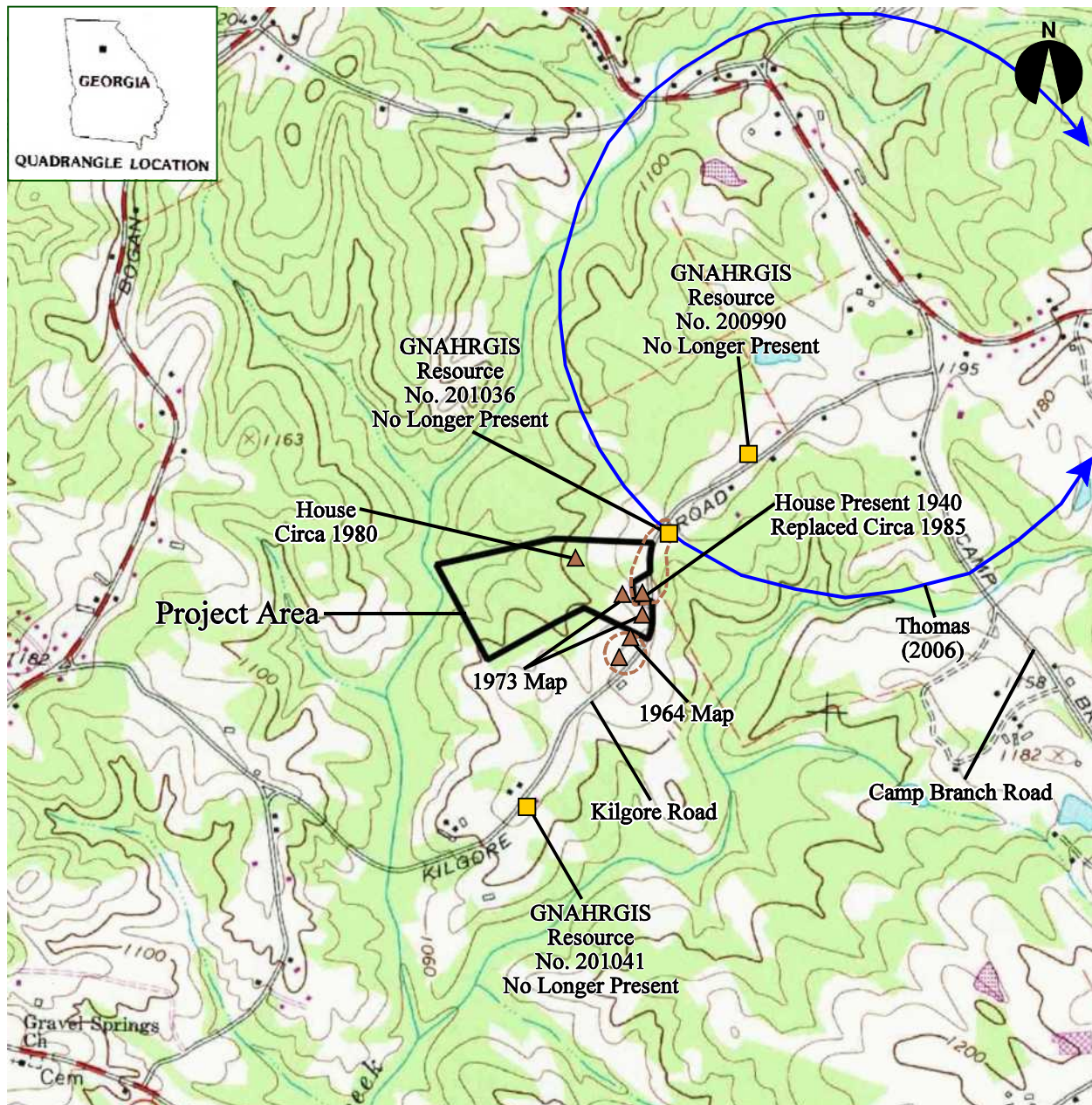
1957 "Garrard's Cavalry Raid". Historic Marker Located on the Grounds of the Gwinnett Historical Courthouse, West of North Clayton Street, Lawrenceville, Georgia. *Georgia Historical Commission*. Atlanta, Georgia.

Scaife, W.R.

1993 *The Campaign for Atlanta*. William R. Scaife, Atlanta, Georgia.

Thomas, L. A.

2006 *Cultural Resource Literature Search for Seven Proposed Cell Tower Sites Across Georgia*. TRC. Atlanta, Georgia.



▲ ○ Structure/Development on Historic Maps and Aerials
 ■ Recorded Historic Resource

— Previous Cultural Resource Project

Map Reference: 7.5-Minute USGS Quadrangle
 Hog Mountain (1964 PR 1973), Georgia

Scale
 0 457 meters
 0 1500 feet

Figure 1 Project Area, Previous Project and Known Cultural Resources

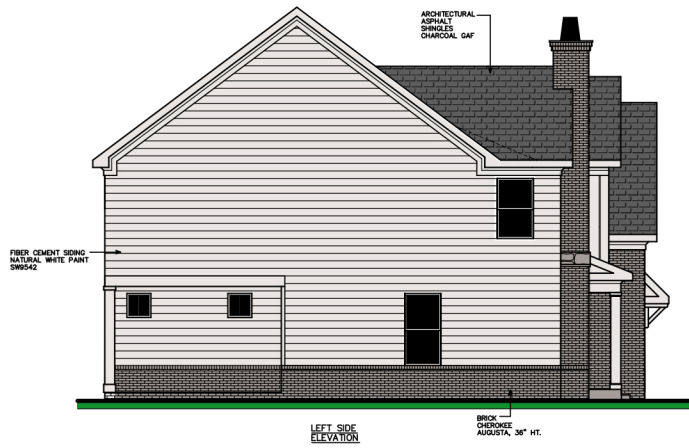
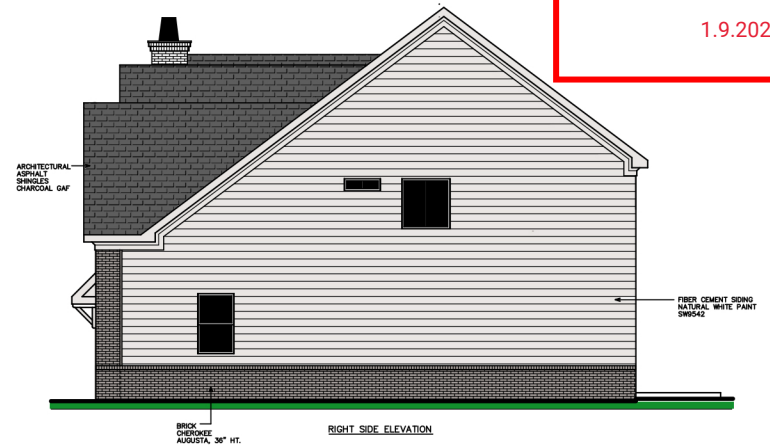
Exhibit D: Building Elevations

[attached]

GWINNETT COUNTY
PLANNING AND DEVELOPMENT

RECEIVED

1.9.2026

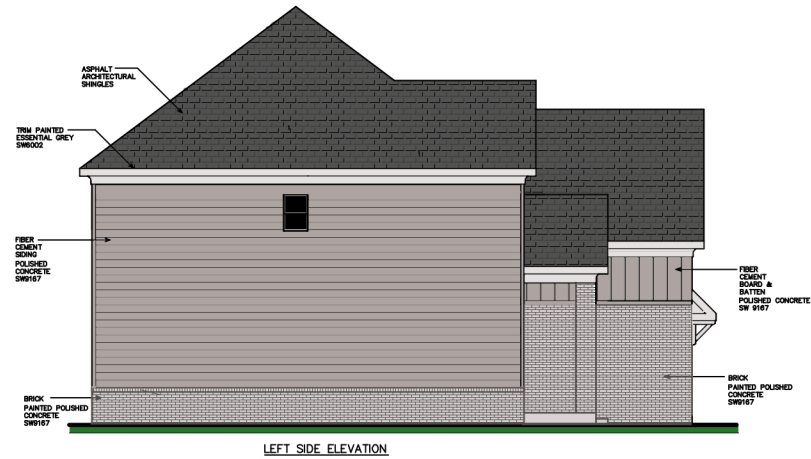
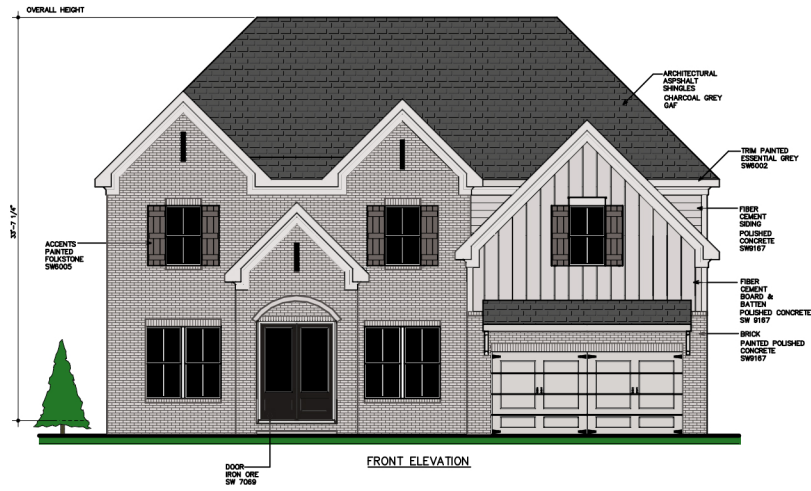


THOMPSON

SCALE: 1/16" = 1'-0"

PLANS ARE ARTISTS RENDERINGS AND ARE SUBJECT TO CHANGE WITHOUT NOTICE. SEE AGENT FOR FURTHER DETAILS.
COPYRIGHT 2024 SPRINGER & CO., LLC

LEGEND	
FIBER CEMENT SIDING: NATURAL WHITE SW9542	
FIBER CEMENT TRIM: NATURAL WHITE SW9542	
FIBER CEMENT BOARD & BATTEN: BLACK FOX 7020	
DOORS: NATURAL WHITE SW 9542	
BRICK: CHEROKEE AUGUSTA BRICK	



MAGNOLIA

SCALE: 1/16" = 1'-0"

PLANS ARE ARTISTS RENDERINGS AND ARE SUBJECT TO CHANGE WITHOUT NOTICE.
COPYRIGHT 2022 SPRINGER & CO., LLC 12-9-24

LEGEND	
SHUTTERS: FOLKSTONE SW605	
FIBER CEMENT SIDING: POLISHED CONCRETE SW9167	
FIBER CEMENT TRIM: ESSENTIAL GREY SW7062	
FIBER CEMENT BOARD & BATTEN: POLISHED CONCRETE SW9167	
FRONT DOOR: IRON ORE SW7069	
BRICK: PAINTED POLISHED CONCRETE SW9167	

Exhibit E: Letter of Intent and Applicant's Response to Standards

[attached]



Matthew P. Benson
Catherine W. Davidson
Gerald Davidson, Jr.*
Brian T. Easley
Rebecca B. Gober
Christopher D. Holbrook
Jessica P. Kelly
Shane M. Lanham

Jeffrey R. Mahaffey
Julia A. Maxwell
Steven A. Pickens
Jack M. Ryan
Gabrielle H. Schaller
S. Tess Shaheen
Andrew D. Stancil
R. Lee Tucker, Jr.

*Of Counsel

LETTER OF INTENT FOR REZONING APPLICATION

Mahaffey Pickens Tucker, LLP submits this Letter of Intent and attached rezoning application (the “Application”) on behalf of Simes Signature Properties, Inc., (the “Applicant”) requesting the rezoning of an approximately 30.282-acre assemblage of land (the “Property”) situated along Kilgore Road just west of its intersection with Camp Branch Road at 2538 and 2570 Kilgore Road. The Property is located within the Suburban Residential Future Development Type of the Gwinnett County 2045 Unified Plan (the “2045 Plan”) and is currently zoned RA-200.

The Applicant submits the Application requesting to rezone the Property to the OSC (Open Space Conservation District) zoning classification of the Gwinnett County Unified Development Ordinance (the “UDO”) to develop the Property as an attractive detached, single-family residential community. The proposed development would include 75 single-family detached homes at an overall density of 2.477 units per acre. Homes in the proposed community would be compatible with homes in the surrounding area and attractive architectural elements which would meet or exceed the requirements of the UDO’s Architectural Design Standards for Detached Residential Buildings. Building façades of the proposed homes would include a variety of architectural materials as depicted in the building elevations submitted with the Application. Further, sewer and water utilities are both available in the immediate vicinity. The minimum heated square footage

Sugarloaf Office || 1550 North Brown Road, Suite 125, Lawrenceville, Georgia 30043

NorthPoint Office || 11175 Cicero Drive, Suite 100, Alpharetta, Georgia 30022

TELEPHONE 770 232 0000

FACSIMILE 678 518 6880

Page 58 of 106
www.mptlawfirm.com

for the homes would be 2,400. Residents and visitors would access the neighborhood by a single entrance on Kilgore Road.

Further, 55.47% of the Property will be devoted as open space with a large portion provided at the rear of the development remaining largely untouched to prioritize the protection of the unique, natural resources found there. The remaining open space will be activated with planted green spaces, walking trails, a sport field, a dog park, and a picnic area for the use and enjoyment of residents. The wetlands at the rear of the Property will remain preserved and untouched.

The proposed development is compatible with surrounding land uses and is in line with the policies of the Gwinnett County 2045 Unified Plan (the “2045 Plan”). The surrounding area is characterized primarily by a variety of residential development including communities zoned the adjacent OSC, R-100MOD, R-100CSO, RZT, R75CSO, R-100, and RA-200. The proposed development is also in line with the policies of the 2045 Plan which broadly promotes residential development and encourages a variety of housing types. Chapter 4: Housing of the 2045 Plan provides abundant data and statistics that reflect a housing shortage in Gwinnett County. In the Needs & Opportunities section of Chapter 4, the 2045 Plan identifies a need of “More Housing Development” and provides that the “housing shortage inhibits population growth, business attraction, and other economic development by raising housing prices and failing to accommodate new residents. A higher volume of housing development is needed to meet economic goals and countywide growth potential.”

Additionally, the proposed development is in line with the policy and intent of the 2045 Plan for the Suburban Residential Future Development Area. The Suburban Residential Future Development Area recommends OSC as an appropriate zoning district, and the proposed development achieves the goals of the Development Area by providing single-family homes, open

space, and conservation areas. Therefore, a single-family residential development devoted to all the aforementioned aspects fits within the plan for the area. The proposed residential development would continue the residential development pattern and provide high-quality, attractive homes which complement the land use mix while preserving and activating natural spaces for residents to enjoy.

The Applicant and its representatives welcome the opportunity to meet with staff of the Gwinnett County Department of Planning & Development to answer any questions or to address any concerns relating to the matters set forth in this letter or in the Rezoning Application filed herewith. The Applicant respectfully requests your approval of these Applications.

Respectfully submitted this 12th day of February, 2026.

MAHAFFEY PICKENS TUCKER, LLP

Gabrielle H. Schaller

Gabrielle H. Schaller
Attorneys for Applicant

JUSTIFICATION FOR REZONING

The portions of the Gwinnett County Unified Development Ordinance (the “UDO”) which classify or may classify the property which is the subject of this Application (the “Property”) into any less intensive zoning classification other than as requested by the Applicant, are or would be unconstitutional in that they would destroy the Applicant's property rights without first paying fair, adequate and just compensation for such rights, in violation of Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States.

The application of the UDO as applied to the subject Property, which restricts its use to the present zoning classification, is unconstitutional, illegal, null and void, constituting a taking of the Applicant's and the Owner’s property in violation of the Just Compensation Clause of the Fifth Amendment and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States, Article I, Section I, Paragraph I, and Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Equal Protection Clause of the Fourteenth Amendment to the Constitution of the United States denying the Applicant an economically viable use of its land while not substantially advancing legitimate state interests.

The Property is presently suitable for development under the OSC classification as requested by the Applicant, and is not economically suitable for development under the present RA-200 zoning classification of Gwinnett County. A denial of this Application would constitute an arbitrary and capricious act by the Gwinnett County Board of Commissioners without any rational basis therefore, constituting an abuse of discretion in violation of Article I, Section I, Paragraph I and Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States.

A refusal by the Gwinnett County Board of Commissioners to rezone the Property to the OSC classification with such conditions as agreed to by the Applicant, so as to permit the only feasible economic use of the Property, would be unconstitutional and discriminate in an arbitrary, capricious and unreasonable manner between the Applicant and owners of similarly situated

property in violation of Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983 and the Equal Protection Clause of the Fourteenth Amendment to the Constitution of the United States. Any rezoning of the subject Property to the OSC classification, subject to conditions which are different from the conditions by which the Applicant may amend its application, to the extent such different conditions would have the effect of further restricting the Applicant's and the Owner's utilization of the subject Property, would also constitute an arbitrary, capricious and discriminatory act in zoning the Property to an unconstitutional classification and would likewise violate each of the provisions of the State and Federal Constitutions set forth hereinabove.

Opponents to the request set forth in the Application, or in any amendments to the Application, have waived their rights to appeal any decision of the Gwinnett County Board of Commissioners because they lack standing, have failed to exhaust administrative remedies, and/or because they failed to assert any legal or constitutional objections.

Accordingly, the Applicant respectfully requests that the rezoning application submitted by the Applicant relative to the Property be granted and that the Property be rezoned to the zoning classification as shown on the respective application.

This 17th day of October, 2025.

Respectfully submitted,

MAHAFFEY PICKENS TUCKER, LLP

Gabrielle H. Schaller

Gabrielle H. Schaller

Attorneys for Applicant

1550 North Brown Road
Suite 125
Lawrenceville, Georgia 30043
(770) 232-0000

Pursuant to requirements of the United Development Ordinance, the Board of Commissioners finds that the following standards are relevant in balancing the interest in promoting the public health, safety, morality, or general welfare against the right to the unrestricted use of property and shall govern the exercise of the zoning power.

- A. Whether a proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property:

Yes, approval of the Rezoning Application will permit a use that is suitable in view of the use and development of adjacent and nearby property. The proposed residential development is compatible with existing residential uses.

- B. Whether a proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property:

No, approval of the Rezoning Application will not adversely affect the existing use or usability of any of the nearby properties. Rather, the proposed development will complement the housing mix of the surrounding area.

- C. Whether the property to be affected by a proposed rezoning has reasonable economic use as currently zoned:

Due to the size, location, layout, and dimensions of the subject property, the Applicant submits that the Property does not have a reasonable economic use as currently zoned.

- D. Whether the proposed rezoning will result in a use which could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools:

No, approval of the Rezoning Application will not result in an excessive or burdensome use of the surrounding infrastructure systems. The proposed development has direct access to Kilgore Road. Additionally, water and sewer utilities are present in the immediate vicinity.

- E. Whether the proposed ~~Choose an item.~~ is in conformity with the policy and intent of the Unified Plan and Future Development Map:

Yes, approval of the Rezoning Application is in conformity with the policy and intent of the Gwinnett County 2045 Unified Plan, which specifically recommends OSC as an appropriate zoning district.

- F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning:

The Applicant submits that the quality of proposed homes, and the large amount of usable green space throughout the development provide supporting grounds for approval of the Application.

Exhibit F: Application and Disclosure of Campaign Contributions

[attached]

REZONING APPLICATION

AN APPLICATION TO AMEND THE OFFICIAL ZONING MAP OF GWINNETT COUNTY, GA.

APPLICANT INFORMATION	PROPERTY OWNER INFORMATION
Name: <u>Simes Signature Properties, LLC c/o Mahaffey Pickens Tucker LLP</u>	Name: <u>Simes Signature Properties Inc, Quintero Land Realty LLC, James W. Roberts (deceased)</u>
Address: <u>1550 N. Brown Rd, Ste 125</u>	Address: <u>23 Morgan St. and 2538 Kilgore Rd.</u>
City: <u>Lawrenceville</u>	City: <u>Buford</u>
State: <u>Georgia</u> ZIP: <u>30043</u>	State: <u>Georgia</u> ZIP: <u>30518</u>
Phone: <u>770-232-0000</u>	Phone: <u>770-232-0000</u>
Email: <u>GSchaller@MPTLawfirm.com</u>	Email: <u>GSchaller@MPTLawfirm.com</u>
Contact Person: <u>Gabrielle Schaller</u> Phone: <u>770-232-0000</u>	
Contact's Email: <u>GSchaller@MPTLawfirm.com</u>	
APPLICANT IS THE: <input type="checkbox"/> Owner's Agent <input type="checkbox"/> Property Owner <input checked="" type="checkbox"/> Contract Purchaser	
Current Zoning District(s): <u>RA-200</u> Requested Zoning District: <u>OSC</u>	
Parcel Number(s): <u>7184 005 & 7184 014</u> Acreage: <u>+/-30.282</u>	
Property Address(es): <u>2538 and 2570 Kilgore Rd, Buford, GA 30519</u>	
Proposed Development: <u>Open Space Conservation Single Family Subdivision</u>	
Variance(s): <u>n/a</u> Waiver(s): <u>n/a</u>	
RESIDENTIAL DEVELOPMENT No. of Dwelling Units: <u>75</u> Dwelling Unit Sq. Ft.: <u>2,400</u> Density: <u>2.477 units per acre</u> Floor Area Ratio (LRR, MRR, HRR): _____	NON-RESIDENTIAL DEVELOPMENT No. of Buildings: <u>n/a</u> Total Building Sq. Ft.: <u>n/a</u> Floor Area Ratio: _____
MIXED-USE DEVELOPMENT	
No. of Dwelling Units: <u>n/a</u> Dwelling Unit Sq. Ft.: <u>n/a</u>	
Total Non-Residential Sq. Ft.: <u>n/a</u> Floor Area Ratio: <u>n/a</u>	



The undersigned below, or as attached, is the owner of the property considered in this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon in less than six (6) months from the date of last action by the Board of Commissioners.

Property Owner

Name: Robin Roberts Head

Signature: Robin Roberts Head

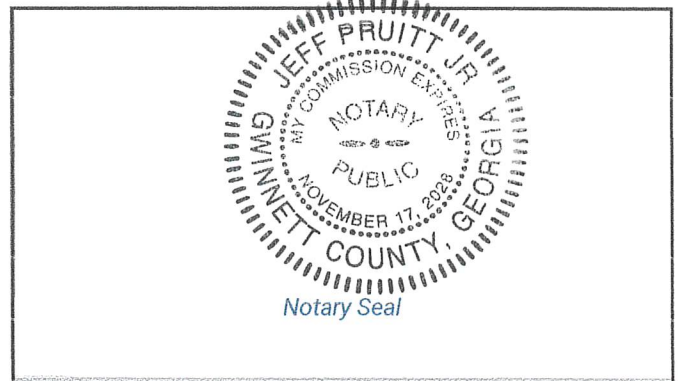
Date: 10-8-2025

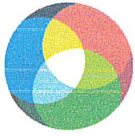
Notary Public

Name: Jeff Pruitt Jr.

Signature: [Handwritten Signature]

Date: 10-8-25





The undersigned below, or as attached, is the owner of the property considered in this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon in less than six (6) months from the date of last action by the Board of Commissioners.

Property Owner

Name: Meridith Simes & David Quijters

Signature: [Handwritten Signature]

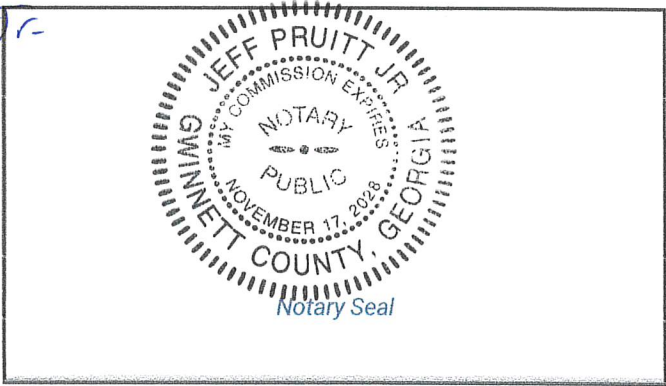
Date: 10-8-25

Notary Public

Name: [Handwritten Signature] Jeff Pruitt Jr.

Signature: [Handwritten Signature]

Date: 10-8-25





Disclosure of campaign contributions aggregating to \$250.00 or more to a member of the Board of Commissioners is required pursuant to Section 270-25.E. of the Unified Development Ordinance.

Name: Kassie Prater

Relationship to Application: Applicant Representative of Applicant

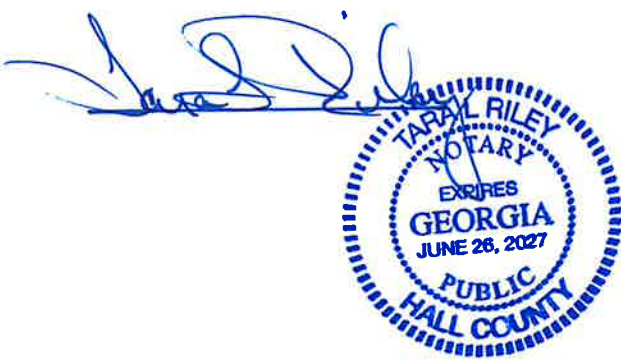
Campaign Contributions? Yes No

Contributions:

Official Name	Contribution Amount	Date of Contribution	Contributor
See Attached			

Signature: *Kassie Prater*

Date: 3/24/20



**CONFLICT OF INTEREST CERTIFICATION FOR REZONING/CHANGE IN
CONDITIONS AND SPECIAL USE PERMIT APPLICATIONS**

MAHAFFEY PICKENS TUCKER, LLP

<u>NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL</u>	<u>CONTRIBUTION</u>	<u>DATE</u>
Nicole Love Hendrickson, Chairwoman	\$3,300	10/01/2024
Ben Ku, Commissioner	\$2,000	04/25/2025
Matthew Holtkamp, Commissioner	\$2,000	06/19/2025
Ben Ku, Commissioner	\$1,300	02/03/2026

Shane M. Lanham, Attorney at Law, PC

<u>NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL</u>	<u>CONTRIBUTION</u>	<u>DATE</u>
Nicole Love Hendrickson, Chairwoman	\$500.00	10/02/2024

Updated 08/29/2025

Exhibit G: DWR Sewer Capacity Certification Letter

[attached]

July 24, 2025

Scott Puckett
Puckett Inc
3215 Thompson Mill Rd
Buford, GA 30519

<input checked="" type="checkbox"/>	APPROVED
<input type="checkbox"/>	DENIED
<input type="checkbox"/>	CONDITIONALLY APPROVED
Sewer Capacity Request #C2025-187-07	
Expiration Date: 07/24/2026	
Tie-In Manhole FID: 761591	

RE: Sewer Availability for Proposed Development – 2538 and 2570 Kilgore Road
Parcel ID 7184 014, 7184 005

Dear Mr. Puckett:

This letter supersedes the earlier Sewer Capacity Certification C2025-096-05 dated May 13, 2025 addressed to Cameron Henderson of Atlas Land Development.

Gwinnett County Department of Water Resources (GCDWR) has reviewed the impact of the proposed development consisting of 76 single-family homes on the above parcel(s) and determined:

Gwinnett County has adequate sewer capacity to serve the proposed development.

This confirmation is based on your anticipated annual average daily flow of 13.2 gpm discharging to the sewer tie-in manhole at Facility ID **761591**.

Capacity Allocation: Sewer capacities are allocated only upon the issuance of a Development Permit, by Gwinnett County Planning & Development and after all applicable conditions established in this Sewer Capacity Certification, if any, are met.

Certification Expiration: The Sewer Capacity Certification is valid for 12 months from the date of this letter. If rezoning is approved, GCDWR will issue a revised Sewer Capacity Certification based on the approved units, as needed. The capacity request can be renewed for an additional 12 months. If a Development Permit has not been obtained by the expiration date above the developer may apply for renewal by providing evidence of active rezoning, plan review, or permitting efforts.

Once the Sewer Capacity Certification expires, the capacity is no longer allocated, and a new Sewer Capacity Certification request must be submitted for re-evaluation with no guarantee of available capacity. The evaluation will be based on the updated conditions of the sewer system.

Certification Invalidation: This certification is not valid if there are proposed changes to your development that could impact downstream sewer capacity (e.g. land use density, sewer tie-in



manhole, parcel changes, and/or rezoning). In this case the development must resubmit for a new Sewer Capacity Request with updated information prior to the expiration of the previous one.

Please contact us at 678-376-7026 if you have any questions.

Sincerely,

GWINNETT COUNTY DEPARTMENT OF WATER RESOURCES

A handwritten signature in blue ink that reads "Della Taylor".

Della Taylor, PE
Division Director, Infrastructure Support
678.376.2130

C: Gwinnett County DWR – Lorraine Campagne, Section Manager; Raghu Vemuru, Engineer V; Mikala Weston, Engineer III;
Geniva Sylvain, Engineer III;
Gwinnett County P&D – Charli Young, Planning Manager; Christina Dejarnette, Engineer I; Wendell Tumale Engineer III

Exhibit H: Traffic Impact Study

[attached]



Traffic Impact Analysis

Kilgore Road Development

Gwinnett County, Georgia

28 November 2025

TRAFFIC IMPACT
GROUP, LLC

Georgia PE Firm #7093
407.609.6985



Kilgore Road development - Gwinnett County

I hereby certify that this report was prepared by me or under my direct supervision, and that I am a duly Licensed Professional Engineer under the laws of the State of Georgia.



A handwritten signature in black ink, appearing to read "Scott P. Israelson".

REV
11/28/2025

Scott P. Israelson, P.E., PTOE
License No. 39784

Executive Summary

Project Description

The Kilgore Road Residential development is proposed in Gwinnett County, Georgia. The 30.57-acre parcel will consist of 75 single-family homes and is situated on the west side of Kilgore Road south of Hamilton Mill Road.

The site will have a full-access driveway to Kilgore Road.

Gwinnett County requires a Tier 2 TIS for developments that generate between 21 and 249 peak hour trips.

Trip Generation

The development is expected to generate 691 daily trips, with 15 entering trips and 42 exiting trips in the AM peak hour, and 47 entering and 28 exiting trips in the PM peak hour.

Turn Lanes/Access Management

Kilgore Road has a posted speed limit of 35 mph and its AADT is less than 6,000 vehicles per day. The criteria for a left-turn lane is 120 lots. This development will consist of 76 lots so will fall below County thresholds for a left-turn lane.

Based on NCHRP Report No. 279, no right-turn lanes are recommended for the project accesses.

Intersection Sight Distance analysis shows no obstructions to sight lines at the project access.

Traffic Impacts

Analysis shows that the project access is expected to operate acceptably.

No improvements are recommended.

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I. Introduction

The Kilgore Road Residential development is proposed in Gwinnett County, Georgia. The 30.57-acre parcel is situated on the west side of Kilgore Road south of Hamilton Mill Road.

The development is proposed to consist of 75 single-family homes and will have a full-access driveway to Kilgore Road.

Gwinnett County requires a Tier 2 TIS for developments that generate between 21 and 249 peak hour trips.

The study area included the following intersections:

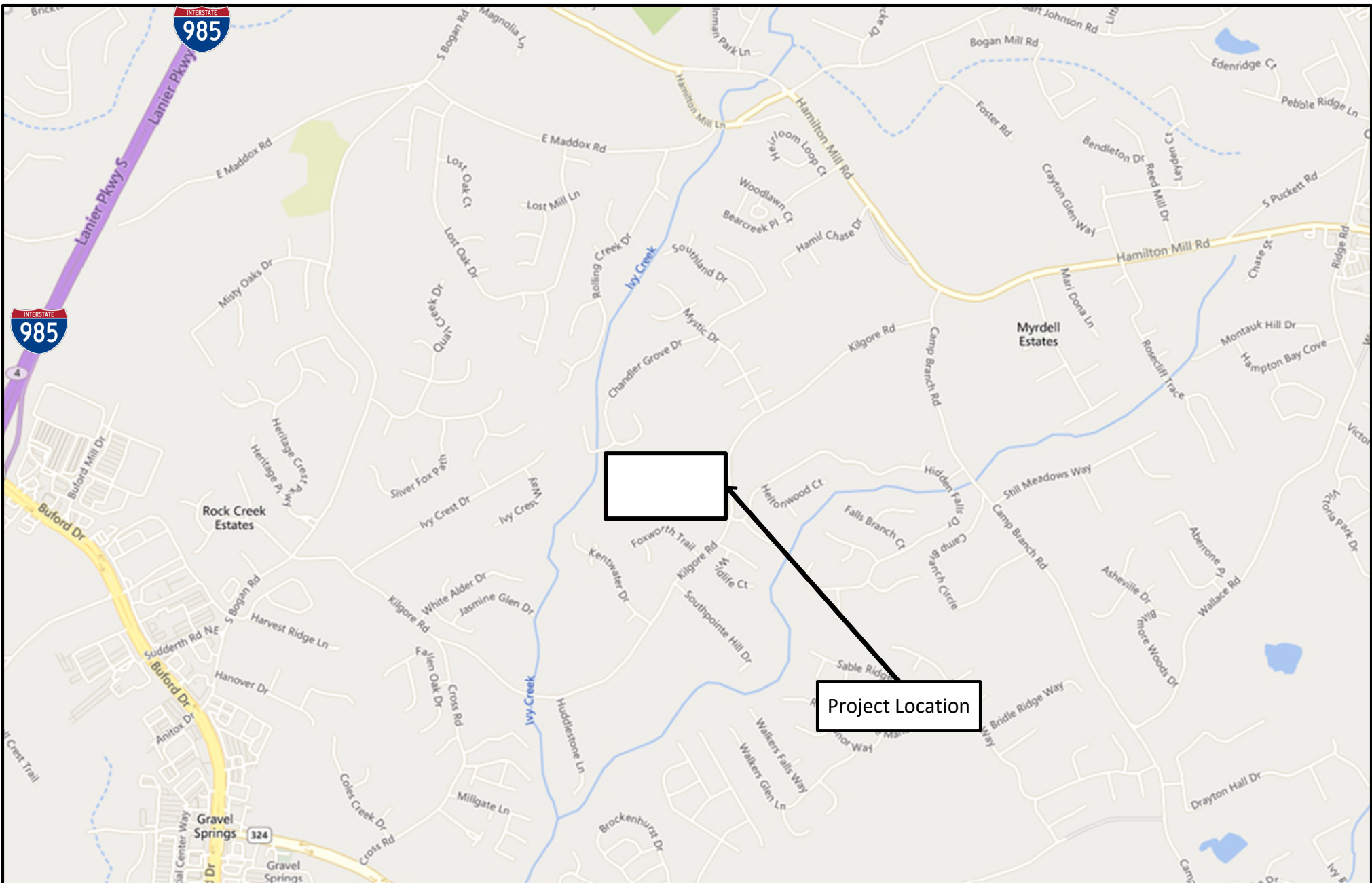
- Kilgore Road & Access

The study analyzed the following scenarios:

- 2025 Existing Conditions
- Full Build 2027 Conditions

The AM peak hour and PM peak hour were analyzed.

Figure 1 shows the most recent site plan. **Figure 2** shows the project vicinity map.



Vicinity Map

Date: 1 October 2025

Figure 2

TRAFFIC IMPACT
GROUP, LLC

Kilgore Road Residential - Gwinnett County
REZ2026-00016

Page 81 of 106

BF

II. Existing Conditions

A. Existing Roadway Conditions

Kilgore Road is a two-lane undivided roadway with a posted speed limit of 35 mph. It has an AADT of 4,616 vehicles per day (November 2025) and is not functionally classified.

B. Existing Intersection Geometry

The project access will be a full-access connection to Kilgore Road aligned with Hidden Falls Drive.

C. Traffic Volumes

Traffic data collection for study area intersections was performed on September 23-24, 2025. **Figure 3** displays existing traffic volumes. These volumes can be found in the Appendix.

The most recent Average Annual Daily Traffic (AADT) volumes were retrieved from the Gwinnett County website.


D. Transit Routes

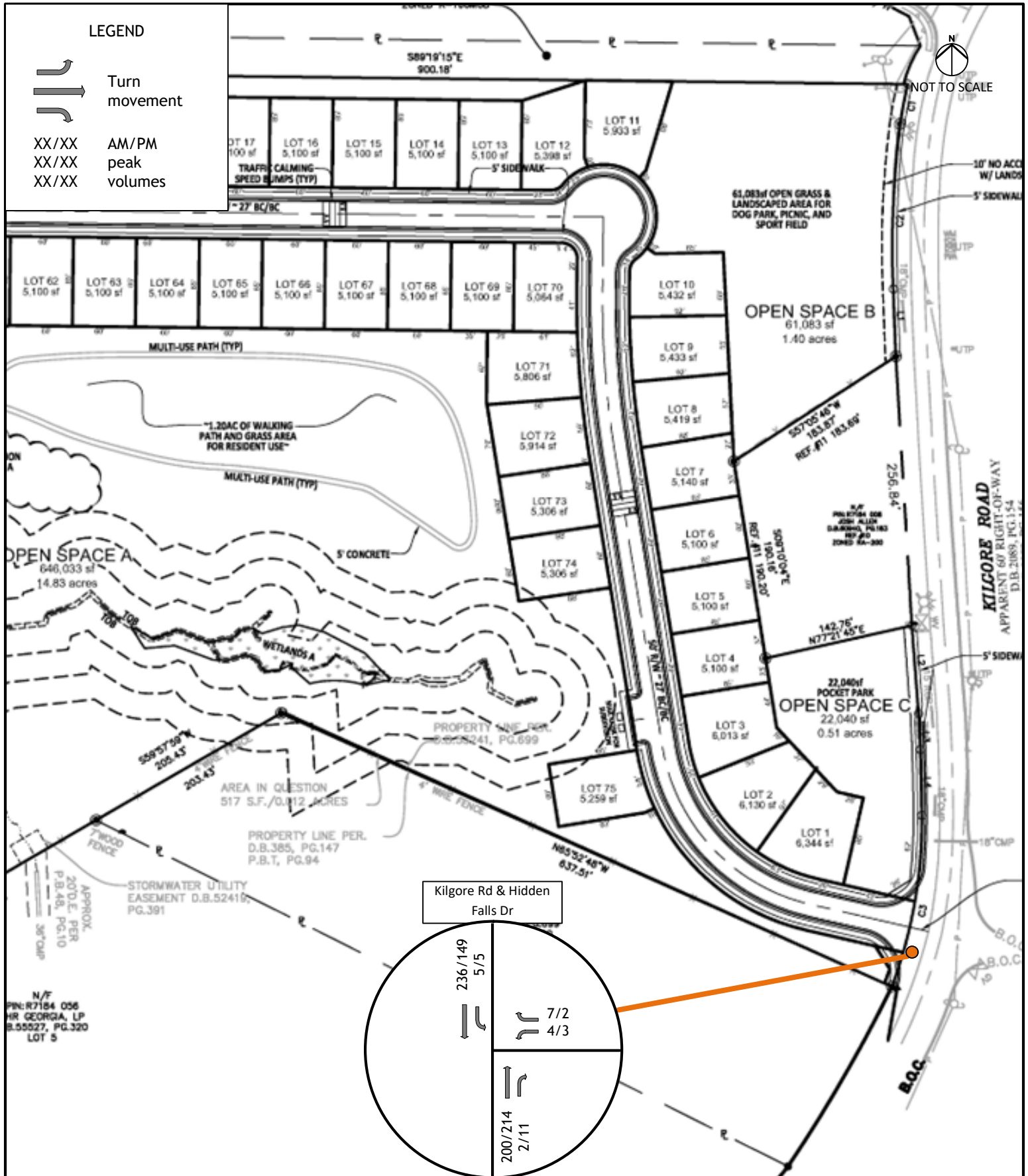
The nearest transit is Route 50 which runs on Buford Road nearly two miles from the development.

E. Pedestrian and Bicycle Facilities

Kilgore Road has sidewalk on the east side of the roadway, and on the west side of the roadway north of Bryant Meadows Circle.

LEGEND

-  Turn movement
- XX/XX AM/PM peak volumes
- XX/XX peak volumes
- XX/XX peak volumes



Existing Traffic Volumes

Date: 28 November 2025

Figure 3

III. Methodology

A. Base Assumptions

Intersection capacity analysis was conducted using Synchro v11.0. Trip generation was calculated using the 12th edition of the Institute of Transportation Engineers (ITE) *Trip Generation Manual*. Right-turn and left-turn lanes were examined using the National Cooperative Highway Research Program (NCHRP) Report No. 279 *Intersection Channelization Design Guide*.

B. Background Growth

Background growth is calculated using historical AADT volumes. Table 1 shows historical AADT and background growth calculations.

Table 1 - Study Area Background Growth				
Roadway	2017	2024	increase	annual growth
	AADT	AADT		
Hamilton Mill Rd west of Camp Branch Rd	14,800	15,100	2.0%	0.3%

Analysis shows that the background growth on Hamilton Mill Road is 0.3% per year. Historical AADT is the appendix.

Existing volumes were increased by 1% to estimate background growth for Full Build 2027 conditions.

C. Trip Generation

The development is proposed to consist of 75 single-family homes.

The *ITE Trip Generation Manual, 12th Edition* was used to estimate the projected trips by this development.

Table 2 contains the summary of the land uses and sizes used for trip generation estimates.

Table 2 - ITE Trip Generation								
Average Weekday Driveway Volumes					AM Peak Hour		PM Peak Hour	
Land Use	ITE Code	Size		Daily Trips	Enter	Exit	Enter	Exit
Single-Family Detached Housing	210	75	Dwelling Units	691	15	42	47	28


D. Trip Distribution

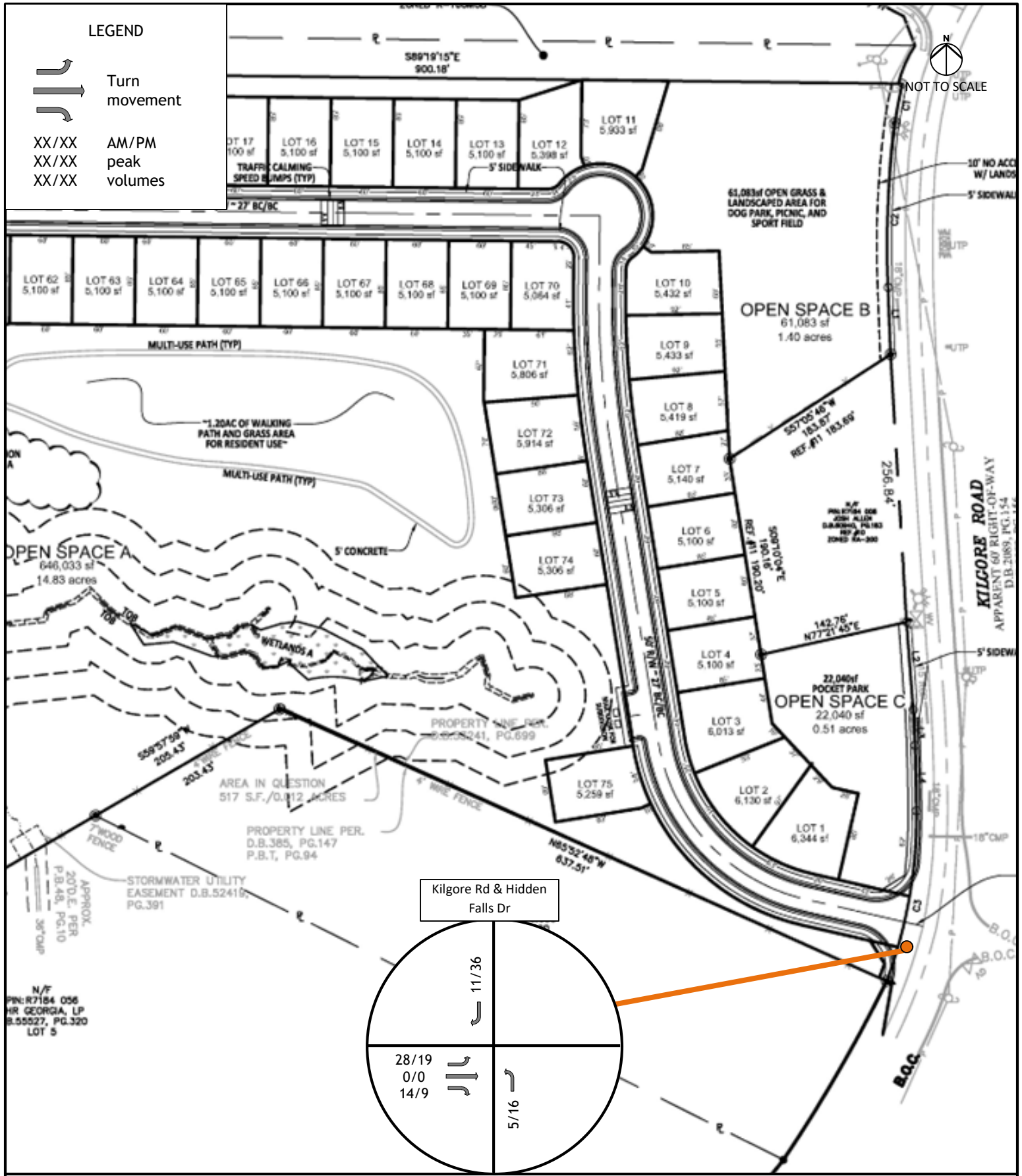
Trips for this proposed development were assigned to the surrounding roadway network based on existing traffic patterns. The proposed trip distribution for this project is 67% to/from the north and 33% to/from the south on Kilgore Rad.

Site trips are shown in **Figure 4**.

Full Build 2027 volumes are shown in **Figure 5**.

LEGEND

-  Turn movement
- XX/XX AM/PM peak volumes
- XX/XX peak volumes



NOT TO SCALE


Site Trips

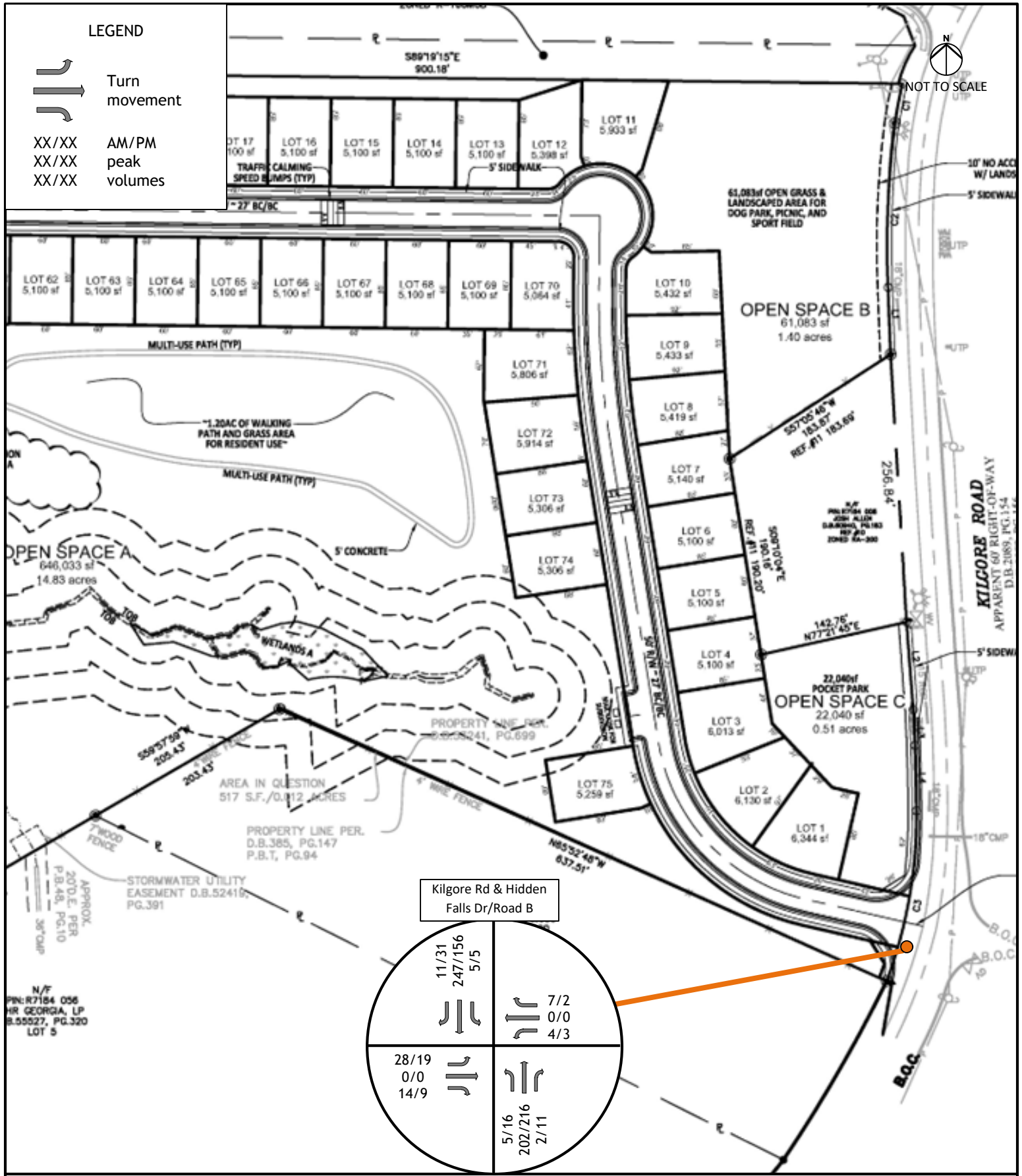
Date: 28 November 2025

Figure 4

TRAFFIC IMPACT
GROUP, LLC

LEGEND

-  Turn movement
- XX/XX AM/PM peak volumes



Full Build Volumes

Date: 28 November 2025

Figure 5

IV. Turn Lane/Access Management

A. Left-Turn Lanes

The Gwinnett County DOT *Criteria and Guidelines for Left Turn Lanes* provides direction on left turn lanes in the County. The table below cites thresholds for turn lanes:

**Gwinnett County – Residential Developments *
Left Turn Lane Criteria** **TABLE I**

Posted Speed Limit (mph)	2 Lane Routes ————ADT————		More Than 2 Lanes on Main Road ————ADT————	
	<6000	>=6000	<10,000	>=10,000
30 to 35	120 Lots	75 Lots	160 Lots	120 Lots
40 to 50	100 Lots	65 Lots	130 Lots	100 Lots
>= 55	75 lots	55 Lots	100 Lots	75 Lots

* Zoning Districts R-XX(X)

Kilgore Road has a posted speed limit of 35 mph and its AADT is less than 6,000 vehicles per day. The criteria for a left-turn lane is 120 lots. This development will consist of 76 lots so will fall below County thresholds for a left-turn lane.

B. Right-Turn Lanes

The County does not have criteria for right-turn lanes, so this analysis uses the National Cooperative Highway Research Program (NCHRP) Report 279 *Intersection Channelization Design Guide* for this study.

The NCHRP Report 279 is a tool in assessing the impacts from development. This report allows the traffic engineering professional to input roadway type, posted speed, advancing volume (and opposing volume for left turns), and number of turning vehicles. The result is a plot on a graph defined by the above inputs recommending turn lanes or not.

Table 3 shows the volumes used for analysis.

Driveway	AM/ PM	Approach	Posted Speed	Advancing Vol	RT Vol	Turn Lane needed?
Kilgore Rd & Road A	AM	SB	35	246	11	No
	PM			164	31	No

Based on Full Build volumes, no turn lanes from Kilgore Road are required for the driveways. These calculations can be found in the Appendix.

C. Intersection Sight Distance

The table below shows required ISD for posted speeds based on AASHTO Greenbook standards.

Speed (mph)	Stopping Sight Distance (ft.)	Design Intersection Sight Distance (ft.)
25	155	280
30	200	335
35	250	390
40	305	445
45	360	500
50	425	555
55	495	610
60	570	665
65	645	720

Source: *A Policy on Geometric Design of Highway and Streets*, 5th Edition, American Association of State Highway and Transportation Officials (AASHTO), 2004.

Kilgore Road has a posted speed limit of 35 mph, which requires an ISD of 390 feet. There are no sight distance obstructions that obscure the view of vehicles.



Kilgore Road & Access - looking north



Kilgore Road & Access - looking south

V. Capacity Analysis

The Transportation Research Board’s Highway Capacity Manual (HCM) uses the term “level of service” (LOS) to measure how traffic operates in intersections. There are currently six levels of service ranging from A to F. Level of Service “A” represents the best conditions and Level of Service “F” represents the worst. Synchro software was used to determine the level of service for intersections in the study area. All worksheet reports from the analyses can be found in the Appendix.

Table 4 shows the control delay per vehicle associated with LOS A through F for signalized and unsignalized intersections.

Table 4 - Highway Capacity Manual Levels of Service and Control Delay			
Signalized Intersection		Unsignalized Intersection	
Level of Service	Control Delay per Vehicle (sec)	Level of Service	Control Delay per Vehicle (sec)
A	≤ 10	A	≤ 10
B	> 10 and ≤ 20	B	> 10 and ≤ 15
C	> 20 and ≤ 35	C	> 15 and ≤ 25
D	> 35 and ≤ 55	D	> 25 and ≤ 35
E	> 55 and ≤ 80	E	> 35 and ≤ 50
F	> 80	F	> 50

A. Kilgore Road & Road B

The project access will be a full-access connection to Kilgore Road aligned with Hidden Falls Drive.

Table 5 shows the expected LOS, control delay, and 95th percentile queue length for Full Build 2027 conditions.

Table 6 - Intersection LOS, Delay, and Queue by Movement - 2026 Full Build								
Intersection	Approach	Movement	AM			PM		
			LOS	Delay	Queue	LOS	Delay	Queue
Kilgore Rd & Access/Hidden Falls Dr	EB	LT	B	13.0	8'	B	12.5	5'
		TH						
		RT						
	WB	LT	B	11.1	3'	B	12.0	-
		TH						
		RT						
	NB	LT	Free					
		TH						
		RT						
	SB	LT	Free					
		TH						
		RT						

VI. Summary and Conclusion

This study serves as an analysis of the traffic impacts from the Kilgore Road Residential development in Gwinnett County, Georgia.

GDOT requires this analysis to examine the project accesses and the effects of development.

Trip Generation

The development is expected to generate 691 daily trips, with 15 entering trips and 42 exiting trips in the AM peak hour, and 47 entering and 28 exiting trips in the PM peak hour.

Turn Lanes/Access Management

Kilgore Road has a posted speed limit of 35 mph and its AADT is less than 6,000 vehicles per day. The criteria for a left-turn lane is 120 lots. This development will consist of 75 lots so will fall below County thresholds for a left-turn lane.

Based on NCHRP Report No. 279, no right-turn lanes are recommended for the project access.

Intersection Sight Distance analysis shows no obstructions to sight lines at the project access.

Traffic Impacts

Analysis shows that the project access is expected to operate acceptably.

No improvements are recommended.

Appendix

Background Information

Traffic Volumes

Trip Generation

Capacity Analysis

Exhibit I: Public Participation Plan and Report

[attached]

Public Participation Plan

- 1. Who do you intend to contact in addition to property owners within 1,000 feet of the site, if any?**

District Planning Commissioners and District Commissioner.

- 2. Where do you plan to host the Public Participation Meeting? Ensure that the meeting is held in person at a convenient location for interested parties.**

Fairfield Inn & Suites By Marriott Mall of Georgia, 1355 Mall of Georgia Blvd., Buford, GA 30519

- 3. What date and time do you plan to host the Public Participation Meeting? Ensure that the meeting is held at a convenient time for interested parties, e.g. not during typical work hours on weekdays.**

November 3, 2025 at 6:00 p.m.

- 4. What is your method for providing opportunities for discussion with interested parties at the Public Participation Meeting?**

After a presentation, the Applicant team would open the meeting for comments and questions from the attendees.

Public Participation Report

- 1. List all groups that you notified of the requested application. Include a copy of the notification package and stamped Postal Service Form 3877 (attached) with itemized name and address of each addressee.**

Please see attached.

- 2. Provide the date, time, and location of all meeting(s) that the applicant and/or representative attended to discuss an application with interested parties.**

November 3, 2025 at 6:00 pm at Fairfield Inn & Suites By Marriott Mall of Georgia, 1355 Mall of Georgia Blvd., Buford, GA

30519

- 3. Provide the number of people who participated in the meeting(s). Include the sign-in sheet(s) with meeting date, time, location, and attendee names.**

Twelve (12) people participated in the meeting.

- 4. What issues and concerns were expressed by attendees at the meeting(s)?**

The attendees indicated concerns regarding the impact of the development on traffic, the conservation of open space, whether this development will be tapping into their sewer/water like a prior development, the size of the homes to be built, and what the timeline will look like. One neighbor expressed her concerns regarding the noise from construction, her concerns about this impacting her use of her property, and her dislike of the access point to the development being placed next to her property.

- 5. What are the applicant's responses to the issues and concerns that were expressed at the meetings? What changes to the development will be made?**

The benefit of the OSC zoning is that it requires the developer to leave 40% of the property as open space conservation, so there will be a significant amount of conservation on the property. A level 2 traffic study was conducted to for the property, so traffic concerns are being considered. Currently, our understanding is that we do not need an easement for access to sewer/water, so we will not be tapping in the way the neighbors described for the prior development; however, we expressed a desire to discuss with the engineer and then provide better answers based on that conversation to the neighbors. We have not settled on an exact size of the homes to be built, but the OSC zoning does have minimum requirements, and our application was for those minimum requirements. The homes will not be 1,200 or 1,500 sqft. as the neighbors worried about. We are looking at at least eighteens months from receiving rezoning before breaking ground on development. We cannot interfere with how a neighbor uses their property, and our rezoning will not affect your property's zoning and allowed uses. The access point was originally in a different spot on the site plan, but was moved by Gwinnett County to comply with requirements.

Exhibit J: Internal and External Agency Review Comments

[attached]



**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		02/20/2026	
Department/Agency Name:		Transportation	
Reviewer Name:		Brent Hodges	
Reviewer Title:		Construction Manager 2	
Reviewer Email Address:		Brent.Hodges@gwinnettcounty.com	
Case Number:		REZ2026-00016	
Case Address:		2538 Kilgore Road, Buford, 30519	
Comments:		<input checked="" type="checkbox"/>	<input type="checkbox"/>
		YES	NO
1	Kilgore Road is a local road. The Average Daily Traffic (ADT) volume is 4,438.		
2	The nearest Ride Gwinnett facility (Stop 5010 / Route 50) is located 1.8 miles away at Buford Drive (SR 20) and Toyota Mall of Georgia.		
3	Per section 360-30 of the UDO, the developer shall construct a 200' deceleration lane with a 50' taper.		
4	Per section 360-90 of the UDO, the developer shall construct a 5' concrete sidewalk along the entire site frontage.		
5	Traffic Calming shall be required for any street that exceeds 500' without a control point.		
6			
7			
Recommended Zoning Conditions:		<input type="checkbox"/>	<input checked="" type="checkbox"/>
		YES	NO
1			
2			
3			
4			
5			

Note: Attach additional pages, if needed

Revised 7/26/2021

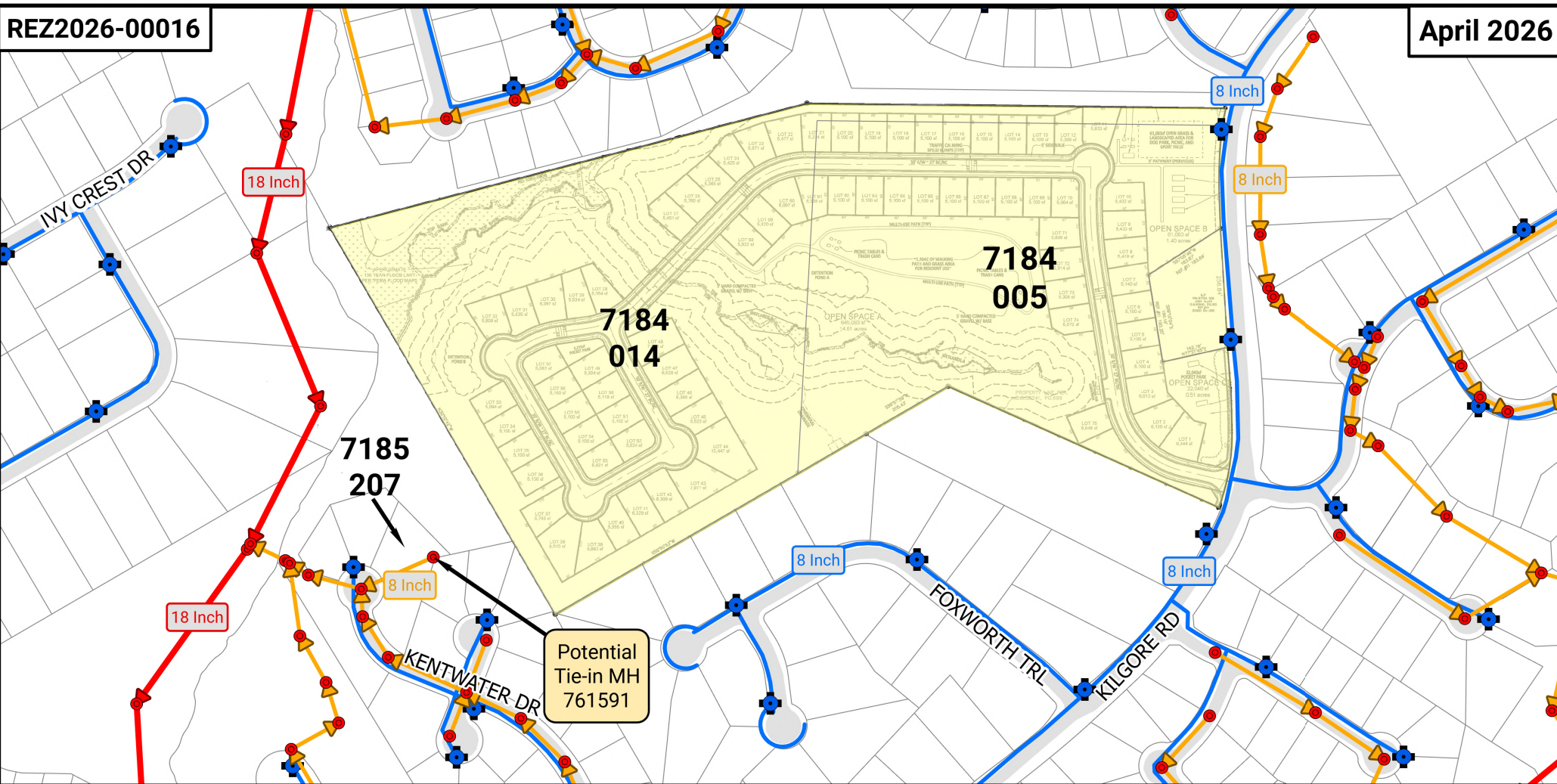


**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		
Department/Agency Name:	DWR	
Reviewer Name:	Lorraine Campagne	
Reviewer Title:	Section Manager	
Reviewer Email Address:	Lorraine.Campagne@gwinnettcounty.com	
Case Number:	REZ2026-00016	
Case Address:	2570 Kilgore Rd	
Comments:		<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO
1	Water: The proposed development may connect to an existing 8-inch water main located on the western right-of-way of Kilgore Road.	
2	Sewer: A Sewer Capacity Certification (C2025-187-07) was approved in July 2025 for 76 single family residences. The proposed development plans to connect to an existing 8-inch gravity sewer located on parcel 7185 207. Off-site easements will be required.	
3		
4		
5		
6		
7		
Recommended Zoning Conditions:		<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO
1		
2		
3		
4		
5		

Note: Attach additional pages, if needed

Revised 7/26/2021

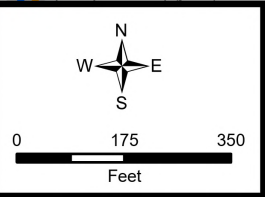


LEGEND

Flow Management	Hydrant	Sewer Collector
Pump Station	City	Sewer Interceptor
Regional	Water Main	Sewer Force Main
Manhole	Proposed Water Main	Proposed Sewer Collector

**2570 Kilgore Rd
RA-200 to OSC**

Water & Sewer Utility Map



Water Comments: The proposed development may connect to an existing 8-inch water main located on the western right-of-way of Kilgore Road.

Sewer Comments: A Sewer Capacity Certification (C2025-187-07) was approved in July 2025 for 76 single family residences. The proposed development plans to connect to an existing 8-inch gravity sewer located on parcel 7185 207. Off-site easements will be required.

Water Availability: Water demands imposed by the proposed development may require upsizing or extensions of existing water mains in order to meet Gwinnett County Standards and fire flow demands. Any cost associated with such required improvements will be the responsibility of the development. Current Gwinnett County Standards require a minimum of 12" pipe size for commercial developments and a minimum of 8" pipe size for residential developments. Additionally, connection to a minimum of 12" and 8" mains are required for commercial and residential developments, respectively. It is the responsibility of the developer's engineer to confirm pressure and volumes are available for the development.

Sewer Availability: A Sewer Capacity Certification must be obtained from Gwinnett County to confirm the existing system can serve the development. Sewer demands imposed by the proposed development may require upsizing and/or extensions of existing sewer mains, and/or upsizing of an existing pump station, and/or installation of a new pump station. Any cost associated with such required improvements will be the responsibility of the development. The developer shall provide easements for future sewer connection to all locations designated by Gwinnett County during plan review.

Water and Sewer Design and Construction Requirements: Extensions of the water and/or sanitary sewer systems within the subject development must conform to this department's policies, Gwinnett County's ordinances, and the Water Main and Sanitary Sewer Design and Construction Standards and Specifications, dated April 5th, 2016. Subsequent to design, construction, inspection, and final acceptance of the required utilities, service would then become available under the applicable utility permit rate schedules.

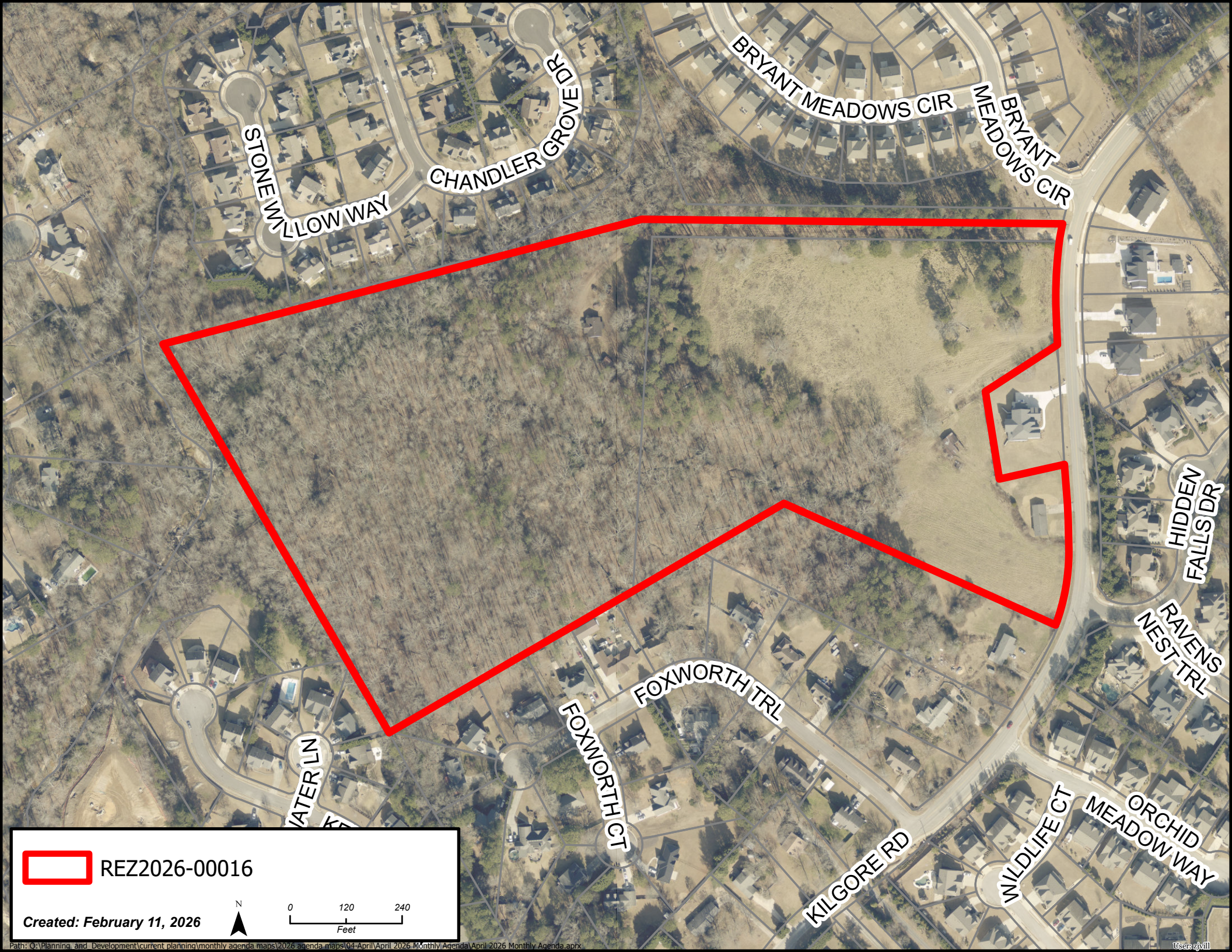
Private Road Developments: Any development with private roads must comply with the Standard Policy Requirement for the Installation of Water and Sanitary Sewer Mains within Private Developments. This policy stipulates minimum easement requirements and location of public mains and appurtenances, among other requirements.

Residential Rezoning Impact on Local Schools
Prepared for Gwinnett County BOC, April, 2026

											Proposed Zoning
	School	2025-26			2026-27			2027-28			Approximate Student Projections from Proposed Developments
		Forecast	Capacity	+/- Cap.	Forecast	Capacity	+/- Cap.	Forecast	Capacity	+/- Cap.	
	Dacula High School	2429	2,550	-121	2,477	2,550	-73	2,526	2,550	-24	5
REZ2026-00013 (ZON2025-00001)	Dacula Middle School	1921	1,900	21	1,959	1,900	59	1,948	1,900	48	4
	Alcova Elementary School	1496	1,150	346	1,525	1,150	375	1,556	1,150	406	7
	Grayson High School	3738	3,000	738	3,850	3,000	850	3,946	3,000	946	7
REZ2026-00014 (ZON2025-00091)	Bay Creek Middle School	1646	1,150	496	1,704	1,150	554	1,755	1,150	605	5
	Trip Elementary School	1,708	1,200	508	1,776	1,200	576	1,838	1,200	638	9
	Seckinger High School	2440	2,800	-360	2,485	2,800	-315	2,525	2,800	-275	18
REZ2026-00016 (ZON2025-00095)	Jones Middle School	1852	1,575	277	1,888	1,575	313	1,907	1,575	332	12
	Patrick Elementary School	1030	1,025	5	1,045	1,025	20	1,059	1,025	34	23
	Collins Hill High School	2420	2,625	-205	2,395	2,625	-230	2,372	2,625	-253	24
REZ2026-00017 (ZON2025-00113)	Creekland Middle School	1702	2,100	-398	1,720	2,100	-380	1,748	2,100	-352	16
	Walnut Grove Elementary School	821	1,200	-379	830	1,200	-370	818	1,200	-382	30

Exhibit K: Maps

[attached]



STONE MILL WAY

CHANDLER GROVE DR

BRYANT MEADOWS CIR

BRYANT MEADOWS CIR

WATER LN

FOXWORTH CT

FOXWORTH TRL

KILGORE RD

WILDLIFE CT

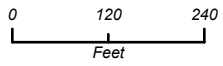
HIDDEN FALLS DR

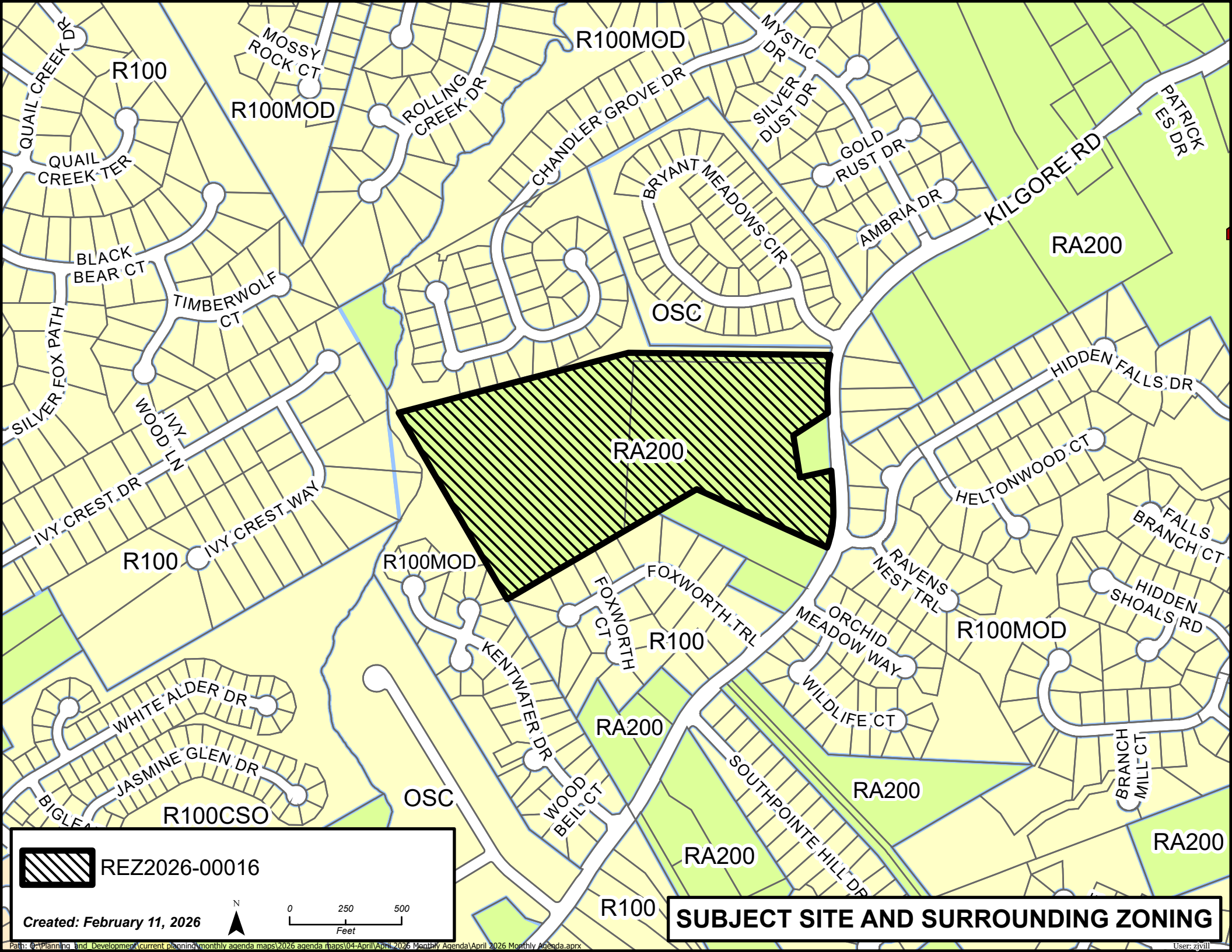
RAVENS NEST TRL


ORCHARD MEADOW WAY

 REZ2026-00016

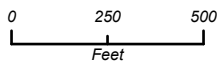
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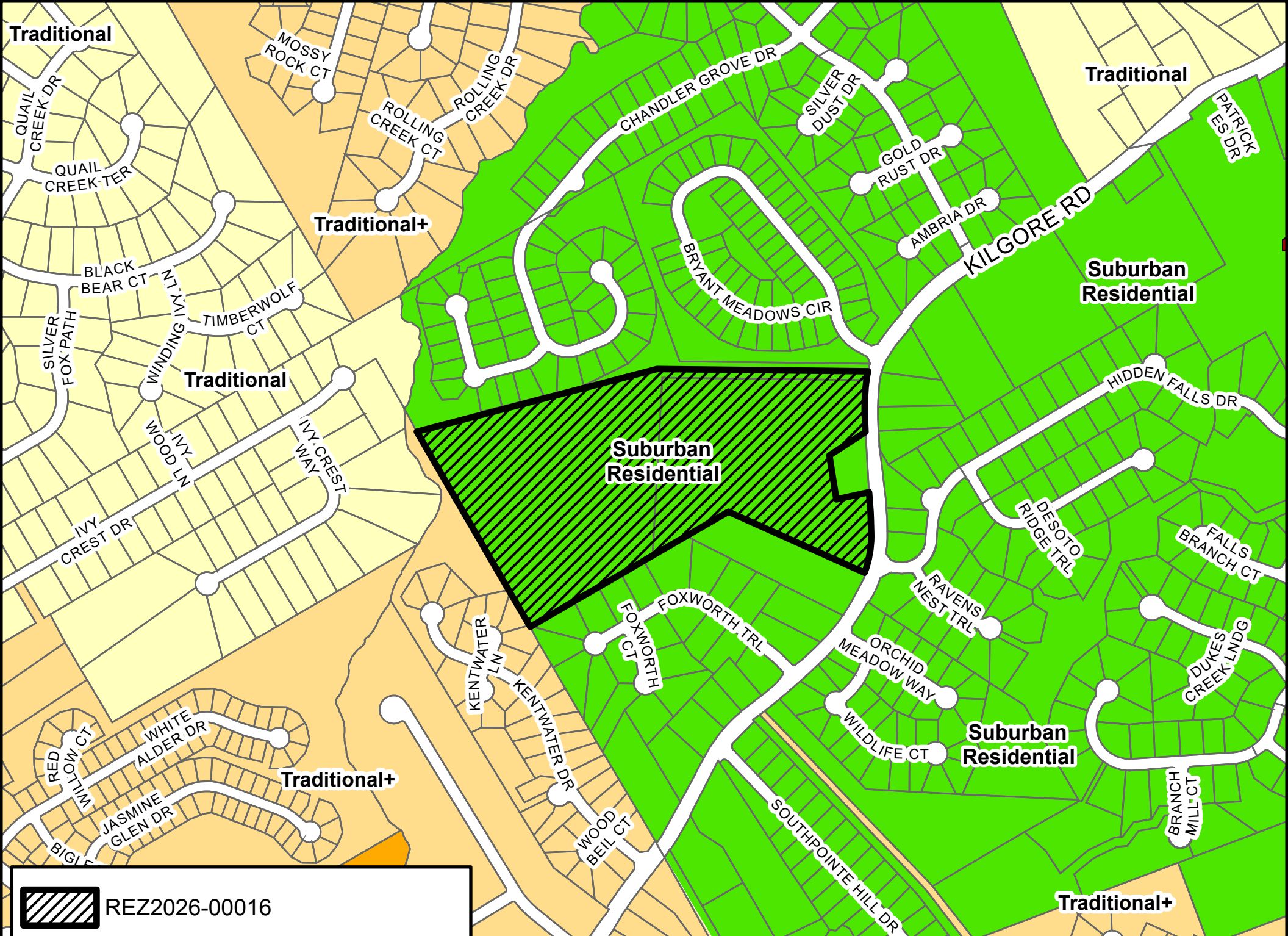



 REZ2026-00016

Created: February 11, 2026



SUBJECT SITE AND SURROUNDING ZONING



 REZ2026-00016

Created: February 11, 2026

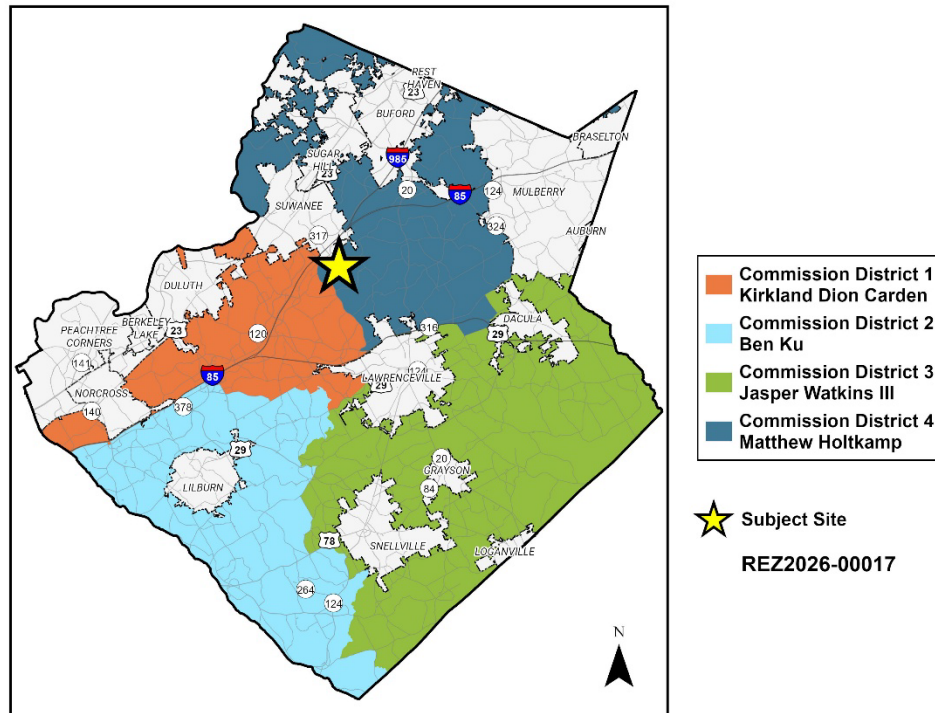
0 250 500 Feet

2045 UNIFIED PLAN FUTURE DEVELOPMENT CLASSIFICATION

PLANNING AND DEVELOPMENT DEPARTMENT CASE REPORT

Case Number: REZ2026-00017
Current Zoning: RA-200 (Agriculture-Residence District)
Request: Rezoning to **MRR** (Mid-Rise Residential Multifamily District)
Additional Request: Variance and Waiver
Address: 2730 McGinnis Ferry Road
Map Number: R7152 002
Site Area: 6.45
Units: 174
Proposed Development: Apartments
Commission District: District 4 – Commissioner Holtkamp
Future Development Type: Urban – Medium

Staff Recommendation: **APPROVAL WITH CONDITIONS**



Applicant: Balsam Green, LLC
c/o Mahaffey Pickens Tucker, LLP
1550 North Brown Road
Lawrenceville, GA 30043

Owner: Jerry Johnson et. al.
2535 Rock Springs Road
Buford, GA 30519

Contact: Shane Lanham

Phone: 770.232.0000

Zoning History

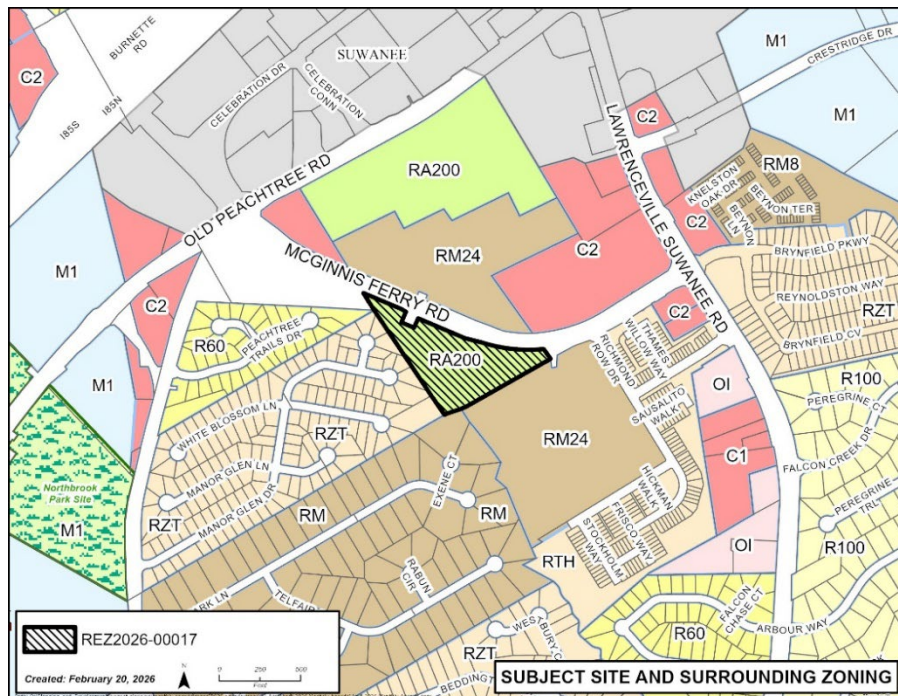
The subject property is zoned RA-200 (Agriculture-Residence District). In 1986, the property was rezoned from HS (Hospital Service District) to RA-200 for a place of worship, pursuant to RZ-85-86.

Existing Site Condition

The subject property is a 6.45-acre parcel located along McGinnis Ferry Road, east of its intersection with Old Peachtree Road. The site is undeveloped with dense vegetation, sloping downwards from the road frontage to a stream and floodplain in the western portion of the site. A 20-foot-wide sanitary sewer easement bisects the property. There is a curb cut, deceleration lane, and 5-foot-wide sidewalk along the road frontage. The nearest Ride Gwinnett transit stop is approximately 2.1 miles from the subject property.

Surrounding Use and Zoning

The site is surrounded by apartments, townhouses, single-family detached subdivisions, and commercial uses. The Stockton apartments are located to the north, across McGinnis Ferry Road. Richmond Row, a residential development containing apartments and townhouses, is to the east. Farther east at the intersection of McGinnis Ferry Road and Lawrenceville-Suwanee Road are commercial uses including a shopping center and medical office. The Jackson Square, Peachtree Manor, and Trails at Old Peachtree single-family detached subdivisions are located to the south and west. The following is a summary of surrounding uses and zoning:



Location	Land Use	Zoning	Density
Proposed	Apartments	MRR	26.99 units per acre
North	Apartments (The Stockton)	RM-24	24.00 units per acre
East	Apartments (Richmond Row) Townhouses (Richmond Row) Commercial (retail and office)	RM-24 R-TH C-2	18.65 units per acre 6.37 units per acre N/A
South	Single-Family Residential (Jackson Square)	RM	2.76 units per acre
West	Single-Family Residential (Peachtree Manor) Single-Family Residential (Trails at Old Peachtree)	RZT R-60	4.36 units per acre 3.68 units per acre

Project Summary

The applicant requests rezoning from RA-200 to MRR for apartments, including:

- A 5-story building with 174 units, yielding a floor area ratio (FAR) of 0.8.
- 35 one-bedroom, 124 two-bedroom, and 15 three-bedroom units, all restricted for households making up to 60% of the Area Median Income.
- Exterior building materials of brick, stone, and cementitious siding.
- 270 parking spaces, comprised of 226 spaces in the parking structure under the building, and 44 surface spaces located behind the building, including 6 EV charging spaces. 18 bicycle spaces are included inside the parking structure.
- 3.5 acres of open space, including 0.33 acres of usable open space around the building, comprised of a playground, pavilions, pocket parks, and a dog run. A fitness center is inside the building.
- Trash and recycling rooms within the parking structure, and a dumpster enclosure to the rear of the site.
- An underground stormwater facility beneath the surface parking lot.
- One full-access entrance with a deceleration lane along McGinnis Ferry Road, while maintaining the existing 5-foot-wide sidewalk.

Zoning and Development Standards

The following is a summary of applicable standards from the Unified Development Ordinance (UDO):

Standard	Required	Proposed	Meets Standard?
FAR	Maximum 1.0	0.8	YES
Lot Width	Minimum 100'	>100'	YES
Building Height	Maximum 75'	74'	YES
Open Space	Minimum 10% or 0.65 acres	54% or 3.5 acres	YES
Usable Open Space	Minimum 5% or 0.33 acres	5% or 0.33 acres	YES
Parking	Minimum 261 spaces Maximum 435 spaces	270 spaces	YES

Public Participation

The applicant held a community meeting on February 18, 2026, at the Home2 Suites in Lawrenceville. There were two community members in attendance, who raised questions regarding parking, dumpster location, retaining walls, and site disturbance. The applicant responded to these concerns by discussing the plan for trash pickup, the need for retaining walls, and that the western part of the site will remain undisturbed.

Staff Analysis

Rezoning Request Analysis: The standards governing the exercise of zoning power are outlined in Section 270-20.6 of the UDO. Staff makes the following findings related to the zoning requests:

A. Whether the proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

The surrounding area includes a mixture of apartments, townhouses, and commercial uses. The proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property.

B. Whether the proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property.

The property is adjacent to existing apartments along McGinnis Ferry Road. The zoning buffer and stream buffer along the western property line would provide separation and screening for existing single-family detached subdivisions. The proposed rezoning will not adversely affect the existing use or usability of adjacent or nearby property.

C. Whether the property to be affected by the proposed rezoning has a reasonable economic use as currently zoned.

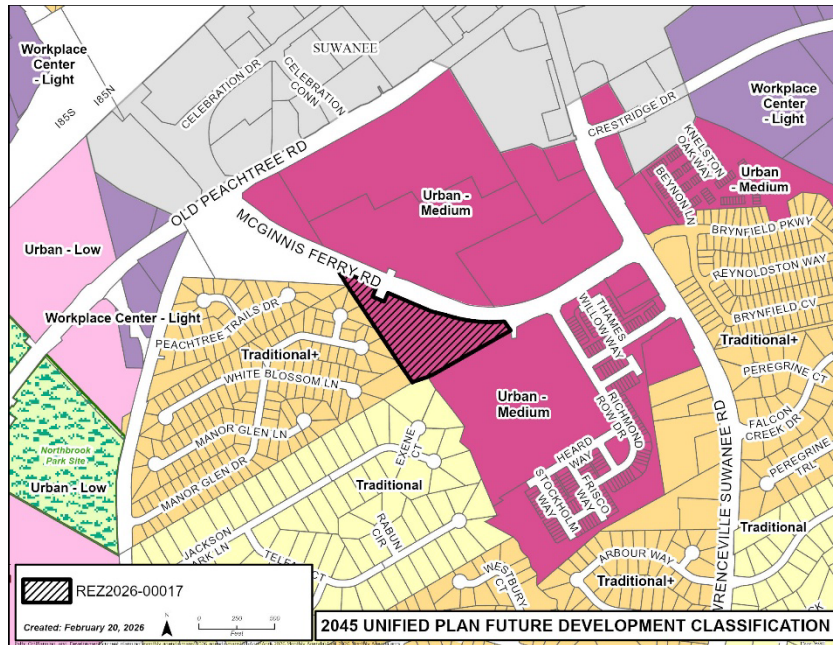
The property has a reasonable economic use as currently zoned.

D. Whether the proposed rezoning will result in a use which will or could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools.

An increase in impacts on public facilities would be anticipated in the form of traffic, utility demand, and stormwater runoff; however, these impacts could be mitigated with appropriate conditions, site development requirements, and planning. The traffic impact study estimates that the development would generate 65 morning and 68 evening peak hour trips. The study recommends restriping the deceleration lane and the U-turn lane. To ensure safe traffic movement along McGinnis Ferry Road, staff has recommended a condition requiring Stage 1 and Stage 2 Intersection Control Evaluation studies, and any recommended improvements. An increased impact is anticipated on school enrollment.

E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map; and

The Unified Plan's Future Development Map indicates "Urban – Medium" for the subject property, which envisions mixed-use development as a step-down from higher-intensity uses to lower-intensity uses, including single-family neighborhoods. Low-rise multifamily is a recommended land use and MRR is a recommended zoning district. The proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map.



F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning.

There are no other conditions giving support for approval or disapproval of the proposal.

Variance and Waiver Request and Analysis:

The applicant is seeking a variance and a waiver from the following provisions of the UDO:

1. **Section 240-70.1 B. Inter-parcel access.** The applicant is requesting not to provide inter-parcel access to the Richmond Row development to the east.
Analysis: There is a retaining wall proposed along the eastern property line due to existing topography. Approval of the variance would not nullify the intent of the UDO.
2. **Section 360-100 Multi-Use Path Requirements.** The applicant is requesting not to provide a 12-foot-wide multi-use path along McGinnis Ferry Road.
Analysis: A 5-foot-wide sidewalk exists along McGinnis Ferry Road. In addition, a 10-foot-wide multi-use path is located on the other side of McGinnis Ferry Road. Due to topographical constraints, approval of the waiver would not nullify the intent of the UDO.

Staff Recommendation

Based on the staff’s evaluation of the request and the standards governing the exercise of zoning power, the Department of Planning and Development recommends **APPROVAL WITH CONDITIONS** of the rezoning request and **APPROVAL** of the following variance and waiver:

1. To not provide inter-parcel access to the adjacent multifamily property to the east.

2. To not provide a 12-foot-wide multi-use path along McGinnis Ferry Road.

Staff Recommended Conditions

Approval as MRR (Mid-Rise Residential Multifamily District) for apartments, subject to the following conditions:

1. The development shall be limited to apartments not exceeding 174 units.
2. The proposed development shall be constructed in general conformance with Exhibit B: Site Plan, and Exhibit C: Elevations, dated received February 6, 2026, with revisions required by zoning conditions and the Unified Development Ordinance, subject to the review and approval of the Department of Planning and Development.
3. Usable open space shall be provided in general conformance with Exhibit B: Site Plan, dated received February 6, 2026, including, but not limited to, a playground, pavilions, pocket parks, and a dog run, subject to the review and approval of the Department of Planning and Development.
4. The following roadway improvements shall be provided in general conformance with Exhibit B: Site Plan, dated received February 6, 2026, prior to the issuance of a Certificate of Occupancy, subject to the review and approval of the Gwinnett County Department of Transportation:
 - a. An entrance, deceleration lane and a 5-foot-wide sidewalk along McGinnis Ferry Road.
 - b. Restriping of the deceleration lane and U-turn lane along McGinnis Ferry Road.
5. Stage 1 and Stage 2 Intersection Control Evaluations (ICE) studies shall be submitted to the Gwinnett County Department of Transportation, prior to the issuance of a land disturbance permit, and an R-cut or and/or right-in/right-out design shall be considered. Recommendations in the ICE studies shall be constructed, prior to the issuance of a Certificate of Occupancy, subject to review and approval of the Gwinnett County Department of Transportation.

Exhibits:

- A. Site Visit Photos
- B. Site Plan
- C. Building Elevations
- D. Letter of Intent and Applicant's Response to Standards
- E. Application and Disclosure of Campaign Contributions
- F. DWR Sewer Capacity Certification Letter
- G. Traffic Impact Study
- H. Public Participation Plan and Report
- I. Internal and External Agency Review Comments
- J. Maps

Exhibit A: Site Visit Photos



View of site frontage and shoulder along McGinnis Ferry Road



View of McGinnis Ferry Road at proposed access point



View of topography and the stream on the site



View of sewer easement area on the site



View of Stockton Suwanee apartments



View of Richmond Row apartments

Exhibit B: Site Plan

[attached]

RECEIVED

2.6.2026

SITE INFORMATION:

ADDRESS: 2730 McGinnis Ferry Road, Suwanee, GA 30024

PARCEL ID: 7152 002

AREA: 6.447 +/- ACRE

ZONED : RA-200

RECOMMENDED :MRR

DENSITY: 26.99 DU / ACRE

SETBACK: 0' FRONT, 0' SIDE, 0' BACK

FAR: 0.8

OPEN SPACE

10% MINIMUM REQUIRED

PROVIDED 3.5 ACRE (28,082 SF)

54 % PROVIDED

USABLE OPEN SPACE

5% MINIMUM REQUIRED (14,041 SF)

5 % PROVIDED (14,500 SF)

COVERED PAVILION/ BARBECUE -PICNIC 2,650SF

COVERED EXTERIOR GATHERING 1,460 SF

DOGRUN 1,320 SF

TOTLOT 1,000 SF

PARK 8,070 SF

BUILDING 1:

5-STORY APARTMENTS

SQUARE FOOTAGE: 213,520 SQFT

ALLOWABLE HEIGHT (MRR): 75'

HEIGHT: 74'

PARKING SPACES: 270

MAX. PARKING REQUIRED 2.5/ APT UNIT

MIN. PARKING REQUIRED 1.5/ APT UNIT

LEVEL -1: 114

LEVEL -2: 112

SURFACE: 44

PARKING RATIO: 1.55/ APT. UNIT

EV CHARGING SPACE

1 FOR 50 PARKING REQUIRED

6 PROVIDED

BIKE PARKING:

12 BIKE PARKING REQUIRED 1/15 APT. UNITS

18 BIKE PARKING PROVIDED

UNIT MIX

1 BR= 35 20%

2 BR= 124 71%

3 BR= 15 9%

TOTAL= 174

Scale: 1 inch = 100 feet



SITE PLAN KEY

- 1 PEDESTRIAN ENTRANCE
- 2 VEHICLE ENTRANCE
- 3 GARAGE ENTRANCE
- 4 TOT LOT
- 5 COVERED PAVILION / BARBECUE -PICNIC AREA
- 6 COVERED EXTERIOR GATHERING AREA
- 7 PARK
- 8 LOADING AREA
- 9 TRASH ENCLOSURE
- 10 UNDERGROUND STORMWATER MANAGEMENT SYSTEM
- 11 EV STATION
- 12 DOGRUN
- 13 CLUBROOM
- 14 COMPUTER ROOM
- 15 FITNESS AREA
- 16 BIKE PARKING
- 17 TRASH ROOM
- 18 RECYCLE ROOM
- 19 MONUMENT SIGN

- OPEN SPACE
- USABLE OPEN SPACE



RETAINING WALL
WALL HEIGHT AVERAGE: 15'
WALL HEIGHT-MAX: 20'
WALL MATERIAL: MSE MODULAR BLOCK
ALL WALL INFORMATION SHOWN IS CONCEPTUAL AND PROVIDED FOR REFERENCE ONLY. FINAL WALL DESIGN, INCLUDING HEIGHT, MATERIALS, AND STRUCTURAL DETAILS, WILL BE DEVELOPED DURING THE PROJECT'S DESIGN PHASE.

1 Site Plan
1" = 100'-0"



SITE PLAN

Balsam Green - Gwinnett County GA

RECEIVED

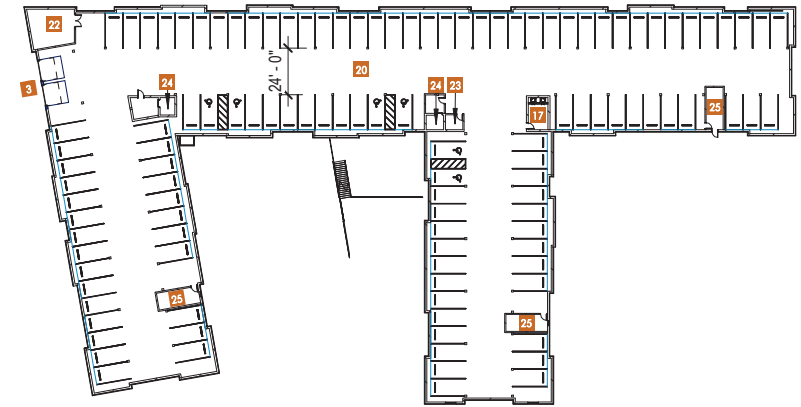
2.6.2026

FLOOR PLAN KEY

- 1 PEDESTRIAN ENTRANCE
- 3 GARAGE ENTRANCE
- 16 BIKE PARKING
- 17 TRASH ROOM
- 18 RECYCLE ROOM
- 20 PARKING
- 21 MECHANICAL
- 22 ELECTRICAL
- 23 STORAGE
- 24 ELEVATOR
- 25 STAIR



① Level -2
1/64" = 1'-0"



② Level -1
1/64" = 1'-0"

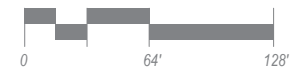


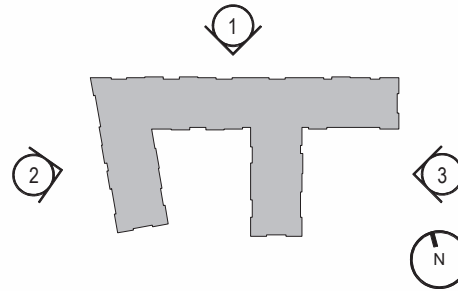
Exhibit C: Building Elevations

[attached]

RECEIVED

2.6.2026

BALCONY:
25 % UNIT REQUIRED
45 PROVIDED (4'X6' SIZE)



EXTERIOR MATERIALS

Material Mark	Description
4.0	04-Stone-Cultured Stone-Cobblefield-Chardonnay
4.1	04-Stone-Cultured Stone-Cobblefield-Texas_Cream
4.2	Brick-SiouxCity-Running-VintageBlackVelour
7.0	07-Siding-JamesHardie-Panel-Smooth-ArcticWhite
7.1	07-Siding-JamesHardie-Lap6Reveal-Smooth-ArcticWhite
7.2	07-Siding-JamesHardie-Lap6Reveal-Smooth-NavajoBeige
7.3	07-Siding-JamesHardie-Lap6Reveal-Smooth-MontereyTaupe
7.4	Asphalt Shingles



① North Elevation
1/32" = 1'-0"



② West Elevation
1/32" = 1'-0"



③ East Elevation
1/32" = 1'-0"

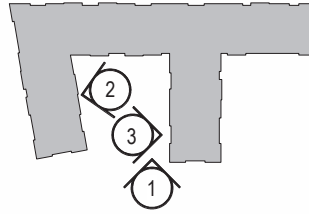
EXTERIOR ELEVATIONS

Balsam Green - Gwinnett County GA

RECEIVED

2.6.2026

BALCONY:
25 % UNIT REQUIRED
45 PROVIDED (4'X6' SIZE)

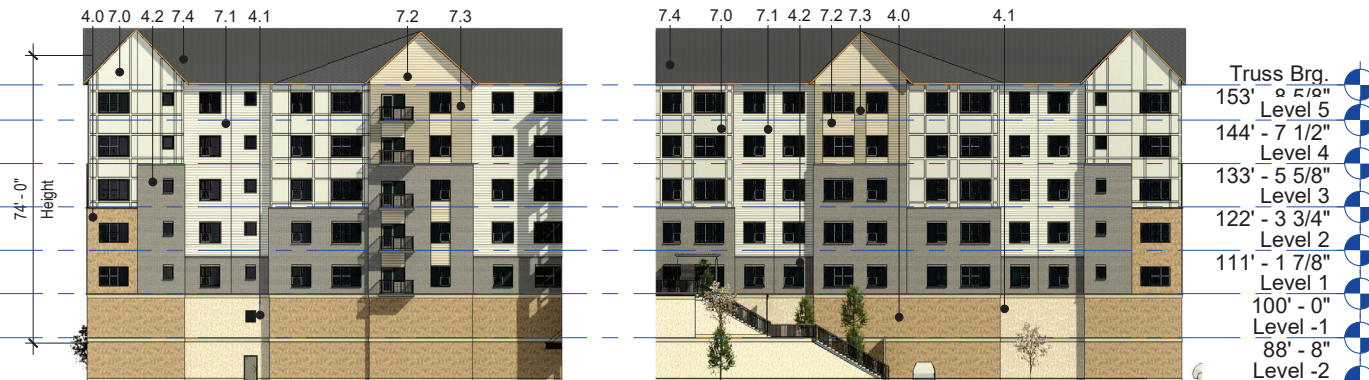


EXTERIOR MATERIALS

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7.1	07-Siding-JamesHardie-Lap6Reveal-Smooth-ArcticWhite
7.2	07-Siding-JamesHardie-Lap6Reveal-Smooth-NavajoBeige
7.3	07-Siding-JamesHardie-Lap6Reveal-Smooth-MontereyTaupe
7.4	Asphalt Shingles



① South Elevation
1/32" = 1'-0"



② West Elevation - Courtyard
1/32" = 1'-0"

③ East Elevation - Courtyard
1/32" = 1'-0"

EXTERIOR ELEVATIONS

Balsam Green - Gwinnett County GA



Exhibit D: Letter of Intent and Applicant's Response to Standards

[attached]

David Belle Isle
Matthew P. Benson
Catherine W. Davidson
Gerald Davidson, Jr.*
Brian T. Easley
Rebecca B. Gober
John D. Hipes*
Christopher D. Holbrook
Jessica P. Kelly
Shane M. Lanham

Jeffrey R. Mahaffey
John N. Mahaffey
Julia A. Maxwell
Steven A. Pickens
Kassie M. Prater
Jack M. Ryan
Gabrielle H. Schaller
S. Tess Shaheen
Andrew D. Stancil
R. Lee Tucker, Jr.

*Of Counsel

**LETTER OF INTENT FOR
REZONING APPLICATION OF BALSAM GREEN, LLC**

Mahaffey Pickens Tucker, LLP submits this Letter of Intent and attached Rezoning application (the “Application”) on behalf of Balsam Green, LLC (the “Applicant”) relative to an approximately 6.45-acre tract of land (the “Property”) located on McGinnis Ferry Road near its intersection with Lawrenceville-Suwanee Road to the East and its intersection with Old Peachtree Road to the West further identified as Tax Parcel Number R7152 002. The Property is currently zoned RA-200 and is located within the Urban-Medium Future Development Map Area as set forth in the Gwinnett County 2045 Unified Plan (the “2045 Plan”).

The Applicant proposes to rezone the Property to the MRR zoning classification to permit the development of a 174-unit multifamily residential community. The maximum floor area ratio (FAR) is 0.8. The proposed development consists of a single building fronting McGinnis Ferry Road, with all parking located underneath the structure or to the rear. This design not only preserves a clean and attractive streetscape, but also minimizes visual impacts by fully screening parking from public view. Residents would benefit from an array of meaningful on-site amenities—including a fitness center, computer room, dog run, and playground—designed to support a strong sense of community and enhance quality of life. Outdoor gathering spaces, including a covered pavilion with barbecue and picnic facilities, further promote a safe, vibrant, and active residential environment.

The proposed development is not only compatible with surrounding zoning and land uses—it represents an ideal fit. The Property is bordered by C-2 zoning to the northeast, RM-24 to the north and south, and RZT zoning to the west, and is located within 500 feet of the existing Richmond Row multifamily community. This project provides a logical and appropriate land-use transition between the more intensive commercial uses to the northeast and the established residential neighborhoods to the west.

The 2045 Plan sets forth overarching policy goals that encourage residential development and diversifying housing types. The 2045 Plan specifically encourages the land uses of “Multifamily – Mid Rise” in the Urban-High Future Development Map Area and lists “MRR” as a “recommended” zoning classification. The proposed development would also expand access to affordable and workforce housing as the community would be income- and rent-restricted at 60% of Area Median Income or less. As set forth in the submitted building elevations, the proposed development would include attractive architectural design and building materials including fiber-cement siding, brick, and stone veneer.

In order to develop the Property as depicted on the site plan submitted with the Application, the Applicant respectfully requests the additional relief described below.

- A variance from UDO §240-70.1(B) to allow the proposed development to be constructed without providing interparcel access to adjacent properties. The nature of the adjacent developments and the location of a stream along the western border of the Property make providing interparcel access impractical.
- A waiver from UDO §360-100 to allow the existing 5-foot wide sidewalk along McGinnis Ferry Road to remain in lieu of constructing a new 12-foot wide multi-use trail. The UDO requires that "Multi-use paths shall be provided along all arterial streets." A multi-use path is already provided on the other side of McGinnis Ferry Road. Although it is the Applicant's position that this UDO section is satisfied, the waiver is requested out of an abundance of caution. Moreover, the property has unusual topographical and other exceptional conditions which frustrate the construction of a multi-use path.

The Applicant and its representatives welcome the opportunity to meet with staff of the Gwinnett County Planning and Development Department to answer any questions or address any

concerns relating to the matters set forth in this letter or in the Application. The Applicant respectfully requests your approval of the Application.

This 19th day of March, 2026.

Respectfully submitted,

MAHAFFEY PICKENS TUCKER, LLP

Julia A. Maxwell

Julia A. Maxwell

Attorneys for Applicant

JUSTIFICATION FOR REZONING

The portions of the Gwinnett County Unified Development Ordinance (the “UDO”) which classify or may classify the property which is the subject of this Application (the “Property”) into any less intensive zoning classification other than as requested by the Applicant, are or would be unconstitutional in that they would destroy the Applicant's property rights without first paying fair, adequate and just compensation for such rights, in violation of Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States.

The application of the UDO as applied to the subject Property, which restricts its use to the present zoning classification, is unconstitutional, illegal, null and void, constituting a taking of the Applicant's and the Owner’s property in violation of the Just Compensation Clause of the Fifth Amendment and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States, Article I, Section I, Paragraph I, and Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Equal Protection Clause of the Fourteenth Amendment to the Constitution of the United States denying the Applicant an economically viable use of its land while not substantially advancing legitimate state interests.

The Property is presently suitable for development under the MRR classification as requested by the Applicant, and is not economically suitable for development under the present RA-200 zoning classification of Gwinnett County. A denial of this Application would constitute an arbitrary and capricious act by the Gwinnett County Board of Commissioners without any rational basis therefore, constituting an abuse of discretion in violation of Article I, Section I, Paragraph I and Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983, and the Due Process Clause of the Fourteenth Amendment to the Constitution of the United States.

A refusal by the Gwinnett County Board of Commissioners to rezone the Property to the MRR classification with such conditions as agreed to by the Applicant, so as to permit the only feasible economic use of the Property, would be unconstitutional and discriminate in an arbitrary, capricious and unreasonable manner between the Applicant and owners of similarly situated

property in violation of Article I, Section I, Paragraph II of the Constitution of the State of Georgia of 1983 and the Equal Protection Clause of the Fourteenth Amendment to the Constitution of the United States. Any rezoning of the subject Property to the MRR classification, subject to conditions which are different from the conditions by which the Applicant may amend its application, to the extent such different conditions would have the effect of further restricting the Applicant's and the Owner's utilization of the subject Property, would also constitute an arbitrary, capricious and discriminatory act in zoning the Property to an unconstitutional classification and would likewise violate each of the provisions of the State and Federal Constitutions set forth hereinabove.

Opponents to the request set forth in the Application, or in any amendments to the Application, have waived their rights to appeal any decision of the Gwinnett County Board of Commissioners because they lack standing, have failed to exhaust administrative remedies, and/or because they failed to assert any legal or constitutional objections.

Accordingly, the Applicant respectfully requests that the rezoning application submitted by the Applicant relative to the Property be granted and that the Property be rezoned to the zoning classification as shown on the respective application.

This 2nd day of February, 2026.

Respectfully submitted,

MAHAFFEY PICKENS TUCKER, LLP

Julia A. Maxwell

Julia A. Maxwell
Attorneys for Applicant

1550 North Brown Road
Suite 125
Lawrenceville, Georgia 30043
(770) 232-0000

Pursuant to requirements of the United Development Ordinance, the Board of Commissioners finds that the following standards are relevant in balancing the interest in promoting the public health, safety, morality, or general welfare against the right to the unrestricted use of property and shall govern the exercise of the zoning power.

- A. Whether a proposed rezoning will permit a use that is suitable in view of the use and development of adjacent and nearby property:
Yes, the proposed request will permit a use that is suitable in view of the use and development of adjacent and nearby property.
- B. Whether a proposed rezoning will adversely affect the existing use or usability of adjacent or nearby property:
No, the proposed request will not adversely affect the existing use or usability of adjacent or nearby property. The proposed request is compatible with surrounding land uses.
- C. Whether the property to be affected by a proposed rezoning has reasonable economic use as currently zoned:
The Applicant submits that the property to be affected by the proposed request does not have a reasonable economic use as currently zoned.
- D. Whether the proposed rezoning will result in a use which could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools:
The proposed request will not result in a use which will or could cause an excessive or burdensome use of existing streets, transportation facilities, utilities, or schools.
- E. Whether the proposed rezoning is in conformity with the policy and intent of the Unified Plan and Future Development Map:
The proposed request is in conformity with the policy and intent of the Unified Plan and Future Development Map. The property is located in the Urban-Medium future development area which specifically encourages mid-rise apartments and lists MRR as a recommended zoning district.
- F. Whether there are other existing or changing conditions affecting the use and development of the property which give supporting grounds for either approval or disapproval of the proposed rezoning:
The proposed request is supported by the pattern of development in the surrounding area as well as the Unified Plan and Future Development Map designation of the property as recommended for mid-rise apartments.

Exhibit E: Application and Disclosure of Campaign Contributions

[attached]

REZONING APPLICATION

AN APPLICATION TO AMEND THE OFFICIAL ZONING MAP OF GWINNETT COUNTY, GA.

APPLICANT INFORMATION	PROPERTY OWNER INFORMATION
Name: <u>Balsam Green, LLC c/o Mahaffey Pickens Tucker, LLP</u> Address: <u>1550 North Brown Road, Suite 125</u> City: <u>Lawrenceville</u> State: <u>GA</u> ZIP: <u>30043</u> Phone: <u>770.232.0000</u> Email: <u>slanham@mptlawfirm.com</u>	Name: <u>Jerry C. Johnson et al</u> Address: <u>2535 Rock Springs Road</u> City: <u>Buford</u> State: <u>GA</u> ZIP: <u>30519</u> Phone: <u>770.232.0000</u> Email: <u>slanham@mptlawfirm.com</u>
Contact Person: <u>Shane Lanham</u> Phone: <u>770.232.0000</u> Contact's Email: <u>slanham@mptlawfirm.com</u>	
APPLICANT IS THE:	
<input type="checkbox"/> Owner's Agent <input type="checkbox"/> Property Owner <input checked="" type="checkbox"/> Contract Purchaser	
Current Zoning District(s): <u>RA-200</u> Requested Zoning District: <u>MRR</u> Parcel Number(s): <u>7152 002</u> Acreage: <u>6.45</u> Property Address(es): <u>2730 McGinnis Ferry Road</u> Proposed Development: <u>Apartments</u> Variance(s): <u>Sec. 240-70.1 B.</u> Waiver(s): <u>N/A</u>	
RESIDENTIAL DEVELOPMENT No. of Dwelling Units: <u>174</u> Dwelling Unit Sq. Ft.: <u>N/A</u> Density: <u>26.99 du/ac</u> Floor Area Ratio (LRR, MRR, HRR): <u>0.8</u>	NON-RESIDENTIAL DEVELOPMENT No. of Buildings: _____ Total Building Sq. Ft.: _____ Floor Area Ratio: _____
MIXED-USE DEVELOPMENT	
No. of Dwelling Units: _____ Dwelling Unit Sq. Ft.: _____ Total Non-Residential Sq. Ft.: _____ Floor Area Ratio: _____	



GWINNETT COUNTY
PLANNING AND DEVELOPMENT
PROPERTY OWNER CERTIFICATION

The undersigned below, or as attached, is the owner of the property considered in this application. The undersigned is aware that no application or reapplication affecting the same land shall be acted upon in less than six (6) months from the date of last action by the Board of Commissioners.

{ aware that no application or reapplication affecting the same land shall be acted upon in less than six (6) months from the date of last action by the Board of Commissioners.
Property Owner

Name: Terry C Johnson

Signature: Terry C. Johnson

Date: 12-04-25

Notary Public

Name: Notary - Kathy Fincher

Signature: Kathy Fincher

Date: 12-4-25



Notary Seal



Disclosure of campaign contributions aggregating to \$250.00 or more to a member of the Board of Commissioners is required pursuant to Section 270-25.E. of the Unified Development Ordinance.

Name: Kassie Prater

Relationship to Application: Applicant Representative of Applicant

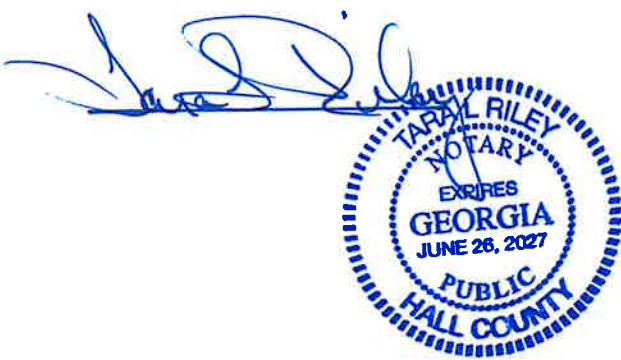
Campaign Contributions? Yes No

Contributions:

Official Name	Contribution Amount	Date of Contribution	Contributor
See Attached			

Signature: *Kassie Prater*

Date: 3/24/20



**CONFLICT OF INTEREST CERTIFICATION FOR REZONING/CHANGE IN
CONDITIONS AND SPECIAL USE PERMIT APPLICATIONS**

MAHAFFEY PICKENS TUCKER, LLP

<u>NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL</u>	<u>CONTRIBUTION</u>	<u>DATE</u>
Nicole Love Hendrickson, Chairwoman	\$3,300	10/01/2024
Ben Ku, Commissioner	\$2,000	04/25/2025
Matthew Holtkamp, Commissioner	\$2,000	06/19/2025
Ben Ku, Commissioner	\$1,300	02/03/2026

Shane M. Lanham, Attorney at Law, PC

<u>NAME AND OFFICIAL POSITION OF GOVERNMENT OFFICIAL</u>	<u>CONTRIBUTION</u>	<u>DATE</u>
Nicole Love Hendrickson, Chairwoman	\$500.00	10/02/2024

Updated 08/29/2025



GWINNETT COUNTY
PLANNING AND DEVELOPMENT | PLANNING DIVISION
DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

Disclosure of campaign contributions aggregating to \$250.00 or more to a member of the Board of Commissioners is required pursuant to Section 270-25.E. of the Unified Development Ordinance.

Name: Mark Lavery

Relationship to Application: Applicant Representative of Applicant

Campaign Contributions? Yes No

Contributions:

Official Name	Contribution Amount	Date of Contribution	Contributor

Signed by:
Signature: Mark Lavery
445884DEF744CD...

Date: 1/28/2026

Exhibit F: DWR Sewer Capacity Certification Letter

[attached]



October 13, 2025

Matt Hodges
Balsam Green
303 Perimeter Center North, Suite 300
Atlanta, GA 30346

<input type="checkbox"/>	APPROVED
<input type="checkbox"/>	DENIED
<input checked="" type="checkbox"/>	CONDITIONALLY APPROVED
Sewer Capacity Request #C2025-268-10	
Expiration Date: 10/13/2026	
Tie-In Manhole FID: 205423	

RE: Sewer Availability for Proposed Development – Balsam Green McGinnis Ferry
Parcel ID 7152 002

Dear Matt Hodges:

Gwinnett County Department of Water Resources (GCDWR) has reviewed the impact of the proposed development consisting of 180 apartment units (35 one-bedrooms, 125 two-bedrooms, and 20 three-bedrooms), leasing office, and clubhouse with a pool on the above parcel and determined:

Gwinnett County can provide sewer service for the proposed development only upon completion of the following conditions:

- Execution of a Developer Participation Agreement (DPA) between Balsam Green and Gwinnett County for the installation of two larger pumps at the Lawrenceville-Suwanee Pump Station adequate to accommodate the proposed development.

This confirmation is based on your anticipated annual average daily flow of 20 gpm discharging to the sewer tie-in manhole at Facility ID **205423**.

Capacity Allocation: Sewer capacities are allocated only upon the issuance of a Development Permit, by Gwinnett County Planning & Development and after all applicable conditions established in this Sewer Capacity Certification, if any, are met.

Certification Expiration: The Sewer Capacity Certification is valid for 12 months from the date of this letter. If rezoning is approved, GCDWR will issue a revised Sewer Capacity Certification based on the approved units, as needed. The capacity request can be renewed for an additional 12 months. If a Development Permit has not been obtained by the expiration date above the developer may apply for renewal by providing evidence of active rezoning, plan review, or permitting efforts.

Once the Sewer Capacity Certification expires, the capacity is no longer allocated, and a new Sewer Capacity Certification request must be submitted for re-evaluation with no guarantee of



available capacity. The evaluation will be based on the updated conditions of the sewer system.

Certification Invalidation: This certification is not valid if there are proposed changes to your development that could impact downstream sewer capacity (e.g. land use density, sewer tie-in manhole, parcel changes, and/or rezoning). In this case the development must resubmit for a new Sewer Capacity Request with updated information prior to the expiration of the previous one.

Please contact us at 678-376-7026 if you have any questions.

Sincerely,

GWINNETT COUNTY DEPARTMENT OF WATER RESOURCES

Della Taylor, PE
Division Director, Infrastructure Support
678.376.2130

C: Gwinnett County DWR – Lorraine Campagne, Section Manager; Raghu Vemuru, Engineer V; Mikala Weston, Engineer III; Geniva Sylvain, Engineer III
Gwinnett County P&D – Charli Young, Planning Manager; Christina DeJarnette, Engineer II; Wendell Tumale Engineer III

Exhibit G: Traffic Impact Study

[attached]



Traffic Impact Study

Suwanee Residential

Gwinnett County, Georgia

Report Prepared:

September 2025

Report Updated:

January 15, 2026

Prepared for:

Balsam Green, Inc.

Prepared by:

Kimley»»Horn

Kimley-Horn and Associates, Inc.
11720 Amber Park Drive, Suite 600
Alpharetta, GA 30009
017304003

REZ2026-00017

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BF

Traffic Impact Study

Suwanee Residential

Gwinnett County, Georgia

Report Prepared:

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Prepared for:

Balsam Green, Inc.



Prepared by:

Kimley»»Horn

Kimley-Horn and Associates, Inc.
11720 Amber Park Drive, Suite 600
Alpharetta, GA 30009
017304003
REZ2026-00017

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- Appendix D: Raw Traffic Counts
- Appendix E: Trip Generation

1.0 INTRODUCTION

This report presents the analysis of the anticipated traffic impacts associated with the *Suwanee Residential* development, which is expected to be completed in 2027 (referred to herein as “build-out year”). This study evaluates the impact of constructing 174 multifamily units. The approximate 6.4-acre site is located south of McGinnis Ferry Road and west of Richmond Row Drive adjacent to the City of Suwanee in Gwinnett County, Georgia.

The report will summarize the analysis of the following three (3) scenarios:

1. Existing 2025 Conditions
2. Projected 2027 No-Build Conditions (Existing 2025 Conditions, plus background traffic growth)
3. Projected 2027 Build Conditions (Projected 2027 No-Build Conditions, plus traffic associated with the proposed *Suwanee Residential* development)

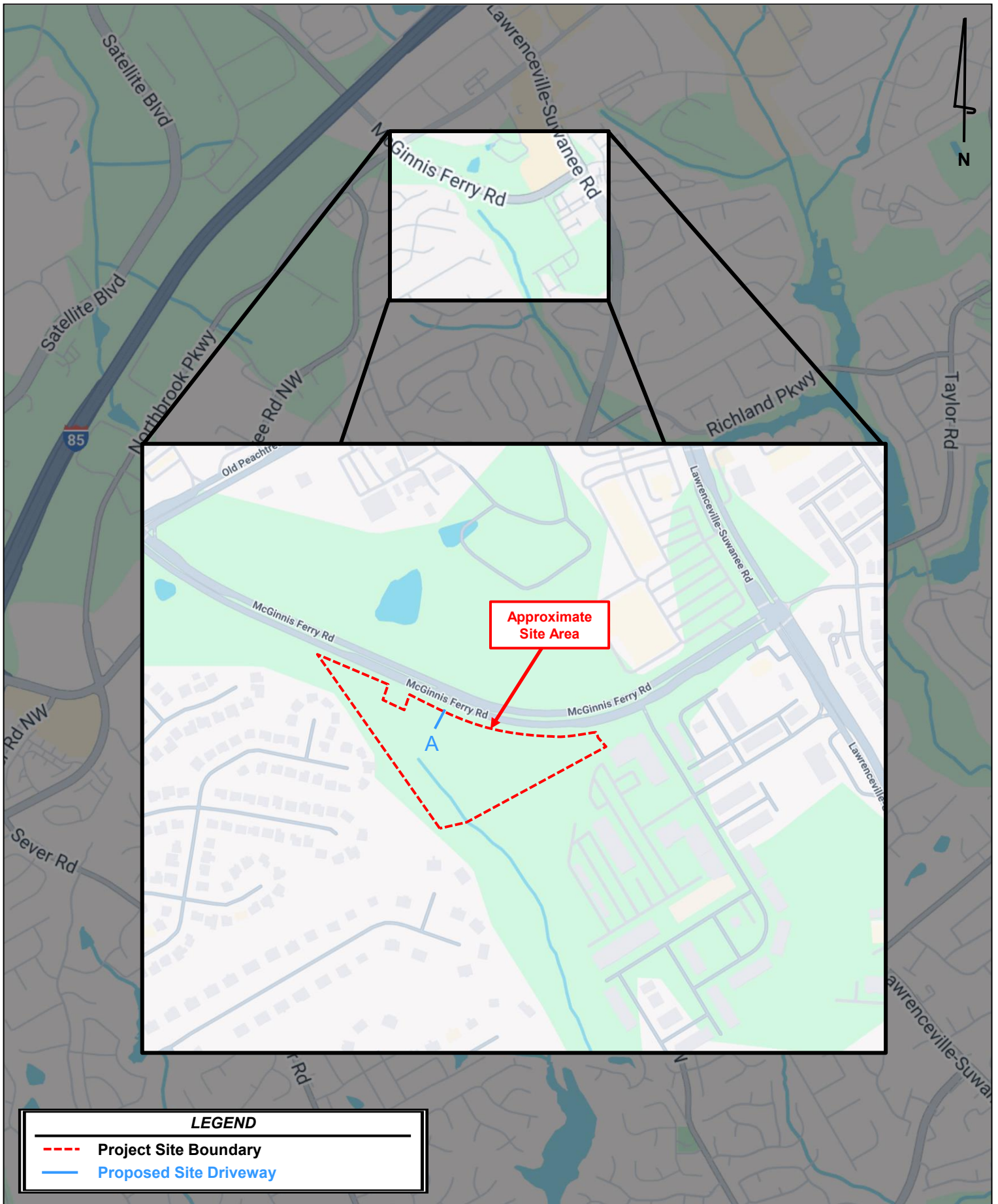
Figure 1 provides a location map of the project site. **Figure 2** provides an aerial image of the project site and study network. Additionally, a copy of the proposed site plan is provided in **Appendix A**.

2.0 STUDY AREA DETERMINATION

The study area consists of the following three (3) existing study intersections:

1. Old Peachtree Road at McGinnis Ferry Road (signalized)
2. McGinnis Ferry Road at proposed Site Driveway A (unsignalized)
3. Lawrenceville-Suwanee Road (SR 317) at McGinnis Ferry Road/Brynfield Parkway (signalized)

For the purposes of this traffic impact study, Old Peachtree Road and Lawrenceville-Suwanee Road are considered to have a north-south orientation. McGinnis Ferry Road and Brynfield Parkway are considered to have an east-west orientation.



LEGEND

- - - Project Site Boundary
— Proposed Site Driveway



LEGEND	
---	Project Site Boundary
---	Proposed Site Driveway
x	Study Intersection

3.0 EXISTING TRAFFIC CONDITIONS

3.1 ROADWAY CHARACTERISTICS

The roadways within the study network have the following characteristics:

McGinnis Ferry Road is a four-lane divided minor arterial roadway (GDOT Functional Classification) and major arterial roadway (GCDOT Functional Classification) with two lanes in each direction. The roadway has a posted speed limit of 45 MPH. GDOT counts taken west of Buford Highway indicated an AADT of 37,600 vehicles per day in 2024.

Old Peachtree Road is a four-lane divided major collector roadway (GDOT Functional Classification) and major arterial roadway (GCDOT Functional Classification) with two lanes in each direction. The roadway has a posted speed limit of 45 MPH north of McGinnis Ferry Road and a posted speed limit of 40 MPH south of McGinnis Ferry Road. GDOT counts taken south of McGinnis Ferry Road indicated an AADT of 16,300 vehicles per day in 2024.

Lawrenceville-Suwanee Road is a four-lane divided minor arterial roadway (GDOT Functional Classification) and major arterial roadway (GCDOT Functional Classification) with two lanes in each direction and a posted speed limit of 45 MPH. GDOT counts taken west of Sawmill Drive indicated an AADT of 50,500 vehicles per day in 2024.

3.2 EXISTING TRAFFIC VOLUMES

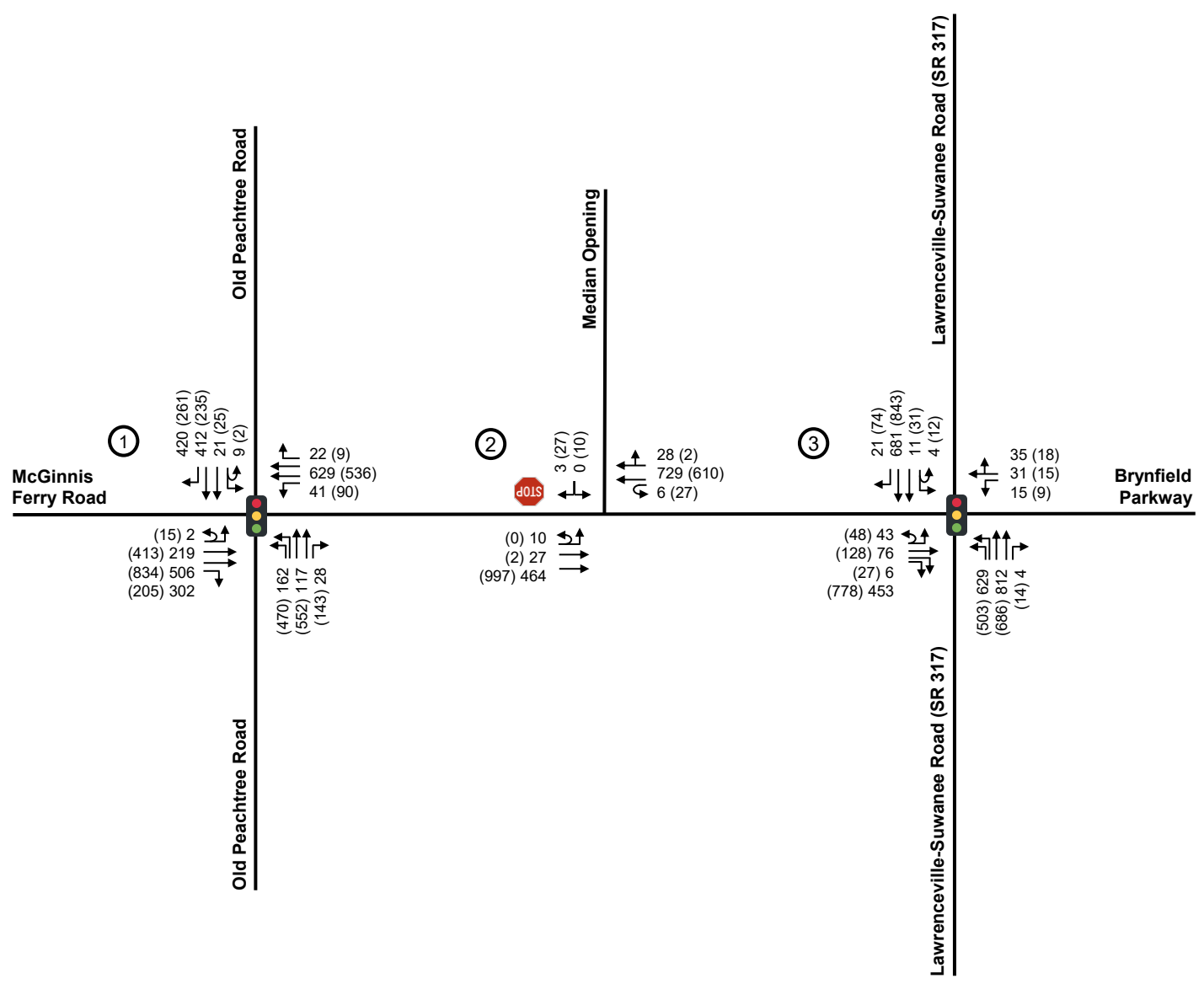
Vehicle peak hour turning movement counts were performed at the following study intersections:

1. Old Peachtree Road at McGinnis Ferry Road (signalized)
2. McGinnis Ferry Road at proposed Site Driveway A (unsignalized)
3. Lawrenceville-Suwanee Road (SR 317) at McGinnis Ferry Road/Brynfield Parkway (unsignalized)

The turning movement counts were collected during the AM period (6:30 AM – 8:30 AM) and the PM period (3:30 PM – 5:30 PM) on Wednesday, August 27, 2025. Raw traffic counts are provided in **Appendix D**. Peak hours for study intersections are shown in **Table 1**.

Table 1: Peak Hour Summary		
Intersection	AM Peak Hour	PM Peak Hour
1. Old Peachtree Road at McGinnis Ferry Road (signalized)	7:15 AM – 8:15 AM	4:30 PM – 5:30 PM
2. McGinnis Ferry Road at proposed Site Driveway A (unsignalized)	7:00 AM – 8:00 AM	4:30 PM – 5:30 PM
3. Lawrenceville-Suwanee Road (SR 317) at McGinnis Ferry Road/Brynfield Parkway (signalized)	7:15 AM – 8:15 AM	4:30 PM – 5:30 PM

Figure 3 illustrates the Existing 2025 traffic volumes of the study intersections and the existing roadway geometry (intersection layout).



LEGEND	
	Existing Roadway Laneage
XX	AM Peak Hour Traffic Volumes
(XX)	PM Peak Hour Traffic Volumes
	Existing Traffic Signal
	Existing Stop Control
	Intersection Reference Number

4.0 PROJECTED BACKGROUND (NON-PROJECT) TRAFFIC

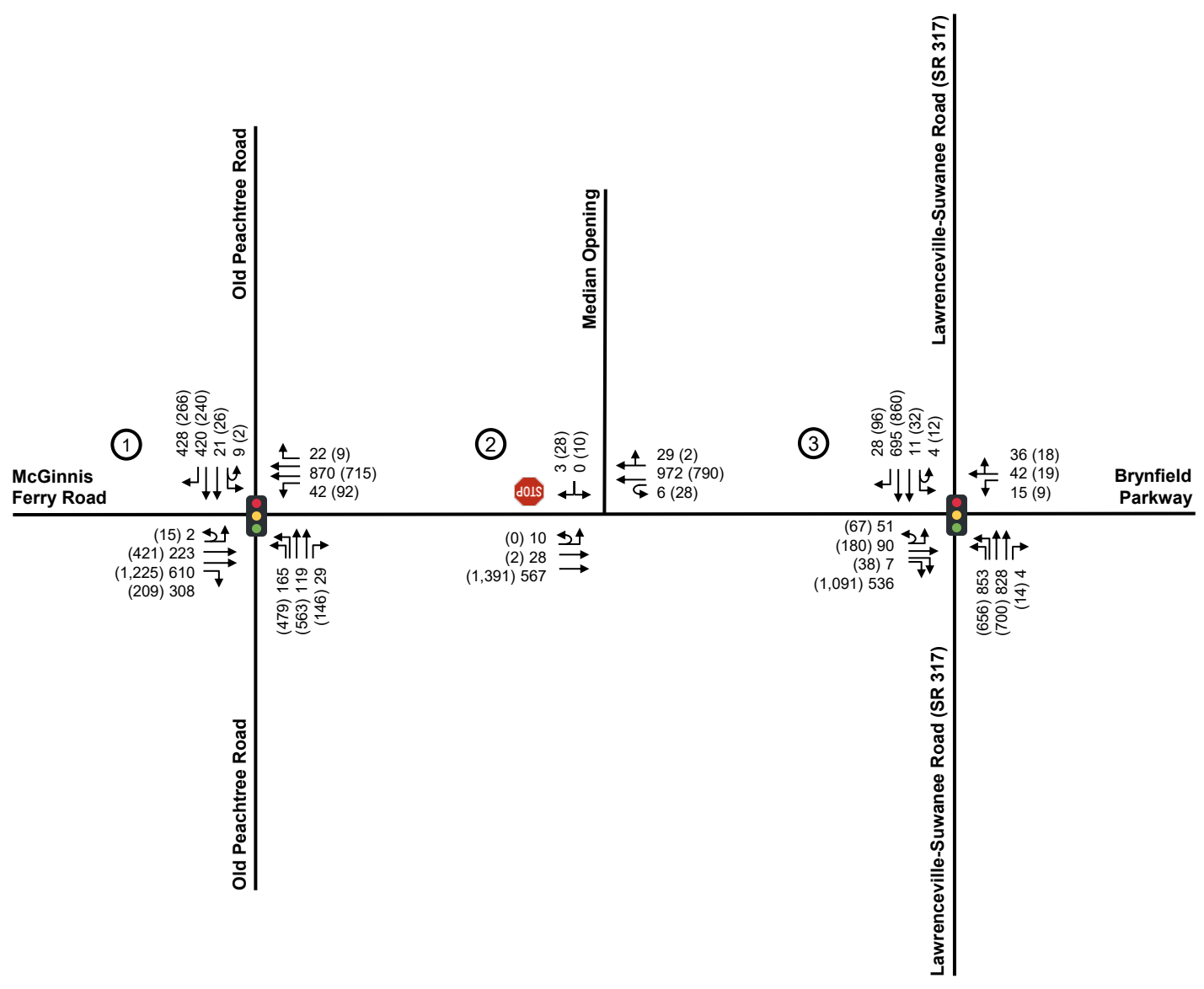
Projected background (non-project) traffic is defined as the expected traffic on the roadway network in the future year(s) absent the *Suwanee Residential* development. The Existing 2025 peak hour traffic volumes were increased by 1.0% per year for two (2) years to account for the expected background growth in traffic through year 2027. **Figure 4** illustrates the Projected 2027 No-Build traffic conditions.

4.1 FUTURE ROADWAY / INTERSECTION PROJECTS

The ARC's Atlanta Regional Plan, Gwinnett County SPLOST, and GDOT GeoPI project lists were researched for currently programmed transportation projects within the vicinity of the proposed development.

- **CIC2022-00006** – This project proposes to repurpose a tract of land along the eastern corner at the intersection of Old Peachtree Road and McGinnis Ferry Road to be a 4,000 square foot gas/convenience store with gas pumps, 2,000 square foot restaurants, and a 1,650 square foot retail space.
- **PI 0013104 (I-85 @ McGinnis Ferry New Interchange)** – This project proposes to construct south facing ramps from McGinnis Ferry Road to I-85, creating a half diamond interchange. The project will also include enhanced pedestrian facilities and a multiuse path. The project is currently under construction and is expected to open in 2026.

Following conversations with Gwinnett County staff, the future projected traffic volumes will assume the McGinnis Ferry New Interchange project will be open. Additional traffic along McGinnis Ferry Road, associated with this project, will be included in all future scenarios.



LEGEND

- Existing Roadway Laneage
- XX AM Peak Hour Traffic Volumes
- (XX) PM Peak Hour Traffic Volumes
- Existing Traffic Signal
- Existing Stop Control
- Intersection Reference Number

*PI 0013104 (I-85 @ McGinnis Ferry New Interchange) 2026 projected volumes were referenced for growth along McGinnis Ferry Road.
 **Existing 2025 traffic volumes grown at 1.0% per year for two (2) years.

5.0 PROJECT TRAFFIC

Project traffic used in this analysis is defined as the vehicle trips expected to be generated by the proposed development, and the distribution and assignment of that traffic through the study roadway network. This traffic impact study evaluates the impact of constructing 174 multifamily units on a 6.4-acre site.

5.1 PROJECT SITE ACCESS

Access to the proposed *Suwanee Residential* development will be provided via one (1) proposed site driveway, which is shown on the proposed site plan in **Appendix A**. A brief description of the site driveway is as follows:

- Site Driveway A (located along McGinnis Ferry Road) – a proposed full-movement, unsignalized driveway located in alignment with the development across the street. The driveway is proposed to have one (1) ingress lane entering the project site and two (2) egress lanes exiting the project site.
- Site Driveway B (Fire Access) – a proposed, right-in/right-out fire access only driveway located along the southern side of McGinnis Ferry Road approximately 585 feet east of Site Driveway A and is proposed to operate under side street gate control.

Refer to the site plan in **Appendix A** for a visual representation of vehicular access and circulation throughout the proposed development.

5.2 TRIP GENERATION

Traffic for the proposed development was calculated using the *Institute of Transportation Engineers' (ITE) Trip Generation Manual, 11th Edition, 2021*. The project trip generation for the proposed development was calculated based upon the Multi-Family Housing (Mid-Rise) land use (Land Use Code 221).

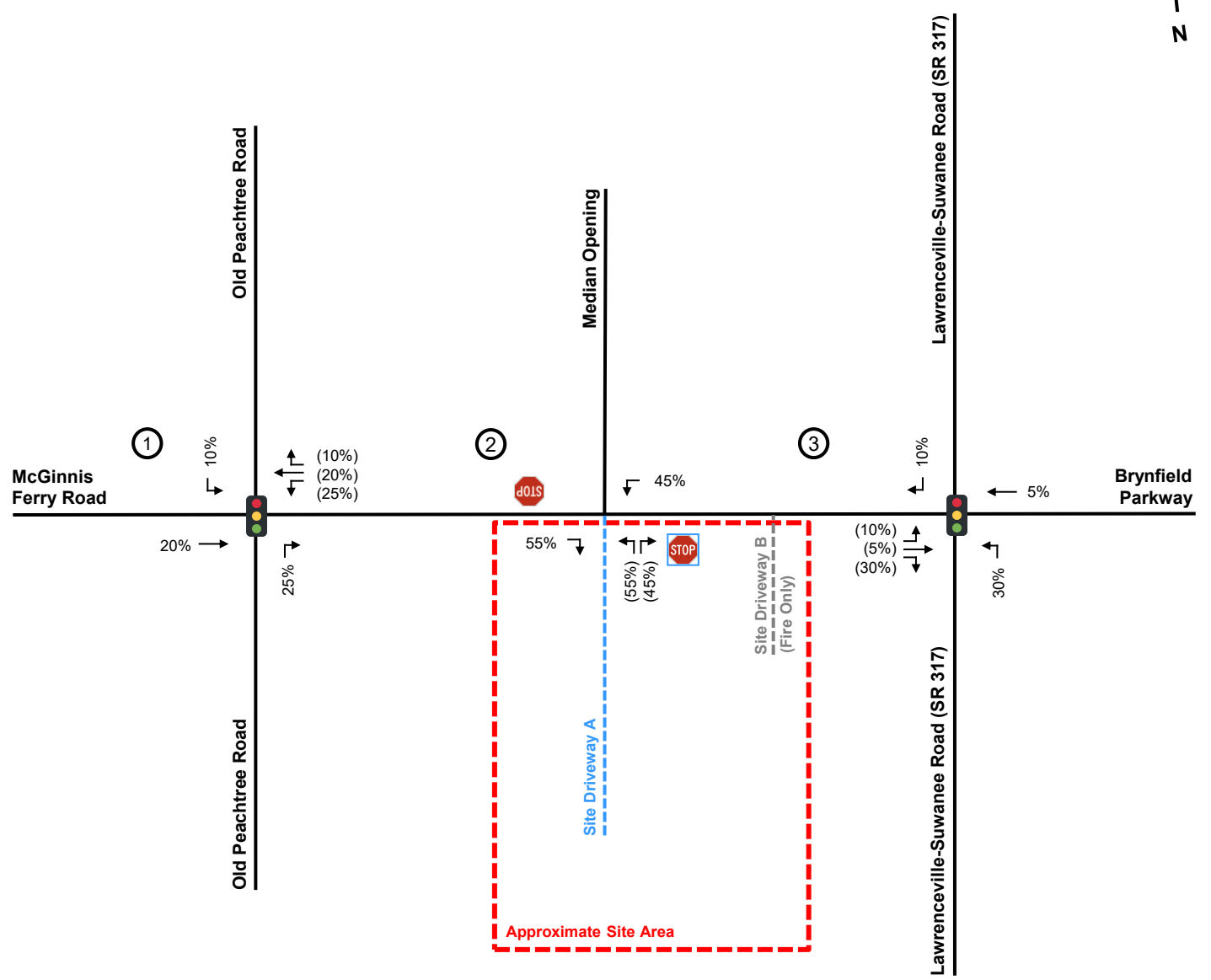
Table 2 summarizes the trip generation for the proposed development upon full build-out (2027). Due to the size, nature, and location of the proposed development, mixed-use, alternative mode, or pass-by reductions were assumed to be zero.

Table 2: Project Trip Generation Summary									
Land Use	ITE Code	Density	Daily Traffic			AM Peak Hour		PM Peak Hour	
			Total	Enter	Exit	Enter	Exit	Enter	Exit
Multi-Family Housing (Mid-Rise)	221	174 units	784	392	392	15	50	41	27
Total Gross Project Trips			784	392	392	15	50	41	27

5.3 TRIP DISTRIBUTION AND ASSIGNMENT

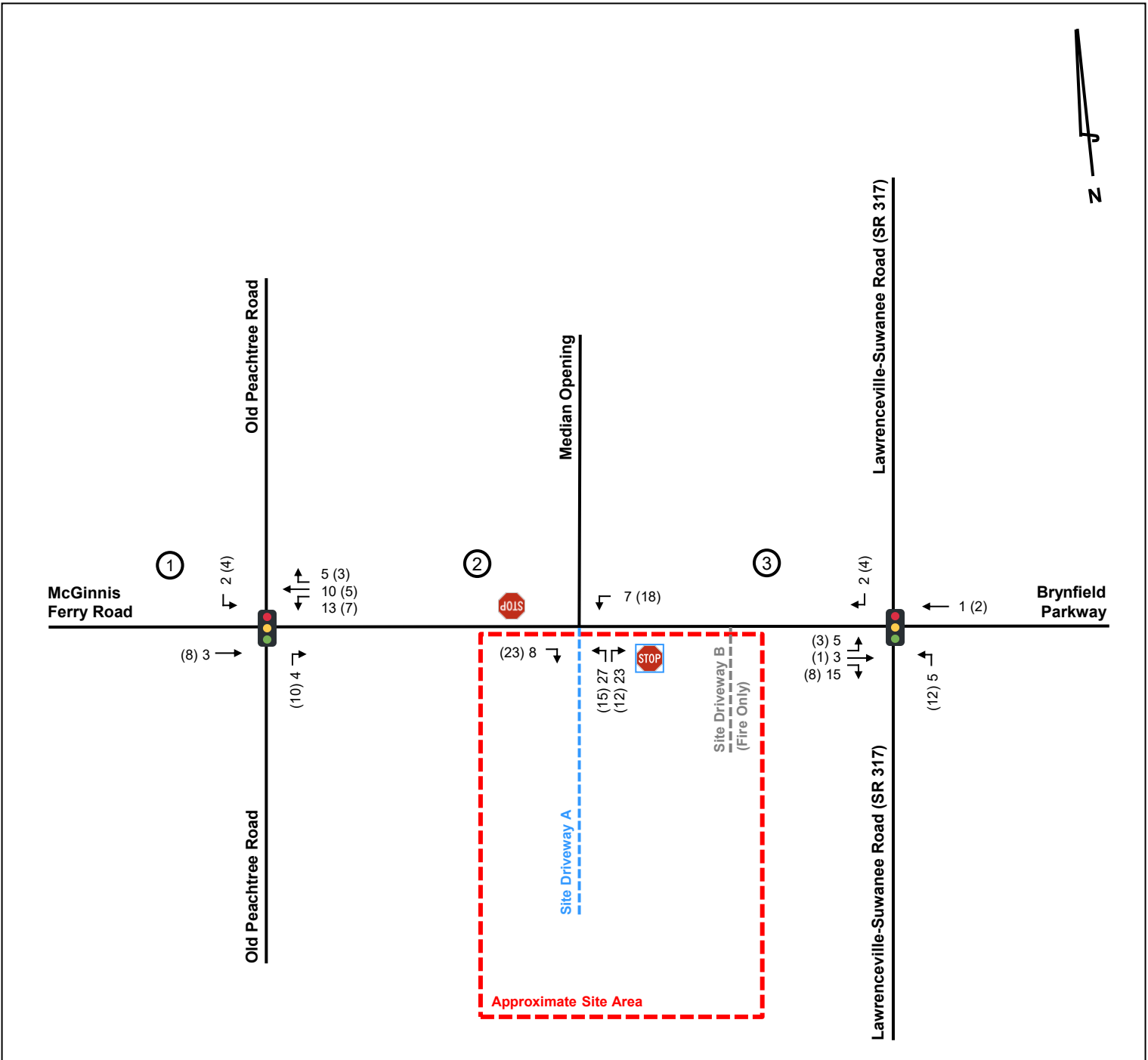
The directional distribution and assignment of adding new trips (project trips) related to the proposed development was based on a review of land uses, population densities, and existing travel patterns in the area. A detailed trip distribution and assignment for the residential trips is shown in **Figure 5**.

Based on the trip generation from **Table 2** and the anticipated trip distribution, new project trips were assigned to the study roadway network. **Figure 6** illustrates the project trips distributed throughout the study network for the Projected 2027 Build conditions. **Figure 7** illustrates the Projected 2027 Build traffic conditions for the AM and PM peak hours. **Appendix B** provides intersection volume worksheets for the study intersections.



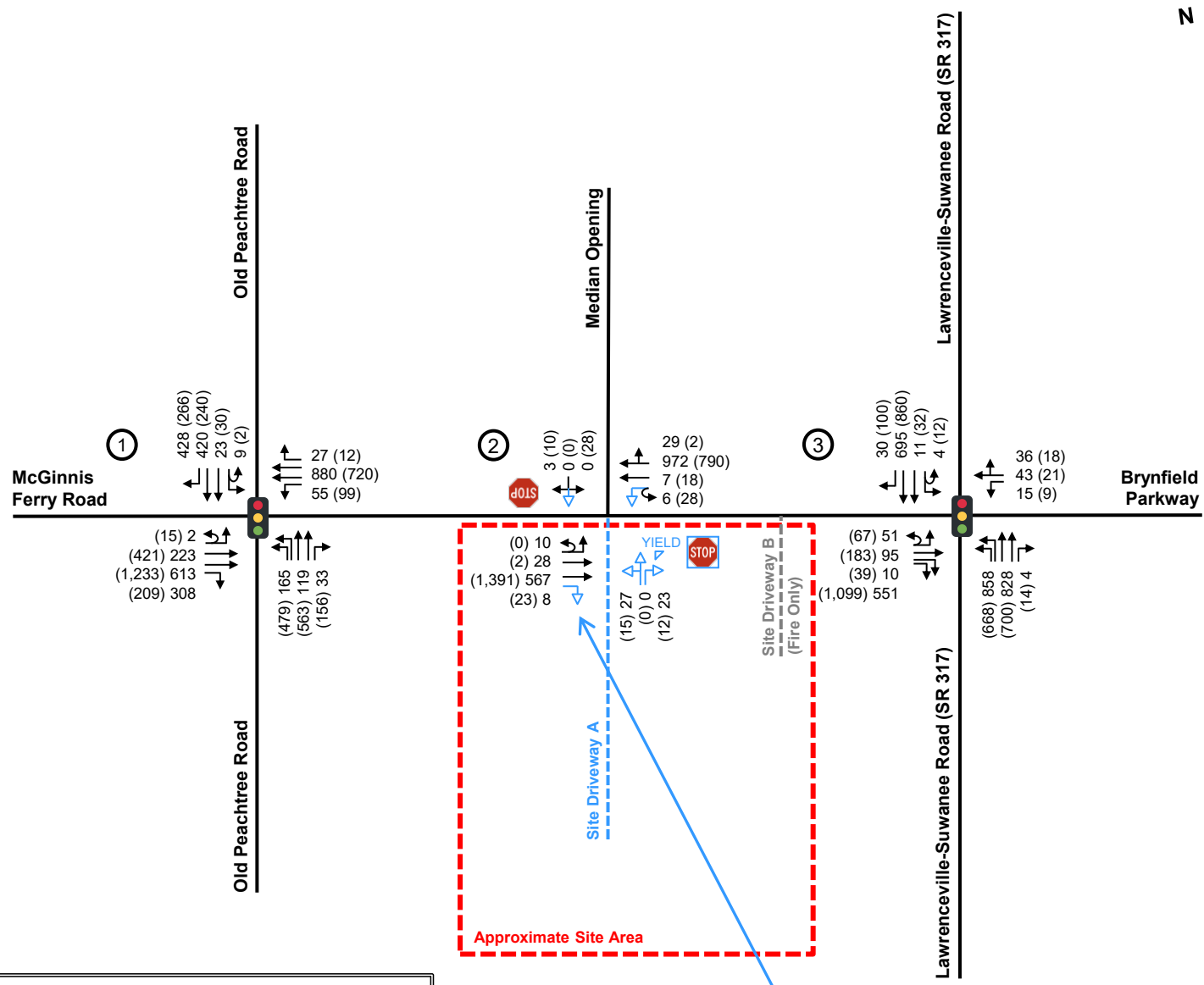
LEGEND

- Turning Movement
- XX % Traffic Entering
- (XX) % Traffic Exiting
- Existing Traffic Signal
- Existing Stop Control
- Proposed Stop Control
- Intersection Reference Number
- - - Proposed Site Driveway



LEGEND

- Turning Movement
- XX AM Peak Hour Project Trips
- (XX) PM Peak Hour Project Trips
- Existing Traffic Signal
- Existing Stop Control
- Proposed Stop Control
- (X) Intersection Reference Number
- Proposed Site Driveway



LEGEND

- Existing Roadway Laneage
- XX AM Peak Hour Traffic Volumes
- (XX) PM Peak Hour Traffic Volumes
- Existing Traffic Signal
- Existing Stop Control
- Proposed Stop Control
- (X) Intersection Reference Number
- - - Proposed Site Driveway
- Proposed Site Driveway

6.0 LEVEL-OF-SERVICE ANALYSIS

Level-of-service (LOS) determinations were made for the weekday AM and PM peak hours for the existing study network intersections using *Synchro Professional, Version 12.0*. The program uses methodologies contained in the *Highway Capacity Manual, 7th Edition* to determine the operating characteristics of an intersection. Capacity is defined as the maximum number of vehicles that can pass over a particular road segment or through a particular intersection within a specified period under prevailing roadway, traffic, and control conditions.

LOS is used to describe the operating characteristics of a road segment or intersection in relation to its capacity. LOS is defined as a qualitative measure that describes operational conditions and motorists' perceptions of a traffic stream. The *Highway Capacity Manual* defines six levels of service, LOS A through LOS F, with A being the best and F the worst.

LOS for unsignalized intersections, with stop control on the minor street(s) only, are reported for the side street approaches. Low levels-of-service for the side street approaches are not uncommon, as vehicles may experience a delay turning onto a major roadway.

LOS for signalized intersections is reported for the intersection as a whole. One or more movements at an intersection may experience a low level-of-service, while the intersection as a whole may operate acceptably.

In addition to the Existing 2025 conditions, an analysis was performed for the AM and PM peak hours under Projected 2027 No-Build and Build traffic conditions. The results of the LOS analysis are summarized for the AM and PM peak hours in **Table 3**.

Table 3: Level-of-Service Summary							
LOS (Delay in Seconds)							
Intersection	Approach & Movement	Existing 2025		Projected 2027 No-Build		Projected 2027 Build	
		AM Peak	PM Peak	AM Peak	PM Peak	AM Peak	PM Peak
1. Old Peachtree Road at McGinnis Ferry Road (signalized)	Overall	D (36.4)	D (36.3)	D (51.4)	D (48.7)	D (54.1)	D (49.2)
2. McGinnis Ferry Road at proposed Site Driveway A (unsignalized)	NB					C (15.9)	E (36.1)
	SB	B (11.9)	B (13.8)	B (13.5)	C (17.1)	B (13.5)	C (20.3)
	EBL	B (11.2)	A (8.9)	B (11.4)	A (9.6)	B (11.4)	A (9.6)
	WBL	B (10.1)	C (18.6)	B (11.0)	D (33.6)	A (9.9)	D (28.9)
3. Lawrenceville-Suwanee Road (SR 317) at McGinnis Ferry Road/Brynfield Parkway (signalized)	Overall	D (38.6)	D (36.7)	D (53.9)	D (53.8)	D (54.6)	D (54.7)

As shown in **Table 3**, the analysis indicates that under Existing 2025, Projected 2027 No-Build, and Projected 2027 Build conditions, all study intersections will operate at an acceptable LOS during both the AM and PM peak hours. The *Synchro* analysis reports are included in **Appendix C**.

7.0 CONCLUSION

This traffic study evaluated the traffic impacts associated with the *Suwanee Residential* development. The approximate 6.4-acre site is located south of McGinnis Ferry Road and west of Richmond Row Drive adjacent to the City of Suwanee in Gwinnett County, Georgia and involves the construction of a 174 multifamily units.

The study network, which consists of three (3) existing intersections, was analyzed for the weekday AM and PM peak hours under Existing 2025 conditions, Projected 2027 No-Build conditions (two years of background traffic growth), and Projected 2027 Build conditions (Projected 2027 No-Build conditions plus traffic generated by the proposed *Suwanee Residential* development).

Based on the results of this traffic impact study, all study intersections will operate at an acceptable LOS during both the AM and PM peak hours under Existing 2025, Projected 2027 No-Build, and Projected 2027 Build conditions.

Kimley-Horn and Associates, Inc. does not recommend any system improvements based on the results of this study (note: this would be the improvements needed to serve the background road network traffic).

7.1 SITE ACCESS IMPROVEMENT RECOMMENDATIONS

Based on the results of this study, Kimley-Horn and Associates, Inc. recommends the following site access improvements to serve the Projected 2027 Build traffic conditions (note: this would be the improvements needed to serve traffic associated with the *Suwanee Residential* development).

- Intersection 2 – McGinnis Ferry Road at proposed Site Driveway A
 - Restripe existing pavement to provide an exclusive eastbound right-turn lane along McGinnis Ferry Road, in accordance with Gwinnett County standards for a minor arterial roadway.
 - On the site, construct a driveway along McGinnis Ferry Road with two (2) egress lanes exiting the site and one (1) ingress lane entering the site.
 - Restripe the westbound Left-turn lane from a U-turn arrow pavement marker to be a Left-turn arrow pavement marker.

Exhibit H: Public Participation Plan and Report

[attached]

Public Participation Plan

- 1. Who do you intend to contact in addition to property owners within 1,000 feet of the site, if any?**

District Planning Commissioners and District Commissioner

- 2. Where do you plan to host the Public Participation Meeting? Ensure that the meeting is held in person at a convenient location for interested parties.**

Courtyard by Marriott Atlanta Suwanee

310 Celebration Dr, Suwanee, GA 30024

- 3. What date and time do you plan to host the Public Participation Meeting? Ensure that the meeting is held at a convenient time for interested parties, e.g. not during typical work hours on weekdays.**

February 18, 2026 at 6:00pm

- 4. What is your method for providing opportunities for discussion with interested parties at the Public Participation Meeting?**

After a presentation, the Applicant team would open the meeting for comments and questions from the attendees.

Public Participation Report

- 1. List all groups that you notified of the requested application. Include a copy of the notification package and stamped Postal Service Form 3665 (attached) with itemized name and address of each addressee.**

Property owners within 1,000ft of the Subject Property, District Planning Commissioners, and District Commissioner.

- 2. Provide the date, time, and location of all meeting(s) that the applicant and/or representative attended to discuss an application with interested parties.**

Wednesday, February 18, 2026 at the Home2Suites (1765 North Brown Road, Lawrenceville) at 6:30pm.

- 3. Provide the number of people who participated in the meeting(s). Include the sign-in sheet(s) with meeting date, time, location, and attendee names.**

Two people attended the meeting. The Applicant team had five representatives present.

- 4. What issues and concerns were expressed by attendees at the meeting(s)?**

Question about parking ratio.

Question about location of dumpster.

Question about the need for a retaining wall.

Question about whether trees will remain on the western portion of the Property.

- 5. What are the applicant's responses to the issues and concerns that were expressed at the meetings? What changes to the development will be made?**

The parking ratio is 1.55 spaces per unit.

The dumpster will be mainly located within the parking structure but moved outside for pickup.

The retaining wall is necessary due to the topography of the Subject Property.

The tress on the western portion of the Property located within the undisturbed buffer will remain.

Exhibit I: Internal and External Agency Review Comments

[attached]



**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		02/20/2026	
Department/Agency Name:		Transportation	
Reviewer Name:		Brent Hodges	
Reviewer Title:		Construction Manager 2	
Reviewer Email Address:		Brent.Hodges@gwinnettcountry.com	
Case Number:		REZ2026-00017	
Case Address:		2730 McGinnis Ferry Road, Suwanee, 30024	
Comments:		<input checked="" type="checkbox"/>	<input type="checkbox"/>
		YES	NO
1	McGinnis Ferry Road is a major arterial. The Average Daily Traffic (ADT) volume is 8,112.		
2	The nearest Ride Gwinnett facility (Stop 5036 / Route 50) is located 2.1 miles away at Satellite Boulevard and Shawnee Industrial Way.		
3	The developer shall confirm that the permanent easement in place along the site frontage of McGinnis Ferry Road is for the maintenance of slopes only and confirm that no utilities are included within this easement.		
4			
5			
6			
7			
Recommended Zoning Conditions:		<input checked="" type="checkbox"/>	<input type="checkbox"/>
		YES	NO
1	The developer shall conduct a stage 1 and stage 2 intersection control evaluation (ICE) analysis for the site driveway located at the median break along McGinnis Ferry Road.		
2	The developer shall implement any upgrades recommended by the ICE analyses, subject to the review and approval of the Gwinnett County Department of Transportation.		
3			
4			
5			
6			
7			

Note: Attach additional pages, if needed

Revised 7/26/2021

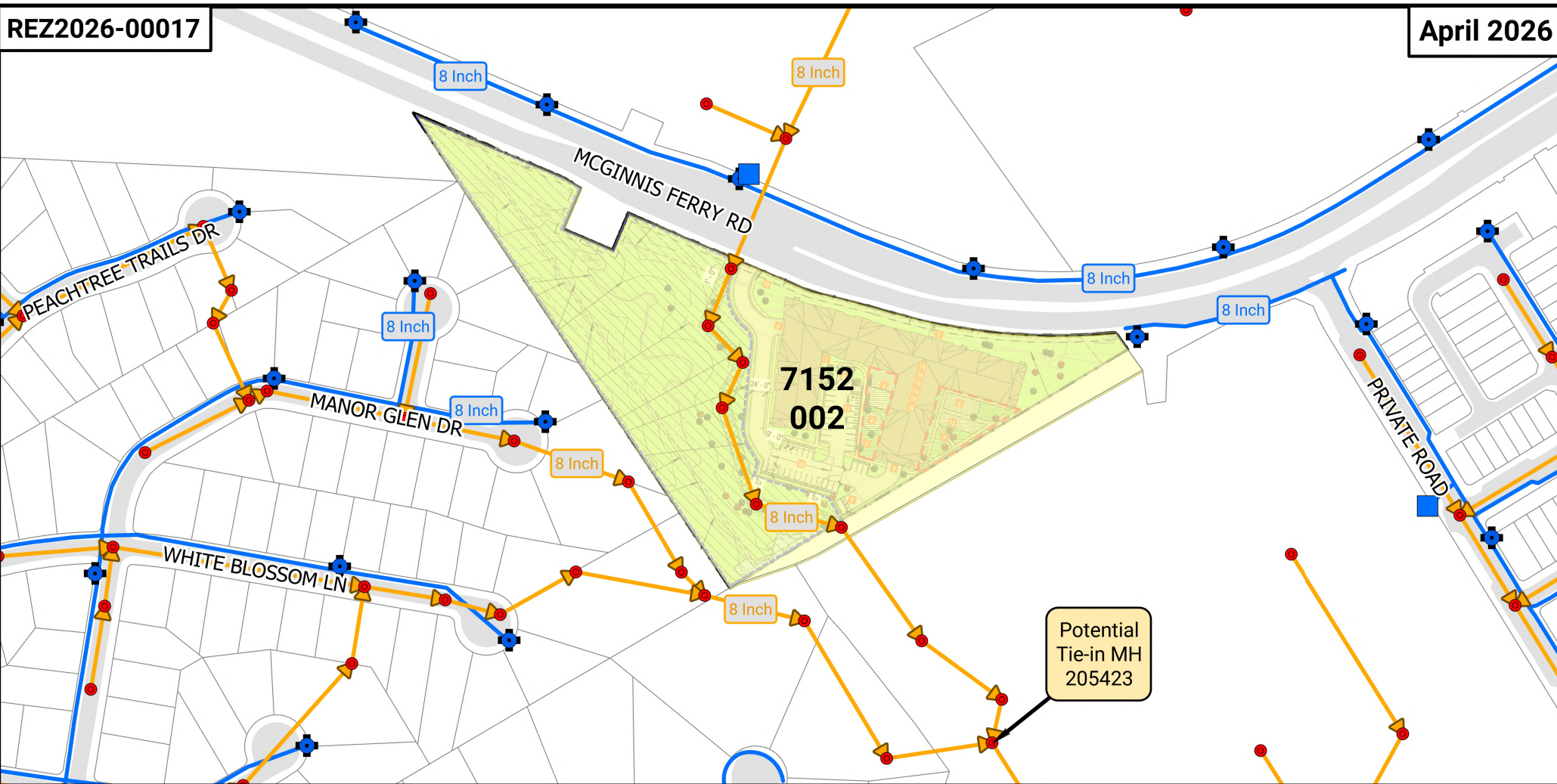


**Department of Planning and Development
TECHNICAL REVIEW COMMITTEE**

TRC Meeting Date:		
Department/Agency Name:	DWR	
Reviewer Name:	Lorraine Campagne	
Reviewer Title:	Section Manager	
Reviewer Email Address:	Lorraine.Campagne@gwinnettcounty.com	
Case Number:	REZ2026-00017	
Case Address:	2730 McGinnis Ferry Rd	
Comments:		<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO
1	Water: The proposed development may connect to an existing 8-inch water main located on the northern right-of-way of McGinnis Ferry Road or an existing 8-inch water main located on the southern right-of-way of McGinnis Ferry Road.	
2	Sewer: A Sewer Capacity Certification (C2025-268-10) was approved in October 2025 for 180 apartment units. The proposed development plans to connect to an existing 8-inch gravity sewer located on the subject site. A Development Participation Agreement (DPA) is required between the developer and Gwinnett County for the installation of two larger pumps at the Lawrenceville-Suwanee Pump Station adequate to accommodate the proposed development.	
3		
4		
5		
6		
7		
Recommended Zoning Conditions:		<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO
1		
2		
3		
4		
5		

Note: Attach additional pages, if needed

Revised 7/26/2021

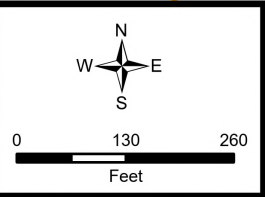


LEGEND

Flow Management	Hydrant	Sewer Collector
Pump Station	City	Sewer Interceptor
Regional	Water Main	Sewer Force Main
Manhole	Proposed Water Main	Proposed Sewer Collector

2730 McGinnis Ferry Rd
 RA-200 to MRR

Water & Sewer Utility Map



Water Comments: The proposed development may connect to an existing 8-inch water main located on the northern right-of-way of McGinnis Ferry Road or an existing 8-inch water main located on the southern right-of-way of McGinnis Ferry Road.

Sewer Comments: A Sewer Capacity Certification (C2025-268-10) was approved in October 2025 for 180 apartment units. The proposed development plans to connect to an existing 8-inch gravity sewer located on the subject site. A Development Participation Agreement (DPA) is required between the developer and Gwinnett County for the installation of two larger pumps at the Lawrenceville-Suwanee Pump Station adequate to accommodate the proposed development.

Water Availability: Water demands imposed by the proposed development may require upsizing or extensions of existing water mains in order to meet Gwinnett County Standards and fire flow demands. Any cost associated with such required improvements will be the responsibility of the development. Current Gwinnett County Standards require a minimum of 12" pipe size for commercial developments and a minimum of 8" pipe size for residential developments. Additionally, connection to a minimum of 12" and 8" mains are required for commercial and residential developments, respectively. It is the responsibility of the developer's engineer to confirm pressure and volumes are available for the development.

Sewer Availability: A Sewer Capacity Certification must be obtained from Gwinnett County to confirm the existing system can serve the development. Sewer demands imposed by the proposed development may require upsizing and/or extensions of existing sewer mains, and/or upsizing of an existing pump station, and/or installation of a new pump station. Any cost associated with such required improvements will be the responsibility of the development. The developer shall provide easements for future sewer connection to all locations designated by Gwinnett County during plan review.

Water and Sewer Design and Construction Requirements: Extensions of the water and/or sanitary sewer systems within the subject development must conform to this department's policies, Gwinnett County's ordinances, and the Water Main and Sanitary Sewer Design and Construction Standards and Specifications, dated April 5th, 2016. Subsequent to design, construction, inspection, and final acceptance of the required utilities, service would then become available under the applicable utility permit rate schedules.

Private Road Developments: Any development with private roads must comply with the Standard Policy Requirement for the Installation of Water and Sanitary Sewer Mains within Private Developments. This policy stipulates minimum easement requirements and location of public mains and appurtenances, among other requirements.

Residential Rezoning Impact on Local Schools
Prepared for Gwinnett County BOC, April, 2026

											Proposed Zoning
	School	2025-26			2026-27			2027-28			Approximate Student Projections from Proposed Developments
		Forecast	Capacity	+/- Cap.	Forecast	Capacity	+/- Cap.	Forecast	Capacity	+/- Cap.	
	Dacula High School	2429	2,550	-121	2,477	2,550	-73	2,526	2,550	-24	5
REZ2026-00013 (ZON2025-00001)	Dacula Middle School	1921	1,900	21	1,959	1,900	59	1,948	1,900	48	4
	Alcova Elementary School	1496	1,150	346	1,525	1,150	375	1,556	1,150	406	7
	Grayson High School	3738	3,000	738	3,850	3,000	850	3,946	3,000	946	7
REZ2026-00014 (ZON2025-00091)	Bay Creek Middle School	1646	1,150	496	1,704	1,150	554	1,755	1,150	605	5
	Trip Elementary School	1,708	1,200	508	1,776	1,200	576	1,838	1,200	638	9
	Seckinger High School	2440	2,800	-360	2,485	2,800	-315	2,525	2,800	-275	18
REZ2026-00016 (ZON2025-00095)	Jones Middle School	1852	1,575	277	1,888	1,575	313	1,907	1,575	332	12
	Patrick Elementary School	1030	1,025	5	1,045	1,025	20	1,059	1,025	34	23
	Collins Hill High School	2420	2,625	-205	2,395	2,625	-230	2,372	2,625	-253	24
REZ2026-00017 (ZON2025-00113)	Creekland Middle School	1702	2,100	-398	1,720	2,100	-380	1,748	2,100	-352	16
	Walnut Grove Elementary School	821	1,200	-379	830	1,200	-370	818	1,200	-382	30

Exhibit J: Maps

[attached]



MCGINNIS FERRY RD

WHITE BLOSSOM CT

MANOR GLEN DR

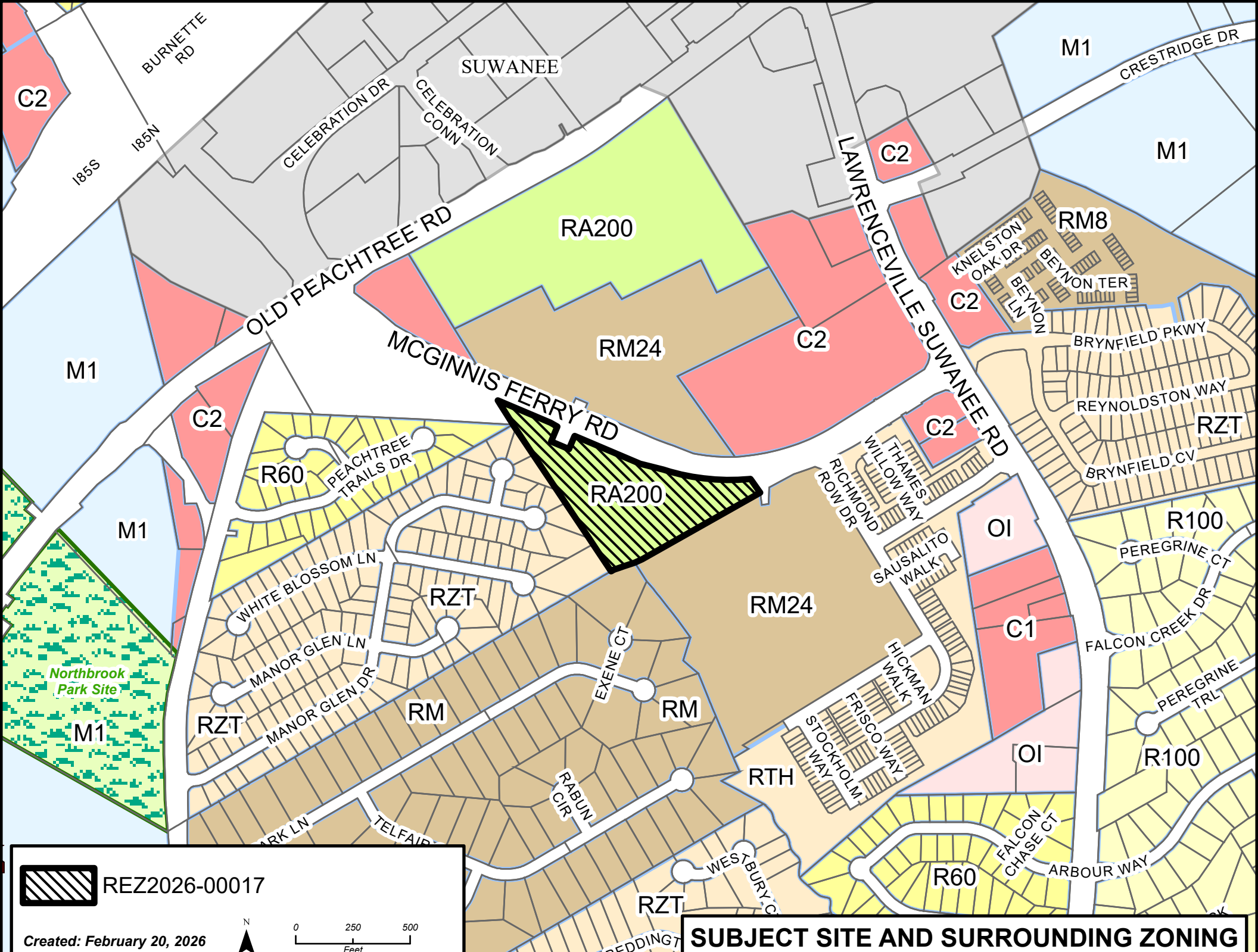
WHITE BLOSSOM LN


 REZ2026-00017

Created: February 20, 2026



0 80 160
Feet



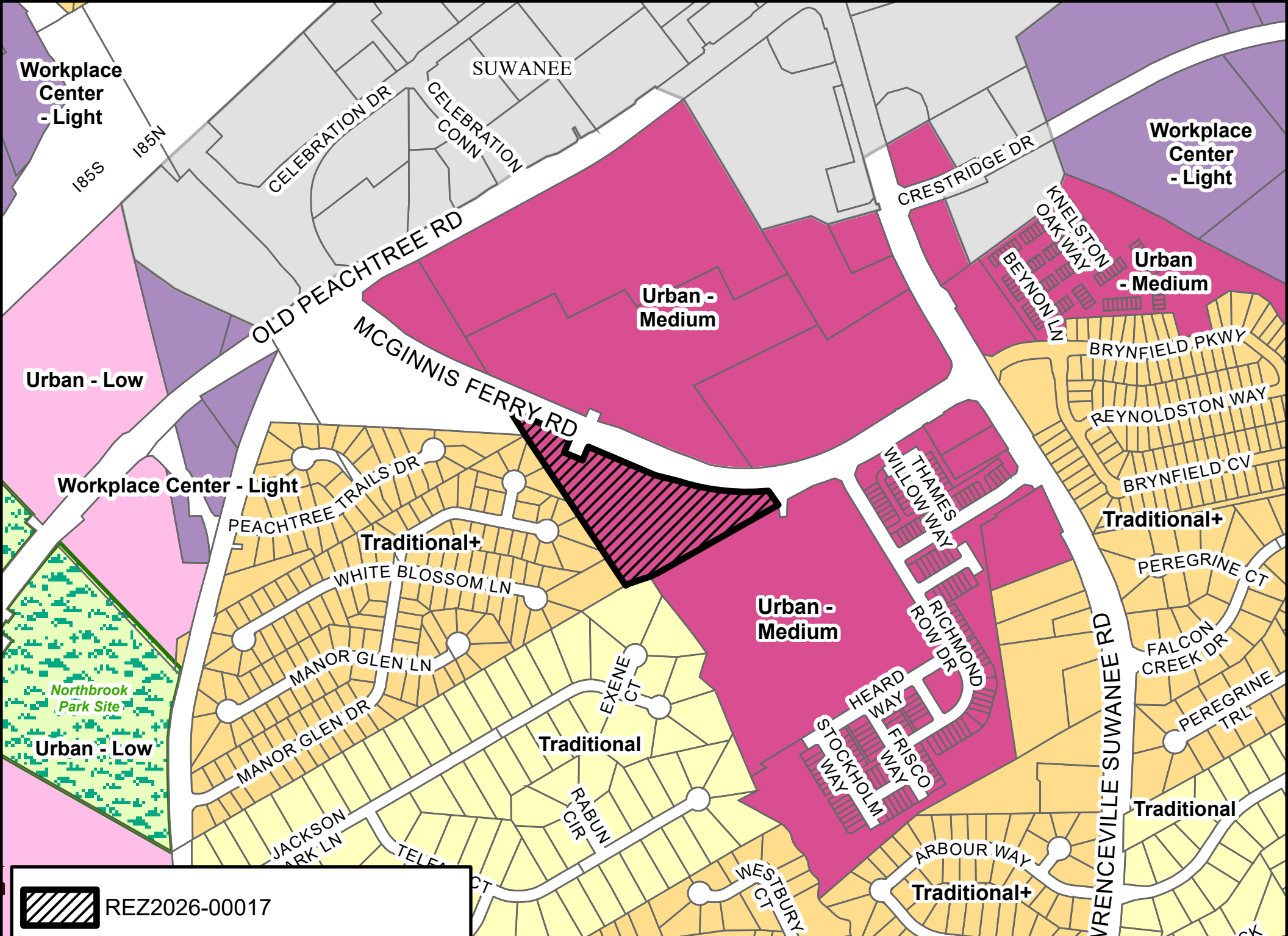
 REZ2026-00017


Created: February 20, 2026

0 250 500 Feet

N

SUBJECT SITE AND SURROUNDING ZONING



 REZ2026-00017

Created: February 20, 2026

2045 UNIFIED PLAN FUTURE DEVELOPMENT CLASSIFICATION